

Livable Communities Model Land Development Code



**Pinellas County
Metropolitan Planning Organization
2008**



**Prepared by the
Pinellas County Planning Department
and
The Renaissance Planning Group**

**Guidance and Oversight of the Project Provided by
The Livable Communities Task Force**

LIVABLE COMMUNITIES MODEL LAND DEVELOPMENT CODE

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Livable Community Model Land Development Code Pinellas County MPO

Overview

As an outgrowth of their efforts to review and update the Pinellas County Transportation Impact Fee Ordinance, the MPO's Livable Communities Task Force began an initiative in 2007 to encourage the development of "livable communities" throughout the County. The term "livable communities" is used to describe urban environments where walking, bicycling and transit service is safe, comfortable and efficient and where the physical environment offers an interesting and unique experience from the standpoint of street, land and building design. Additionally, "livable communities" are characterized by a mix of land uses that allow people to live closer to places where they work, shop and play.

The first phase of the MPO's "livable communities" initiative was completed on June 13, 2007 with the approval of the *Livable Communities Model Comprehensive Plan Objectives and Policies* by the MPO policy board. The development of the Model Land Development Code as presented in this document is the second phase of the "livable communities" initiative. The Model Code is intended to provide a tool-kit for local governments wishing to maintain the character or livability of an area or to change it to a more desirable urban form.

Section One of this Model Code defines the five individual districts, *Urban Core*, *Town Center*, *Traditional Neighborhood*, *Neighborhood Center* and *Suburban Center*, and associated land development standards. These districts are intended for utilization by the local governments to identify the areas within their jurisdictions where the corresponding land development, building and site design standards, provided in Sections One and Two, could be effectively applied. It would be the decision of the local governments to determine which of the districts and associated standards could be utilized to meet their comprehensive planning objectives relative to maintaining and/or developing livable community environments through their local site plan review processes.

In addition, Section One includes a listing of the Countywide Plan map categories that correspond with the land development standards in terms of density/intensity and land use activity. Implementation of the development standards needs to be consistent with the Countywide Plan Rules as well as locally adopted comprehensive plans.

Section Two provides building and site design standards addressing architecture, parking, public art, orientation of structures on a site, driveways, open space, connectivity, signs, pedestrian movement and stormwater. Section Three provides for

optional development standards that could be implemented as credit toward a development project's transportation impact fee assessment or to allow the project to comply with local concurrency management system requirements. Determining impact fee credits would be based on the cost value of the "livable community" improvement(s) in accordance with the Transportation Impact Fee Ordinance. Credit toward meeting the project's concurrency requirements would be based primarily on the benefit the livable community improvement(s) would provide to the transportation system.

It is important to note that although the focus of the Model Code is on land development, the design and construction of road improvement projects is critical to supporting livable community environments. The Federal Highway Administration and U.S. Department of Transportation have been advancing the concept of Context Sensitive Solutions (CSS) in recent years as a method to develop transportation systems that are compatible with their surroundings from the standpoint of aesthetics, and the scenic, historic and environmental assets of adjacent lands. Context Sensitive Solutions is a process that provides state and local governments a flexible approach in the application of engineering standards relative to the planning and design of transportation projects that provide for bicycle, pedestrian and transit needs as well as for automobile travel. The Institute of Transportation Engineers' 2006 publication, *Context Sensitive Solutions in Designing Major Urban Thoroughfares for Walkable Communities*, is recommended as a guide for the application of CSS for use in association with the implementation of this Model Code.

Lastly, it is important to emphasize that, as with the Model Comprehensive Plan Objectives and Policies, the Model Code is not intended to be "one size fits all" in terms of its application by Pinellas County's local governments. Understanding that the objectives, policies and needs of the local governments relative to livable communities may vary, it is recognized that some portions of the Model Code will not be applicable to all the jurisdictions that choose to utilize it. It is further recognized that some of the provisions included in the Model Code may require some modification by the local governments in order for the Code to better serve their individual needs and objectives.

Section 1

District Descriptions and Development Standards

Urban Core



Renaissance Planning Group



Downtown Clearwater



Downtown St. Petersburg

District Type: Urban Core

The Urban Core District applies to the downtown areas of larger cities that serve as countywide and regional hubs of employment, commercial and civic activity. This District includes high concentrations of population density as well as a prevalence of mixed-use development and an active pedestrian environment. The street network for this District is typically a combination of minor arterial and collector roads with on-street parking. The District is usually well served by public transportation. The distance between the center of this District and its periphery is approximately a 20 to 30 minute walk or about .5 mile to .75 mile.



Downtown St. Petersburg

Uses within this District include major employers, hotels, high rise multi-family complexes, auditoriums, government services and facilities, hospitals, educational institutions, restaurants, specialty retail, marinas and commercial recreational complexes. Building architecture within the Urban Core District is unique and distinctive. Additionally, this District offers a wide range of civic amenities such as parks, plazas or public squares, public restrooms, informational displays, outdoor dining, transit terminals, libraries, and art displays. Local examples of this District are the downtown areas of Clearwater and St. Petersburg.

Applicable Countywide Plan Map Categories:

The chart shown below is intended to identify Countywide Plan Map categories by plan classification that are typically found within the boundaries of the Urban Core District. It should be noted that development projects seeking the maximum floor area ratio, units per acre and/or impervious surface ratio identified in the Urban Core development standards on the following page would require a special area plan approved by the local jurisdiction, the Pinellas Planning Council and the Countywide Planning Authority.

Special Designation:	Activity Center, Central Business District and Community Re-development District
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Urban Core Development Standards

Block Face:	200' - 400'
Single-Use Size Limit (ground floor):	75,000
Floor Area Ratio:	3.0 - 4.0
Units Per Acre:	75 - 90
Impervious Surface Ratio:	.95 maximum
Ground Level Open Space:	.05 minimum total land area of site
Build To Lines:	Building height between 0 to 75' - 0' Building height above 75' - 10'
Percent Of Building Oriented To Build To Line:	.80 minimum
Building Fenestration/ Transparency:	Non-residential or mixed-use: Facades fronting primary street - .50 transparency min. Facades fronting secondary street - .30 transparency min.
Distances Between Buildings:	Blank wall to blank wall for buildings up to 75' - 0' min. Blank or window wall to window wall up to 75' - 15' min. All conditions above 75' - 60' min.
Maximum Floor Plate:	Above 75' - 30,000 square feet per building
Sidewalks:	10' - 15' minimum width with 5' minimum clearance
Stormwater/ Drainage :	Regional/community, underground, green roofs, pervious surfaces. On-site ponds are prohibited.

Notes:

- 1) Single-use size limit applies only to commercial/retail land uses.
- 2) Fenestration includes windows, doors, shutters, trim and other compatible architectural features.
- 3) Buildings must be no less than 10 feet from curb. Colonnades, galleries, and arcades may not encroach into the ROW.
- 4) Awnings and canopies which have a minimum vertical clearance of 10' are exempt from build-to line provision.
- 5) Parking garages within principal structure not included in calculation of floor area ratio.
- 6) Floor area ratios and units per acre are maximums that could be exceeded with inclusion of bonus provisions, consistent with a special area plan, such as alleviating the visibility of parking from the street with liner uses or through architectural design, financial support to streetscape improvements, public art, restoration of historic structure or workforce housing.

Town Center



Renaissance Planning Group



Downtown Safety Harbor



Downtown Dunedin

District Type: Town Center

The Town Center District applies to compact downtown areas that are less densely populated than the Urban Core District. The Town Center District is developed with a mix of residential and non-residential uses with individual architectural styles, ranging in densities and intensities that are reflective of the area's cultural heritage and distinctive character. The District contains uses such as dry cleaners, banks, hair salons, offices, small scale hotels, restaurants, and specialty boutique retail that are oriented toward serving the surrounding community .



Downtown Palm Harbor

Also, this District provides for a range of public amenities although not to the scale and extent provided by the Urban Core District. These amenities include, but are not limited to, public gathering areas, trails, public art displays, parks and transit service. The street network consists of one or two collector roads with relatively low traffic volumes and on-street parking. The distance between the center of this District and its edge is approximately a 10 minute walk or about .33 to .25 mile. Local examples of this District are the downtown areas of Tarpon Springs, Safety Harbor, Gulfport, Palm Harbor, Largo and Dunedin.

Applicable Countywide Plan Map Categories:

The chart shown below is intended to identify Countywide Plan Map categories by plan classification that are typically found within the boundaries of the Town Center District. It should be noted that development projects seeking the maximum floor area ratio, units per acre and/or impervious surface ratio identified in the Town Center development standards on the following page would require a special area plan approved by the local jurisdiction, the Pinellas Planning Council and the Countywide Planning Authority.

Planned Redevelopment:	Mixed-Use and Commercial
Special Designation:	Central Business District and Community Redevelopment District

Town Center Development Standards

Block Face:	200' - 450'
Single-Use Size Limit (ground floor):	30,000 square feet
Floor Area Ratio:	1.25 - 2.5
Units Per Acre:	25 - 40
Impervious Surface Ratio:	.95 maximum
Ground Level Open Space:	.05 minimum total land area of site
Build To Lines:	Building height between 0 and 50' - 0' Building height above 50' - 20'
Percent Of Building Oriented To Build To Line:	.75 minimum
Building Fenestration/ Transparency:	Non-residential or mixed-use: Facades fronting primary street -.50 transparency min. Facades fronting secondary street -.30 transparency min.
Distances Between Buildings:	Blank wall to blank wall for buildings up to 50' - 0' min. Blank or window wall to window wall up to 50' - 15' min. All conditions above 50' - 60' minimum
Maximum Floor Plate:	Above 50' - 15,000 square feet per building
Sidewalks:	6' - 10' minimum width, with 5' minimum clearance
Stormwater/ Drainage :	Community, underground, green roofs, pervious surfaces. On-site ponds are prohibited.

Notes:

- 1) Single-use size limit applicable only to commercial/retail land uses.
- 2) Fenestration includes windows, doors, shutters, trim and other compatible architectural features.
- 3) Buildings must be no less than 10 feet from curb. Colonnades, galleries, and arcades may not encroach into the ROW.
- 4) Awnings and canopies which have a minimum vertical clearance of 10' are exempt from build-to line provision.
- 5) Parking garages within principal structure not included in calculation of floor area ratio.
- 6) Floor area ratios and units per acre are maximums that could be exceeded with inclusion of bonus provisions, consistent with a special area plan, such as alleviating the visibility of parking from the street with liner uses or through architectural design, financial support to streetscape improvements, public art, restoration of historic structure or workforce housing.

Traditional Neighborhood



Renaissance Planning Group



West Bay Village, Largo



Scotland Street, Dunedin

District Type: Traditional Neighborhood

The Traditional Neighborhood District applies to older residential areas with compact development on small lots adjacent to alleys along the rear property lines. The street network is served primarily by two-lane local streets with low traffic volumes.

Small scale non-residential uses such as cafés, pubs, family owned restaurants, convenience markets, laundromat/dry cleaner, professional office, churches and daycare facilities that primarily serve the neighborhood residents may exist on corner lots adjacent to more heavily traveled collector streets.



Old Northeast Neighborhood, St. Petersburg

Residential uses within the Traditional Neighborhood District are characterized by a predominance of wood frame homes with large front porches, detached garages and/or accessory dwelling units. From the center of the District to its edge is approximately .5 to 1 mile or a 15 to 30 minute walk. Local examples of these Districts include Old Northeast and Woodlawn in St. Petersburg as well as the neighborhoods located adjacent to the downtown areas of Dunedin, Largo and Safety Harbor.

Applicable Countywide Plan Map Categories:

The chart shown below is intended to identify Countywide Plan Map categories by plan classification that are typically found within the boundaries of the Traditional Neighborhood District.

Residential:	Residential Low, Residential Urban and Residential Low Medium
Mixed-Use:	Residential/Office Limited, Residential/Office General and Residential/Office/Retail
Commercial:	Commercial Neighborhood, Commercial Limited and Commercial General
Public/Semi-Public:	Recreation/Open Space and Institutional
Planned Redevelopment:	Residential and Mixed-Use
Special Designation:	N/A

Traditional Neighborhood Development Standards

Block Face:	200' - 500'	
Single-Use Size Limit (ground floor):	15,000 square feet	
Floor Area Ratio:	.40 - .50	
Units Per Acre:	5 - 10	
Impervious Surface Ratio:	.55 maximum	
Ground Level Open Space:	n/a	
Build To Lines:	Residential: Front:15' Side: 10'	Non-residential: Front:10' Side: 10'
Percent Of Building Oriented To Build To Line:	.75 minimum	
Building Fenestration/ Transparency:	.30 minimum fenestration on primary street facades. .20 minimum fenestration on rear and side of building. Further detail provided in Section Two.	
Height:	2-story maximum	
Distances Between Buildings:	n/a	
Maximum Floor plate:	n/a	
Sidewalks:	5' minimum width. Connection from building entrances to frontage sidewalks required.	
Stormwater/ Drainage :	Community, underground, green roofs, pervious surfaces. On-site ponds are prohibited.	

Notes:

- 1) Single-use size limit applies only to commercial/retail land uses.
- 2) Fenestration includes windows, doors, shutters, trim and other compatible architectural features.
- 3) Buildings must be no less than 10 feet from curb. Colonnades, galleries, and arcades may not encroach into the ROW.
- 4) Awnings and canopies which have a minimum vertical clearance of 10' are exempt from build-to line provision.
- 5) Floor area ratios and units per acre are maximums.

Neighborhood Center



Renaissance Planning Group



Northeast Shopping Center, St. Petersburg



Easton, Maryland

District Type: Neighborhood Center

The Neighborhood Center District applies to mixed-use and non-residential developments that are primarily commercial, which serve surrounding neighborhoods. This District is typically located along collector roads and contains uses such as banks, grocery stores, restaurants and general retail. Non-residential developments with this District are considerably larger than those within the Traditional Neighborhood District and usually contain anchor tenants such as grocery stores that draw people from a broader area.



Oakhurst Plaza, Seminole

These Districts typically take the appearance of auto-oriented suburban centers, albeit on a smaller scale, with oversized parking lots that are largely absent of any pedestrian treatment (e.g., landscaping with tree canopy, textured walkways and crosswalks) and concrete block buildings that lack architectural character with no parking areas for bicycles. Examples of Neighborhood Centers in Pinellas County are LaBelle Plaza on Highland Avenue in Clearwater, the Northeast Shopping Center on 38th Avenue North in St. Petersburg and Oakhurst Plaza in Seminole.

A primary objective of the Model Code with regard to this District is to orient the design of the site so that parking is located in the rear or side of the property and the building is moved to the front to shorten the distance from the entrance to the frontage sidewalk. The Model Code calls for redevelopment projects in Neighborhood Center Districts to adhere to the development and site design standards in Sections One and Two if 50 percent or more of the total floor area of the subject property is proposed for reconstruction. Land development regulations that integrate residential and non-residential land uses within these centers is also encouraged in order to reduce trip lengths as well as the demand for automobile travel.



LaBelle Plaza, Clearwater

District Type: Neighborhood Center

Applicable Countywide Plan Map Categories:

The chart shown below is intended to identify Countywide Plan Map categories by plan classification that are typically found within the boundaries of the Neighborhood Center District. It should be noted that development projects seeking the maximum floor area ratio, units per acre and/or impervious surface ratio identified in the Neighborhood Center development standards on the following page would require a special area plan approved by the local jurisdiction, the Pinellas Planning Council and the Countywide Planning Authority.

Residential:	Residential Low Medium and Residential Medium
Mixed-Use:	Residential/Office General, Residential/Office/Retail, Resort Facilities Overlay and Resort Facilities Medium
Commercial:	Commercial General
Public/Semi-Public:	Recreation/Open Space, Institutional and Transportation Utility
Planned Redevelopment:	Mixed-Use and Commercial
Special Designation:	Community Redevelopment District

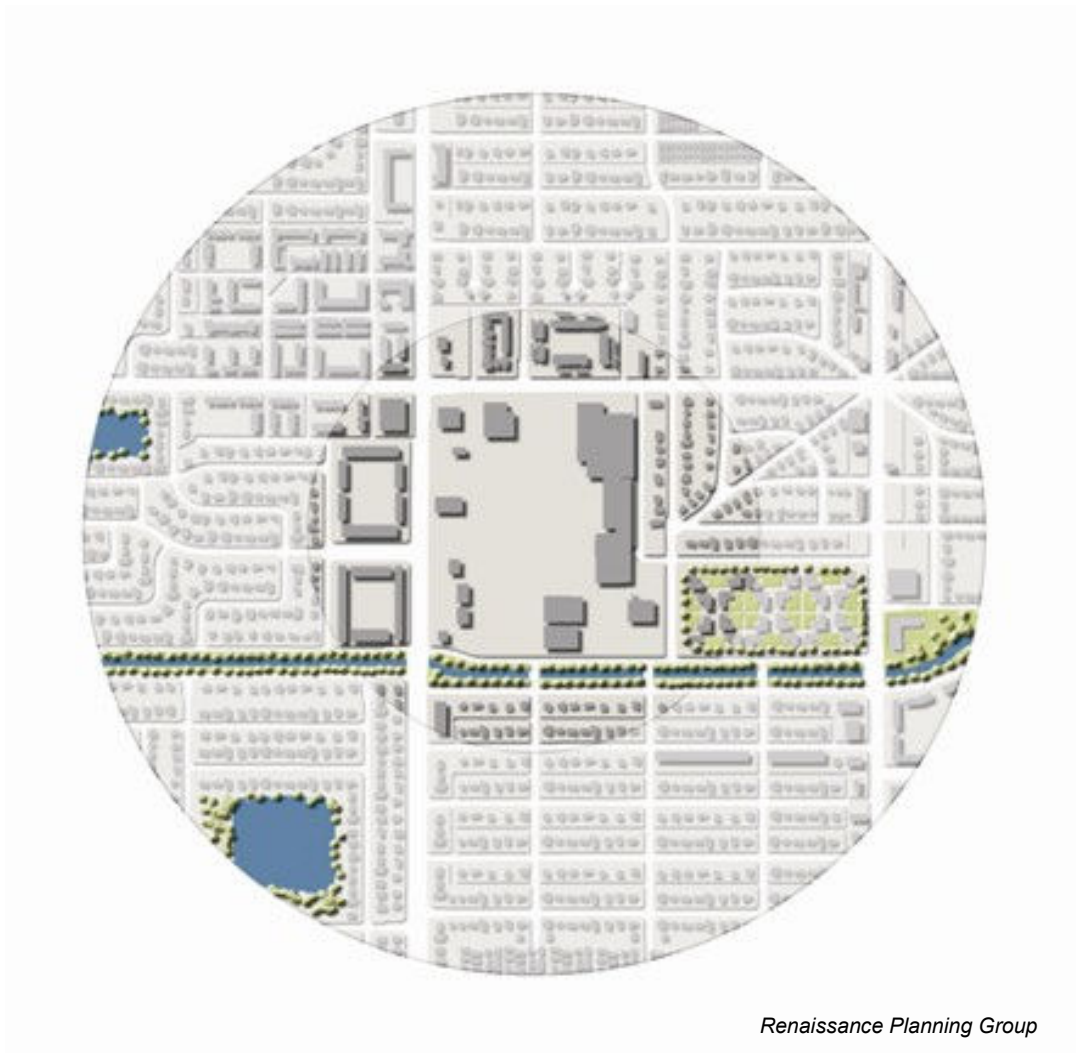
Neighborhood Center Development Standards

Block Face:	200' - 600'	
Single-Use Size Limit (ground floor):	40,000 square feet	
Floor Area Ratio:	.55 - .80	
Units Per Acre:	10 - 20	
Impervious Surface Ratio:	.80 maximum	
Ground Level Open Space:	.05 minimum total land area of site	
Build to Lines:	Residential: Front: 15' Side: 10'	Non-residential: Front: 10' Side: 10'
Percent of building oriented to build to line:	.60 minimum	
Building Fenestration/ Transparency:	.50 min. of street level building facades shall be transparent. Additional requirements included in Section Two.	
Height:	3-story maximum	
Distances between buildings:	n/a	
Maximum Floor Plate:	n/a	
Sidewalks:	6' minimum width	
Stormwater/ Drainage :	Community, underground, green roofs, pervious surfaces. On-site ponds located to the rear.	

Notes:

- 1) Single-use size limit applies only to commercial/retail land uses.
- 2) Spatial enclosure is building-to-road right-of-way (ROW) ratio (i.e., 1:2 = ROW width two times greater than building height).
- 3) Fenestration is applicable to ground floor and includes windows, doors, shutters, trim and other compatible architectural features.
- 4) Buildings must be no less than 10 feet from curb. Colonnades, galleries, and arcades may not encroach into the ROW.
- 5) Awnings and canopies which have a minimum vertical clearance of 10' are exempt from build-to line provision.
- 6) Floor area ratios and units per acre are maximums.

Suburban Center



Clearwater Mall



Rockville Center, Maryland

District Type: Suburban Center

The Suburban Center District primarily applies to large commercial centers located at intersections of heavily traveled principal and major arterial roadways. They are often major shopping hubs in the region as well as in their respective communities. Suburban Center uses include large big box retail stores (e.g., Walmart, Target, Office Depot, Toys R' Us) with outparcels occupied by restaurants and banks that are located near the frontage road(s).



Clearwater Mall

Similar to the Neighborhood Center Districts, Suburban Center Districts in Pinellas County are primarily oriented to automobile travel with large minimally landscaped parking lots devoid of bicycle and pedestrian accommodations such as bicycle racks, walkways connecting parking bays with building entrances, frontage sidewalks and adjacent bus stop pads.

The Model Code aims to improve the architectural design of suburban shopping centers, to move the parking areas to the rear or side of the buildings and to improve the landscaping and design of parking areas that front the road in order to provide a safer, more convenient and comfortable experience for pedestrians and bicyclists. The Model Code calls for redevelopment projects in Suburban Center Districts to adhere to the development and site design standards in Sections One and Two if 50 percent or more of the total floor area of the subject property is proposed for reconstruction.



Largo Mall

Mixed land uses in the Suburban Center District should also be encouraged by including requirements within commercial zoning districts that allow for integration of residential and non-residential land uses in order to reduce trip lengths as well as the demand for automobile travel. Local examples of Suburban Centers are Tyrone Square Mall in St. Petersburg, Clearwater Mall, Largo Mall and Woodlands Shopping Center in Oldsmar.

District Type: Suburban Center

Applicable Countywide Plan Map Categories:

The chart shown below is intended to identify Countywide Plan Map categories by plan classification that are typically found within the boundaries of the Suburban Center District. It should be noted that development projects seeking the maximum floor area ratio, units per acre and/or impervious surface ratio identified in the Suburban Center development standards on the following page would require a special area plan approved by the local jurisdiction, the Pinellas Planning Council and the Countywide Planning Authority.

Residential:	Residential Low Medium and Residential Medium
Mixed-Use:	Residential/Office General, Residential/Office/Retail, Resort Facilities Overlay and Resort Facilities Medium
Commercial:	Commercial General
Public/Semi-Public:	Recreation/Open Space, Institutional and Transportation/Utility
Planned Redevelopment:	Mixed-Use and Commercial
Special Designation:	Community Redevelopment District

Suburban Center Development Standards

Block Face:	300' - 600'	
Single-Use Size Limit (ground floor):	75,000 square feet	
Floor Area Ratio:	.65 maximum	
Units Per Acre:	15 maximum	
Impervious Surface Ratio:	.65 maximum	
Ground Level Open Space:	.05 minimum total land area of site	
Build to Lines:	Residential: Front: 15' Side: 10'	Non-residential: Front: 15' Side: 10'
Percent of building oriented to build to line:	.60 minimum	
Building Fenestration/ Transparency:	.50 min. of street level building facades shall be transparent. Additional requirements included in Section Two.	
Spatial Enclosure/ Height:	4-story maximum	
Distances between buildings:	n/a	
Maximum Floor Plate:	n/a	
Step Back:	n/a	
Sidewalks:	6' minimum width	
Stormwater/ Drainage:	Community, underground, green roofs, pervious surfaces. On-site ponds located to the rear.	

Notes:

- 1) Single-use size limit applies only to commercial land uses.
- 2) Spatial enclosure is building-to-road right-of-way (ROW) ratio (i.e., 1:2 = ROW width two times greater than building height).
- 3) Fenestration is applicable to ground floor and includes windows, doors, shutters, trim and other compatible architectural features.
- 4) Buildings must be no less than 10 feet from curb. Colonnades, galleries, and arcades may not encroach into the ROW.
- 5) Awnings and canopies which have a minimum vertical clearance of 10' are exempt from build-to line provision.

Section 2

Building and Site Design Standards

Urban Core and Town Center Districts

Building Design

1. Buildings shall include identifiable architectural style compatible with existing neighboring structures and recognized by design professionals as having a basis in academic architectural design philosophies. Examples of architectural styles are provided in the appendix.
2. Building materials and finishes must be consistent throughout the building.
3. The bottom of windows shall begin no higher than two feet above grade level and top of all windows and doors shall be no lower than eight feet above grade level. Taller windows are encouraged.
4. The base of the buildings, where the building meets the sidewalk(s) and entryways, should be constructed of high-quality, hardened materials.
5. Buildings shall be equally detailed on all facades visible from a street.
6. All facades for floors above the first and second floor storefronts shall have at least 30 percent total fenestration. At least two-thirds of the fenestration shall be transparent. This percentage applies to all sides of buildings.
7. A zero lot line building or buildings that have interior facades or portions thereof that cannot provide glazing due to building and fire code regulations are exempt from providing fenestration on any exempt portion of the building. Portions of these facades, which are not exempt shall have fenestration and architectural detailing consistent with the design style of the building which shall comprise at least 20 percent of the façade. Permanent, durable architectural features such as shutter, tile mosaics, medallions, Trompe L'oeil, or other items are acceptable. Examples of these features are provided in the appendix.
8. No floor of any street façade shall have a blank area greater than 36 feet in width and the height of the floor. All facades shall include fenestration and/or architectural features.

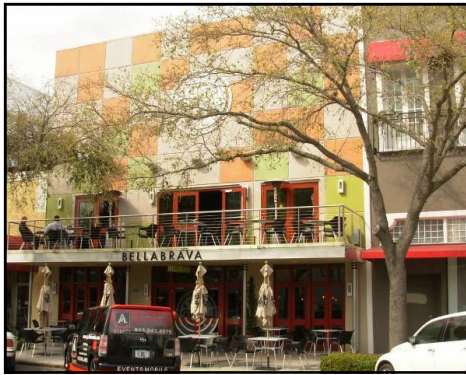


Fort Harrison Avenue, Clearwater



Downtown Oldsmar

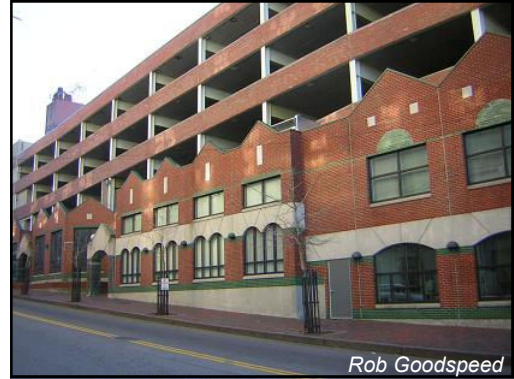
9. Window fenestration on the street facades shall be organized in a rational pattern.
10. Setbacks of new construction shall be consistent with that of existing structures on the block or with the planned redevelopment of the area.
11. On multi-story buildings, the ground level façade shall differ from upper stories through the application of two or more of the treatments listed below.
 - Variation in wall or window plane.
 - Art or decorative detailing.
 - A projecting element above the entrance such as an awning.
 - Materials or detailing.
 - Architectural elements such as flanked columns or decorative fixtures.
 - Recessed doors, archways or cased openings.



Commercial district along Central Avenue, St. Petersburg

Parking

1. Parking structures shall utilize a recognized architectural style compatible with the existing or planned architecture of the neighboring buildings.
2. Parking structures that are part of an overall project shall utilize the same architectural style, fenestration and detailing as the principal structure.
3. The ground level of all parking structures shall have non-residential uses with a minimum average depth of 20-40 feet on all public roadways, excluding vehicular entry areas.
4. Parking structures are encouraged to either encase the parking decks with liner buildings that provide for uses or an architecturally compatible design that creates an attractive façade to screen the structure from the streets.
5. Surface parking lots that are visible from the street shall provide a solid knee wall not less than 36 inches high.
6. Angled and parallel on-street parking is permitted on minor arterial, collector and local streets in accordance with the table below.



Portland, Maine

Rob Goodspeed

Degrees	Minimum Width	Minimum Depth
60	9 feet	17 feet
45	9 feet	19 feet
0 (Parallel)	9 feet	24 feet

Signs

Permitted number of signs		1
Freestanding Signs	Maximum sign area	1 square foot per linear foot of building front up to a maximum of 48 square feet
	Maximum height	15 feet
Wall Signs	Maximum sign area	1.75 square feet per linear foot of building front up to a maximum of 150 square feet
		For buildings of 4 or more stories in height, 1 additional sign shall be permitted for building identification at the top of the building. The allowable sign area shall be 3 square feet per lineal vertical foot of the building up to a maximum of 300 square feet.
Projecting Signs (Ground Floor)	Maximum sign area	4 square feet.

Freestanding signs shall be designed to compliment the architectural design of the principal building, utilizing the same materials, colors, finishes and details. In addition to color, freestanding signs shall incorporate at least one additional element (building material, architectural feature, etc.) to reflect the architectural design of the principal building.

Wall signs shall not be installed in a manner that detracts from the architectural design of a building. Wall signs shall not be installed over the surface of windows, door or other types of fenestration.

Projecting signs are permitted in addition to a freestanding or wall sign in commercial areas. A minimum 9 foot vertical clearance measured from sidewalk and minimum 3 foot horizontal clearance from edge of curb to front edge of sign is required.



Wall sign, Downtown Clearwater



Projecting sign

Public Art

All private development projects, exceeding 100,000 square feet of floor area must allocate 1% of the Aggregate Job Value (AJV) towards an on-site public art project or choose to contribute an “in lieu of” amount of a minimum 0.50% of the AJV to a public art discretionary fund. The developer is not required to spend more than \$200,000 when selecting either option (but can spend more at their own discretion). Municipalities vary in their required “in lieu of” contribution (for example the City of Clearwater requires a 0.75% contribution).



Downtown Safety Harbor

If the developer chooses the “in lieu of” option, funds are directed first towards the local jurisdiction or City public art fund in which the project is located. If no such public art fund exists within that jurisdiction, the contribution is received and utilized for general County public art projects in the amount of 0.50%. These funds will be administered through the Pinellas County Cultural Affairs Department. The intent will be to find a suitable location for the artwork on public land that is located near the development site that generated the funds.

This requirement includes both new construction and renovation projects. If renovations affect multiple structures on a project site, which may be permitted separately, the AJV is based on the construction valuation of all permits for the site.

Excluded from this requirement are the following:

- a. Projects with less than 100,000 square feet of floor area;
- b. Housing developments where at least 75 percent of the units are “affordable”; and
- c. Projects pending approval for a building permit that have submitted a complete application, prior to the local adoption of these provisions, provided that:
 1. The application is approved within six months of the date of application; and
 2. Construction begins within six months of the issuance of such approval and is diligently pursued to completion.



Program Benefits

Associated with each completed work of public art that is funded through the “in lieu of” public art fund, there will be a public recognition plaque and supporting government marketing material, that recognizes the financial support of the contributing development companies. Development companies are also encouraged to promote their contribution and acknowledge support of public art through their own in-house marketing material.

If the developer selects to complete a public artwork project on their own development property site, benefits can include: increased property identity, direct public recognition of being a supporter of the arts, potential increased media exposure and additional in-house marketing opportunities.

With effective planning, developers can integrate their public art project into a functional component of the building or site infrastructure. In this way, the developer can supplement and enhance their building project by commissioning an artist to design some component of a standard building or landscape element such as sidewalk, entryway, railing, lobby artwork, stained glass window, water feature, etc. Public art enhancements become a wise investment by improving the vitality and design quality of the local community, while also contributing to the bottom line value of an artistically enhanced development project.



Downtown Palm Harbor

Program Details

Although developers have autonomy in their selection of their public art project, as well as the artist—certain program guidelines must be followed. For example, artwork must be commissioned from a professional artist (generally defined as an artist whose primary income is derived from the sale of their artwork). Also, there are several exclusions regarding what qualifies as a public art project—(such as no business signs or company logos) and projects must be located within public view (such as from a public sidewalk) or be directly accessible to the public (such as within a public lobby).



Pinellas Trail in Gulfport

If the “in-lieu of” option is selected, this fee must be paid prior to the issuance of a building permit. If the developer selects to complete a public art project on their development site, this must be planned for and stated within their building permit application and completed before issuance of the building’s Certificate of Occupancy.

Details of how this program operates are included within *The Developer's Information Packet*. This document is available at the Clearwater Public Art/Cultural Affairs website and on the internet at http://www.myclearwater.com/gov/depts/parksrec/arts_culture/pdf/Developers_Information_Packet.pdf.

Ownership of all works of art, incorporated into private construction projects, shall be vested with the property owner who shall retain title to each work of art. If the property is sold, the seller may either include restrictions in the deed that require maintenance of the artwork and prevent its removal from the property, transfer ownership of the artwork to the local government to be maintained as a public artwork, or remove the artwork and make a contribution to a local government sponsored public art and design fund or the Public Art Trust Fund of the Pinellas County Cultural Affairs Department, in an amount equal to 0.50 - 0.75% of the initial Aggregate Job Value (amount varies according to locally adopted "in lieu of" specification).

Property owners will be required to maintain the work of art in good condition in the approved location, as required by law or other applicable guidelines, including but not limited to normal code enforcement rules, to ensure that proper maintenance is provided. The owner may request that the artwork be removed from the site, due to a hardship as defined within *The Developer's Information Packet*, with the approval of a local government public art and design program or the Pinellas County Cultural Affairs Department.



Vinoy Park, St. Petersburg

Traditional Neighborhood

Building Design

1. Buildings shall include identifiable architectural style compatible with existing neighboring structures and recognized by design professionals as having a basis in academic architectural design philosophies. Examples of architectural designs are provided in the appendix.
2. Renovations and additions shall utilize the architectural style of the existing structure or the entire existing structure shall be modified to utilize an identifiable architectural style, which is recognized by design professionals as having a basis in academic architectural design philosophies.
3. Each building shall contain a principal entry facing the primary street, which should contain a porch, portico or stoop with a minimum usable depth of 6 feet and 48 square feet of area.
4. Stoops shall not exceed three feet in height and 6 feet in width.
5. Porches in the front yard should remain open and unenclosed. Porches shall be appropriately scaled to the building and consistent with the selected architectural style.
6. Building materials shall be consistent throughout the project including roofing, siding and window materials.
7. At least two-thirds of building fenestration shall consist of window glass. Entry doors shall be counted toward fenestration if panels, decorative windows or lights are provided. Garage doors shall not count toward fenestration percentage on street facing facades. Windows must be distributed in an even pattern.
8. Windows shall be recessed within the opening three inches and feature a window-sill or architectural trim including header, sill and side trim or decorative shutters to make the window appear recessed into the wall a minimum of one inch. Windows shall not be flush mounted without trim detailing.



Old Northeast Neighborhood, St. Petersburg



Dunedin

Garages and Driveways

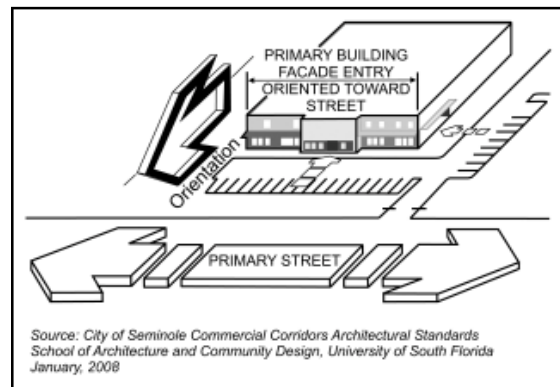
1. Access for new garages and driveways shall be designed to take advantage of the first available alternative in the priority list shown below.
 - a) Driveways and garage doors shall face the alley.
 - b) Where no alley exists, driveways and garage doors shall face the side street and shall be restricted to the rear one-third of the lot.
 - c) Where the access via the rear one-third of the lot is not possible, driveways and garage doors shall be permitted anywhere along the side street.
 - d) Where no side street is present, then a single width curb cut and driveway is allowed to the side of the principal structure where parking would occur behind the principal façade line of the residence.
2. Garages facing the primary street shall be located behind the front façade line of the principal structure by a minimum of 10 feet.
3. Garages that face the primary street shall not exceed 40 percent of the linear frontage of the façade.



Site Design

1. New multi-building development shall relate to the development of the surrounding properties. This means there shall be no internally oriented buildings which cause rear yards or rear façades to face toward abutting properties.
2. All service areas and loading docks shall be located behind the front facade line of the principal structure.
3. All principal structures shall be oriented toward the primary street. A building on a corner property may be oriented to the secondary street so long as all street façades are articulated as primary façades. Buildings at the corner of two intersecting streets are encouraged to highlight and articulate the corner of the building.
4. All mechanical equipment and utility functions (e.g. electrical conduits, meters, HVAC equipment) shall be located behind

Figure 2-1: Building Orientation



the front façade line of the principal structure. Mechanical equipment that is visible from the primary street or that is elevated more than 18 inches above grade shall be screened with material compatible with the architecture of the principal structure.

Parking

1. Parking, retention ponds and accessory structures shall be placed to the rear of the property.
2. Access to parking shall be from the street. If the primary street is utilized for vehicular access, the driveway shall serve the entire complex, not individual units, and shall not exceed one lane in each direction.
3. Access to parking shall be designed to take advantage of the first available alternative as listed below in order of priority.
 - a) Access shall be made from the alley or secondary street.
 - b) Where no alley or secondary street are present, access shall occur from the primary street.
 - c) For multi-unit structures, the driveway shall serve the entire complex, no individual units and shall not be wider than one lane in each direction.
4. Each ground floor multi-family unit or commercial unit shall contain a main entry which faces the primary street. This entry shall include decorative door surrounds, porches, porticos and/or stoops.
5. Where a single building includes separate commercial and residential entrances, the residential entrance(s) shall be raised at least 16 inches above ground-level or recessed with the façade to reinforce a privacy zone and distinguish it from the commercial entrance(s).



Fort Harrison Avenue, Clearwater

Signs

The table below applies to all developments within the Traditional Neighborhood District with the exception of subdivision entries and multi-family uses.

Freestanding Signs	Permitted number of signs	1
	Maximum sign area	1 square foot per linear foot of building front up to a maximum of 48 square feet
	Maximum height	10 feet
Wall Signs	Maximum sign area	1.75 square feet per linear foot of building front up to a maximum of 48 square feet.
Projecting Signs (Ground Floor)	Maximum sign area	4 square feet

The table below applies to subdivision entries and multi-family uses.

Freestanding Signs	Permitted number of signs	Up to 2 single-faced signs per subdivision entrance, one on each side of the entry if the subdivision/development is located on both sides of the entry Or 1 double-faced sign
	Maximum sign area	24 square feet per sign face; for properties with 100 or more feet of frontage, an additional 12 square feet per sign face shall be permitted for every additional 50 feet of frontage up to a maximum of 72 square feet.
	Maximum height	10 feet
Wall Signs	Maximum sign area	1 wall sign may be substituted for 1 permitted freestanding sign. Sign area shall be the same as would be permitted for the freestanding sign.

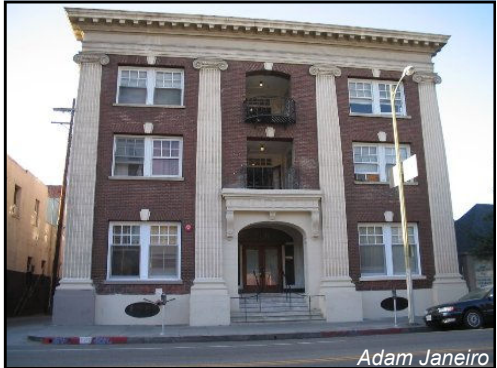


Freestanding signs shall be designed to compliment the architectural design of the principal building, utilizing the same materials, colors, finishes and details. In addition to color, freestanding signs shall incorporate at least one additional element (building material, architectural feature, etc.) to reflect the architectural design of the principal building.

Wall signs shall not be installed in a manner that detracts from the architectural design of a building. Wall signs shall not be installed over the surface of windows, door or other types of fenestration.

Projecting signs are permitted in addition to freestanding or wall signs in commercial areas. A minimum 9 foot vertical clearance measured from sidewalk and minimum 3 foot horizontal clearance from edge of curb to front edge of sign is required.

Neighborhood and Suburban Center

Building Design

1. Commercial buildings should have a width to height ratio of no more than 3:1. Buildings that exceed the width to height ratio of 3:1 shall have architectural fenestration creating a bay system that divides the building design into a maximum ratio of 3:1. This may be done through pilasters, arcades, building line and roof line off-sets, materials and other appropriate architectural features.
- 
- Adam Janeiro
- Building with pilasters, Los Angeles
2. Residential buildings should have a width to height ratio of no more than 2:1. Buildings that exceed the width to height ratio of 2:1 shall have architectural fenestration creating a bay system that divides the building design into a maximum ratio of 2:1.
 3. The first floor of each multi-story building shall be at least 12 feet in height measured to the bottom of the second floor.
 4. On multi-story buildings, the ground level façade shall differ from the upper story(ies) through the application of two or more of the treatments listed below.
 - Variation in wall or window plane.
 - Art or decorative detailing.
 - A projecting element above the entrance such as an awning.
 - Materials or detailing.
 - Architectural elements such as flanked columns or decorative fixtures.
 - Recessed doors, archways or cased openings.
 5. Structures that are situated on corner lots, through lots, or by the nature of the site layout are clearly visible from rights-of-way, shall be designed with full architectural treatment on all sides visible from public rights-of-way. Full architectural treatment shall include roof design, wall materials, and architectural trim, and door and window openings. While it is recognized that buildings have primary and secondary facades, the construction materials and detailing should be similar throughout.
- 
- West Park Village, Tampa
- 
- Downtown Oldsmar

6. There shall be no blank facades. All facades shall include fenestration, architectural features, or both. For multi-story buildings, no portion of a facade corresponding to the height between two floors shall have a blank area greater than 24 feet in width.
7. At least 50 percent of street level facades of non-residential buildings abutting streets shall be transparent. The bottom of windows shall begin no higher than three (3) feet above grade level, and the top of all windows and doors shall be no lower than eight (8) feet above grade level. Taller windows are encouraged.
8. Windows on the street side façades shall be evenly distributed in a consistent pattern.
9. At least 20 percent of street side facades of residential buildings shall be transparent and at least 15 percent of all other facades shall be transparent.
10. Buildings shall provide a pitched roof or a flat roof with a decorative parapet wall compatible with the architectural style of the building.



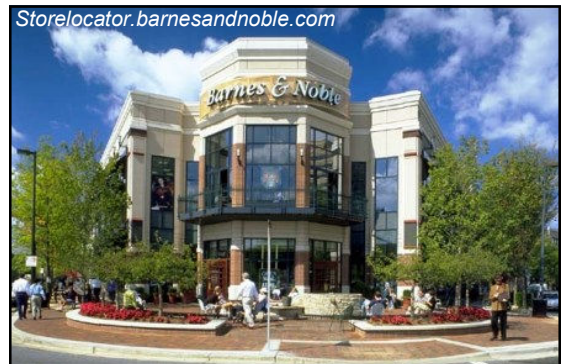
Corey Avenue, St. Pete Beach



Oldsmar Public Library

Site Design

1. All principal structures shall be oriented toward the primary street. The first floor of big box buildings shall be edged with a use liner containing any permitted use (e.g. retail, restaurant, residential) or the entire wall shall include architectural details such as fenestration, large display windows, natural finishes and other architectural features.
2. Redevelopment projects shall adhere to the development standards described in Section One and the site design standards described in this Section if 50 percent or more of the total floor area of the subject property is proposed for reconstruction.
3. New multi-building development shall relate to the development of the surrounding properties. This means there shall be no internally oriented buildings which cause rear yards or rear façades to face toward abutting properties.



Bethesda, Maryland

4. All service areas and loading docks shall be located behind the front facade line of the principal structure.
5. All mechanical equipment and utility functions (e.g. electrical conduits, meters, HVAC equipment) shall be located behind the front facade line of the principal structure. Mechanical equipment that is visible from the primary street or that is elevated more than 18 inches above grade shall be screened with material compatible with the architecture of the principal structure.
6. If the development proposes to locate outparcels or liner buildings (e.g., retail, office uses) to the front of the property, parking lots may be located between outparcels and principal buildings providing applicable requirements for surface parking lots described in this section are met.
7. Parking structures are permitted internal to the site with architectural features consistent with the existing or planned architectural characteristics of the surrounding buildings.
8. Where multiple store fronts or multiple buildings exist within the same development, each store front and building shall be connected by an internal sidewalk system that is clearly delineated from the vehicular pavement. The internal sidewalk system shall connect to any public sidewalk that abuts the property.
9. Each ground floor multi-family dwelling unit or commercial unit that faces a primary street shall contain a primary entry, which faces the primary street. The primary entry shall include decorative door surrounds, porches, porticos or stoops or a combination thereof. Rear entries shall also be provided on buildings within developments where parking is located behind the building.

Figure 2-2: Site Design with Outparcels



South parking lot, Pinellas County Courthouse

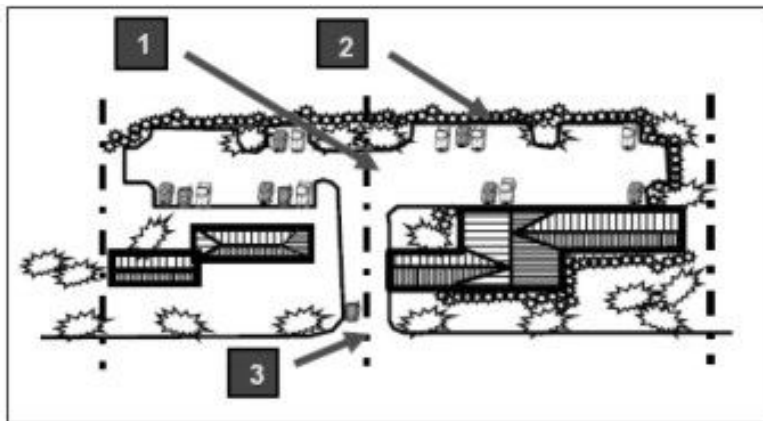


10. Where a single building includes separate commercial and residential entrances, the residential entrance(s) shall be raised at least 16 inches above ground-level or recessed within the facade to reinforce a privacy zone and distinguish it from the commercial entrance(s).
11. All development projects shall be required to improve and integrate bus stops located on the adjacent right-of-way. Improvements shall include, at a minimum, a concrete pad for the installation of a bus shelter and sidewalk connecting the pad to the existing sidewalk network.

Connectivity

1. Cross easements which connect the internal pedestrian system between abutting property owners are required.
2. Development projects that exceed 15 acres shall be required to infill a local street network with block lengths as specified in Section One. Cul-de-sacs and dead end streets are discouraged with the exception of “stub-out” streets designed to connect to a future development on an adjacent property. This requirement applies to new development and redevelopment projects whereby 50 percent of the total floor area on the subject property is proposed for reconstruction.

Figure 2-3: Shared Parking and Driveway



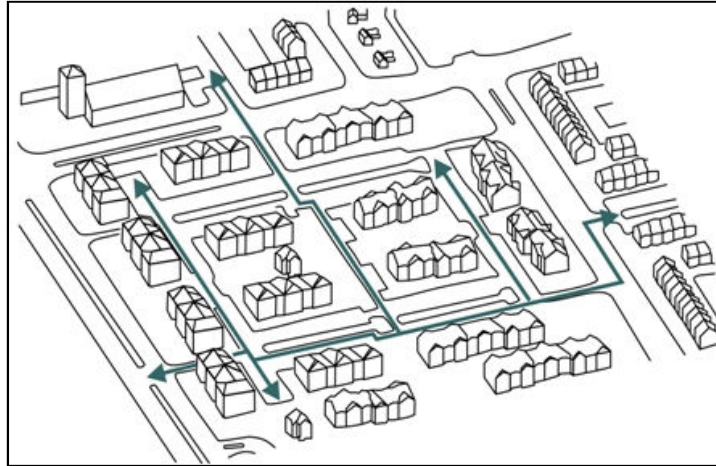
1. Shared parking lot is located completely to the rear of the site behind the buildings.
2. The parking lot is adequately buffered with landscaping.
3. A single, shared driveway serves two sites.

Clearwater Downtown Redevelopment Plan, February, 2004

3. Cross-access must be provided between adjacent parcels to minimize the number of driveways and to improve local connectivity.
4. Developing parcels shall incorporate internal streets and access ways into the site design that stub at the boundary with adjacent parcels, and that provide cross-access upon development or redevelopment of the adjacent parcels.

5. For developing parcels adjacent to a parcel with a previously stubbed or approved development plan indicating a stubbed street or access way, the developing parcel shall incorporate the continuation of the street or access way into the site design.

Figure 2-4: Inter-connectivity Between Parcels



Cary Design Guidelines Manual, Frazier Associates and Strategic Land Planning, 2001.

Parking

	Vehicle Parking Maximum		
Use	Transit Access ¹	No Transit Access	Bicycle Parking (Bike Racks) ³ Minimum
1. Residential:			
(a) Detached single-family	N/A	N/A	N/A
(b) Attached single-family and multi-family (includes condominium and town-home)	2.5 spaces per unit	2.5 spaces per unit	If no garage, 1 rack per 2 units
2. Non-Residential:			
(a) Retail	2.5 spaces per gross 1,000 s.f. ²	5 spaces per gross 1,000 s.f.	2 racks per building, or 1 per gross 8,000 s.f., whichever is greater
(b) Office	3 spaces per gross 1,000 s.f.	4 spaces per gross 1,000 s.f.	2 racks per building, or 1 per gross 10,000 s.f., whichever is greater
(c) Transient Accommodation	1.5 spaces per guest room	1.5 spaces per guest room	2 racks per building, or 1 per every 5 rooms, whichever is greater
(d) Restaurants/ Entertainment Venues	1 space per 4 seats	1 space per 4 seats	2 racks per building, or 1 space per 15 seats, whichever is greater

Notes

¹Sites with transit access include those with bus stop or terminal within ¼ mile where bus service with peak hour headways of 15 minutes or less is provided.

²Exceptions to this standard for retail uses more dependent on vehicle travel such as hardware/lumber, pet and appliance stores, as established by local governments, may be permitted.

³Racks must provide a minimum of four bicycle parking spaces.

- 1) A minimum of two vehicle parking spaces should be set aside and striped for six two-wheeled motor vehicles with corresponding signage.
- 2) Parking standards set forth in this table may be implemented either as site plan requirements or as an incentive based approach. The latter would allow development projects implementing the standards to be eligible for bonus provisions such as reduced open space requirements, reduced permit fees, expedited permit approval and/or density/intensity bonuses.

Signs

Freestanding Signs	Permitted number of signs	1 for signs greater than 8 feet tall 2 if signs are 8 feet in height or less and with a minimum of 100 feet in frontage
	Maximum sign area	64 square feet per sign face
Wall Signs	Maximum height	15 feet
	Maximum sign area	1.75 square feet per linear foot of building front up to a maximum of 150 square feet. For buildings of four or more stories in height, one additional sign shall be permitted for building identification at the top of the building. The allowable sign area shall be 3 square feet per lineal vertical foot of the building up to a maximum of 300 square feet.
Projecting Signs (Ground Floor)	Maximum sign area	4 square feet

Freestanding signs shall be designed to compliment the architectural design of the principal building, utilizing the same materials, colors, finishes and details. In addition to color, freestanding signs shall incorporate at least one additional element (building material, architectural feature, etc.) to reflect the architectural design of the principal building.



Wall signs shall not be installed in a manner that detracts from the architectural design of a building. Wall signs shall not be installed over the surface of windows, door or other types of fenestration.

Village Plaza at Westchase,
Tampa

Projecting signs are permitted in addition to freestanding or wall signs. A minimum 9 foot vertical clearance measured from sidewalk and minimum 3 foot horizontal clearance from edge of curb to front edge of sign is required.

Standards Applicable to Non-residential and Mixed-uses in all Districts

Surface Parking

For surface parking lots that are over one acre, the following requirements apply:

- a) No more than a double row of parking spaces with a single drive lane shall be allowed between the principal building and the primary street;
- b) Pedestrian walkways with textured surfaces are required to connect all parking bays, landscape islands, building entrances and adjacent frontage sidewalk(s);
- c) All walkways must adhere to ADA standards;
- d) A landscape island must be provided between parking bays;
- e) One canopy tree shall be provided for every 2,000 square feet of parking area, including the drive aisles;
- f) Landscape islands must accommodate pedestrian movement with a walkway connecting the parking bays to the building entrances and to the frontage sidewalk(s);
- g) Parking shall be divided into rows with no more than 8 spaces per row. Each row of parking shall be divided by a minimum 245 square foot landscape island with a minimum 6 foot width;
- h) All landscape vegetation must be native and drought tolerant and all trees must provide canopy; and
- i) Specifications for landscape and tree installation must adhere to applicable local landscape management codes.



Citrus Park Mall, Tampa

An illustration of a large parking area with these features is shown on the following page.

Large Parking Lot Design



Shared Parking

Shared parking requirements may be determined by calculating the total number of maximum spaces required for each use and dividing the total by the sharing factor in the table below.

Figure 2-6: Shared Parking Standards

Function	with		Function
RESIDENTIAL			RESIDENTIAL
LODGING			LODGING
OFFICE		1	OFFICE
RETAIL	1.4	1.1	RETAIL
	1.2	1.7	
	1.3	1	
	1.2	1.2	
	1		

Smartcode shared parking standards, Duany, Plater-Zyberk.

As an example, a mixed use building consists of 2,000 square feet of retail and 10 dwelling units. Individually, the retail use would have to provide 10 spaces, while the residential use would have to provide 25 spaces, for a total of 35. Using the shared parking standards, the maximum space requirement would be 29 (35/1.2). It should be noted that the table in Figure 2-6 is intended by the authors to be applied to minimum parking requirements rather than maximum. It is being applied to maximum requirements in this case since the Model Code is prescribing the use of maximum standards.

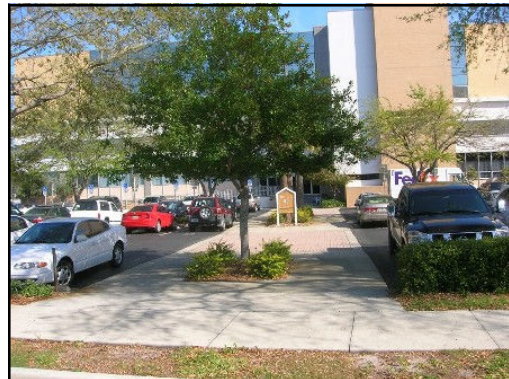
Sidewalks

Provisions listed in this section must be in compliance with requirements identified in Section 1 and local site plan review processes.

1. Development projects shall provide sidewalks along frontage roads within the public right-of-way. Sidewalks may be constructed within private property if not feasible to construct within the right-of-way due to limited width, location of utilities, etc.
2. Development projects shall provide shade for frontage sidewalks through the installation of native trees, arcades, colonnades or galleries. Trees should be planted on the side of the sidewalk closest to the development.
3. A minimum of six feet of unobstructed sidewalk width and a minimum vertical clearance of ten feet should be provided for branches overhanging a sidewalk and a minimum vertical clearance of 15 feet should be provided for branches overhanging a street. Trees shall not be planted within the sight triangle, 12 feet of a fire hydrant or access-way apron or in any location that impedes a driver's line of sight to other vehicles, signs or signals.
4. If an existing or planned transit stop is located along a road adjacent to the development project, a sidewalk shall be provided to connect the stop with the sidewalk fronting the property.
5. A two foot minimum permeable surface buffer should be provided between the street pavement edge or curb and the sidewalk edge.



Fort Harrison Avenue/Turner Street,
Clearwater



South parking lot, Pinellas County Court-
house

6. Pedestrian scaled lighting no higher than 16 feet shall be placed in appropriate areas along the public sidewalk in the buffer area or on the edge of the sidewalk nearest to the street at a spacing of no greater than 80 feet. Lighting shall be located as close as possible to the center point between adjacent street trees to avoid interference by the tree canopy.

Open Space

All developments over 10 acres shall provide public open and/or civic space. Open space can include, but is not limited to, pocket parks, public squares, greenways, trails, sports fields, plazas and amphitheaters. Public art is also encouraged in these areas to add visual interest and to create a sense of place.

Stormwater Attenuation

To reduce the size of stormwater retention and detention ponds, development projects may utilize alternative retention options such as rain gardens, pervious pavers and green roofs that are consistent with local and Southwest Florida Water Management District requirements.



Downtown Dunedin

Section 3

Optional Standards

Off-site Sidewalk

Development projects may construct off-site sidewalk to fill a gap in the existing network.

Pedestrian Crosswalks

Pedestrian crosswalks may be provided at proximate intersections at mid block where blocks are longer than 500 feet. Pedestrian crosswalks shall be marked with a contrasting pavement color, texture and/or reflective materials, and shall provide ADA accessibility.

Transit Improvements

1. *Bus Access.* Development projects may construct pullout bays to allow bus access onto properties where warranted by existing or anticipated ridership demand or, for Suburban Center developments, require on-site access to service passengers. Approval of pullout bays associated with development projects would be subject to approval by the Pinellas Suncoast Transit Authority.



Corey Avenue, Downtown St. Pete Beach

2. *Transit Stop Connection.* Development projects may provide a sidewalk connection between the on-site sidewalk and the off-site sidewalk that leads to the transit stop. This would also include a sidewalk connecting the bus pad with the public sidewalk.
3. *Enhanced Transit Stops/Shelters.* This would involve the installation of an enhanced transit stop that would include a passenger shelter with a roof that is a minimum of seven feet high and five feet wide, a bench that accommodates a minimum of eight people and an area to accommodate one wheelchair and a trash receptacle, and outdoor lighting for safe pedestrian movement at night.



Tyrone Square Mall, St. Petersburg

An enhanced transit stop/shelter may also include one or more of the following: a location/information sign that, at a minimum, shows the location of the transit stop on a system-wide map, a bike rack that accommodates at least three bikes and a decorative shelter design.

For some of the larger developments, a passenger information booth and restroom facilities could be provided with the requirement that the developer would supply electricity for lighting and real-time information markers. The developer would also need to provide water service for restrooms, drinking fountains, and cleaning of the shelters.

Note: Transit stop improvements must be designed to accommodate all routes and buses that use the stop throughout the day. (Ex: Largo Mall has to accommodate up to 5 buses at one time, because of the number of routes and buses that service that area). All transit areas must meet ADA guidelines.

Bicycle Facilities

In addition to the bicycle parking provisions which would be required of non-residential and some multi-family projects, development projects may also provide additional improvements to encourage bicycle use for commuting purposes. These include covered parking facilities and on-site shower facilities.



Ride Sharing/Vanpooling

Proposed developments involving companies with over 50,000 gross square feet of floor space may participate in Bay Area Commuter Services (BACS)-sponsored or other alternate commute programs designed to encourage their employees to commute to the workplace by ride-sharing or in a BACS vanpool. As an alternative, companies may establish their own commute alternative program.

To encourage modes of transportation other than the single-occupant vehicle, carpool and/or vanpool parking places could be provided on site for nonresidential uses with over 50,000 gross square feet of floor space per the following standards:

1. One percent of the minimum number of parking spaces required shall be designated for carpool / vanpool parking;
2. Carpool/vanpool parking spaces shall be located as close as practical to the main entrance(s) of the building(s), without displacing parking provided for use by the disabled; and



3. Carpool/vanpool parking spaces shall be clearly designated for exclusive use by carpool and vanpool vehicles between specified times. The spaces shall be identified or designated as such through the use of signage or pavement markings.

Green Building

Leadership in Energy and Environmental Design (LEED) or similar certification as it pertains to the reduction of single-occupant vehicle trips, vehicle miles traveled and the reduction of a project's carbon footprint would allow it to earn impact fee credits and/or to meet concurrency requirements. The project must be registered and certified with the U.S. Green Building Council. The minimum number of points for LEED certification must be satisfactorily achieved as documented by the applicant.

Appendix

Glossary

ACCESS-WAY APRON is an extension of a driveway lying between the public sidewalk (or the right-of-way line if there is no sidewalk) and the curb and gutter of a street that provides a way to enter and exit a property.

ARCADE is an arch covered passageway usually with shops on either side.

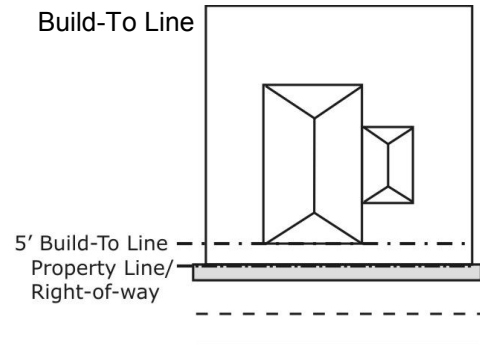
ARTERIAL ROADS provide service which is relatively continuous and of relatively high traffic volume, long trip length, and high operating speed. The Federal Highway Administration further defines these roads as “principal” and “minor” arterials.



Burlington Arcade, London (Wikimedia.org)

BLOCK FACE is the total frontage, measured in linear feet, of lots on the same side of the street between the nearest intersecting streets.

BUILD-TO LINE is a zoning requirement calling for buildings to push up closer to the street. By placing the building entrance closer to the street, this is intended to create a more pedestrian and transit friendly environment.



COLLECTOR ROADS provide service of relatively moderate traffic volume, trip length, and operating speed. Collector roads collect and distribute traffic between local and arterial roads.

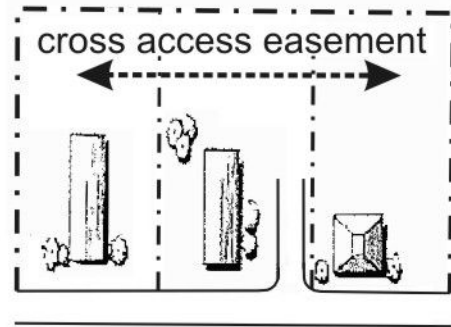
COLONNADE is a series of regularly spaced columns supporting an entablature and usually one side of a roof. A colonnade may also be a series of trees planted in a long row on either side of a driveway or walkway.

CONCURRENCY MANAGEMENT SYSTEM means the procedures and/or processes that local governments utilize to ensure that development orders and permits are not issued unless the facilities and services needed to accommodate the impacts of the development are in place.

COUNTYWIDE PLAN RULES apply to the implementation and administration of the Countywide Future Land Use Plan and Countywide Plan map. A primary function of the Countywide Plan Rules is to ensure consistency between local government comprehensive plans and regulations and the Countywide Future Land Use Plan and Countywide Plan map.

COUNTYWIDE PLANNING AUTHORITY (CPA) is the Pinellas County Board of County Commissioners serving in their role as the decision-making authority regarding the adoption and administration of the Countywide Plan.

CROSS ACCESS EASEMENT is an access route allowing traffic to pass through one property to another as granted by the affected property owners. It is typically used on commercial properties that share a driveway entrance or that are within large commercial or industrial complexes.



DOOR SURROUNDS are decorative features or molding that enclose a door frame.

FAÇADE is the principal face of a building.

FENESTRATION refers to exterior openings in a building, such as windows, doors and skylights, that allow for light, vehicles or people to pass through.

FLOOR AREA RATIO (FAR) is the total floor area of a site divided by its land area. Maximum FAR is established in local zoning codes to establish allowable density. For example, on a 10,000 square-foot zoning lot in a district with a maximum FAR of 1.0, the floor area of a building cannot exceed 10,000 square feet.

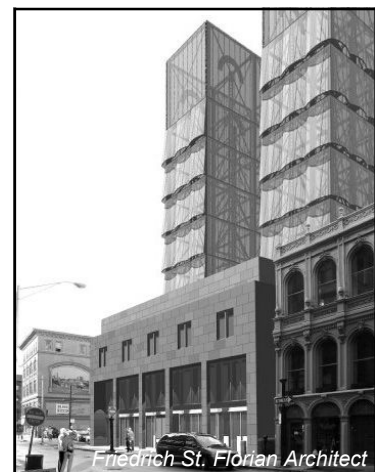
GALLERY is a narrow balcony usually having a railing or balustrade extending along the outer wall of a building supported by arches or columns on the outer side. It may also be a long enclosed hallway or a projecting or recessed passageway along an upper story on the interior or exterior of a large building.

IMPERVIOUS SURFACE RATIO (ISR) equals the total area of impervious surface on a site divided by its net area (excluding right-of-way). Impervious surface includes hard-surfaced, man-made area that does not readily absorb or retain water, including but not limited to building roofs, parking areas, driveways and sidewalks.

LEADERSHIP IN ENERGY AND ENVIRONMENTAL DESIGN (LEED) is a green building rating and certification program of the U.S. Green Building Council designed to create residential and commercial buildings that are environmentally healthy.

LINER USE (or building) is intended to hide an area such as a parking lot or garage from the street. Liner uses are usually residential or retail buildings with a depth of less than 50 feet.

MINOR ARTERIAL is a road that accommodates higher-speed, longer-length trips, and serves one of seven significant trip purposes listed under federal guidelines.



Liner use building

MIXED-USE DEVELOPMENT generally occurs as one of the following types:

1) Mixed-Use Projects

Mixed-use projects combine single-use buildings, typically on distinct parcels, in a range of land uses in one planned development project, although combining both vertical and horizontal mix of uses can also occur. The overall project must result in a walkable area and there must be significant physical and functional integration of the different uses. These projects may occur within a mixed-use walkable area or they may be located in a more suburban setting.

2) Mixed-Use Areas – Larger Scale

Areas where there is a mixing of different land uses – e.g. residential, shopping, eating establishments, employment, lodging, civic, cultural – in one relatively discrete area. These areas usually have multiple ownerships, and combine both a vertical and horizontal mix of uses where there is significant physical and functional integration of the different uses, resulting in a walkable area. Urban centers and town centers usually exhibit these characteristics; examples in Pinellas County include the downtowns of St. Petersburg, Dunedin, Palm Harbor, Clearwater and Largo. A larger-scale mixed-use area may or may not include a mixed-use project.

3) Mixed-Use Areas- Neighborhood Scale

Areas where there is a mixing of land uses at a neighborhood scale in one relatively discrete area. The range of uses is generally more restricted focusing on neighborhood retail and services, eating establishments, and residential. These areas may be under one ownership or have multiple owners, and often emphasize a horizontal mix of uses, although some vertical mix may occur. There is significant physical and functional integration of the different uses that result in a walkable area. The collection of retail, service, office, and civic uses in Ozone in north Pinellas County exemplifies these characteristics. A neighborhood-scale mixed-use area may or may not include a mixed-use project.

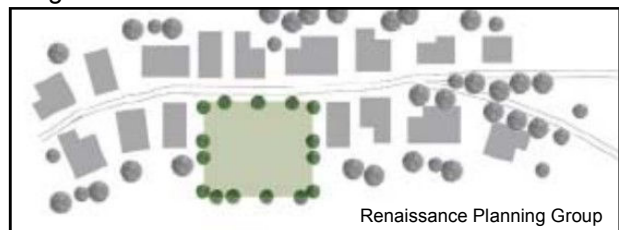


Medallion on exterior of building.

MEDALLIONS are oval or circular designs resembling medals that may be used as an architectural feature on buildings.

NEIGHBORHOOD PARK is an open public space serving a surrounding residential area. It may be used for civic gatherings and recreation. Neighborhood parks provide a safe area free from moving traffic for children and local residents. It may be bound by residences or small-scale institutional or civic buildings to form a com-

Neighborhood Park



mon green. These parks are intended to serve the local area, unlike recreational parks which serve a larger residential population.

OPEN SPACE is that part of a site, which may include courts or yards, that is open and unobstructed, except for specific permitted obstructions (e.g., artwork, pavilion, etc.), for use by all persons occupying dwelling units or utilizing floor space on the site. Open space may also include outdoor theaters, neighborhood parks, recreational parks, urban parks, plazas, pocket parks and off-road trails.

OPEN SPACE RATIO (OSR) is the amount of open space required on a site expressed as a percentage of the total floor area on the site. For example, if a building with 20,000 square feet of floor area has an OSR of .20, 4,000 square feet of open space would be required on the site ($0.20 \times 20,000$).

PARAPET is a architectural feature that projects from the edge of a platform, terrace, or roof.

PERCENT OF BUILDING ORIENTED TO BUILD-TO LINE is a zoning regulation intended to position sufficient building frontage toward the front of the property to create a more pedestrian and transit friendly environment.



Parapet roofline on Oldsmar Public Library

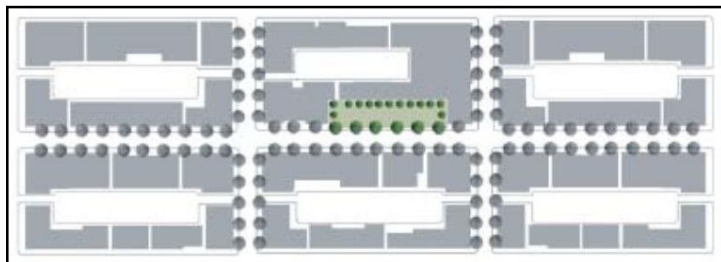
PINELLAS BY DESIGN is intended to provide the tools necessary to shape the redevelopment of Pinellas County in accordance with the publication, *Pinellas by Design: An Economic Development & Redevelopment Plan for the Pinellas Community*.

PINELLAS COUNTY CULTURAL AFFAIRS DEPARTMENT is a Pinellas County Government agency responsible for integrating arts and culture into the community and to heighten awareness of the civic value of culture through such vehicles as arts education, economic development activity and the promotion of cultural tourism.

PINELLAS PLANNING COUNCIL (PPC) is a countywide planning agency established under a special act of the Florida Legislature (Chapter 88-464, F.S.) responsible for countywide land use planning and for administering and implementing the Countywide Plan.

POCKET PARK is a small park that occupies a “left over” space between buildings. Typically, no longer than 100 feet of frontage, pocket parks provide vegetation, shade and open space within densely populated built areas. Due to their small scale, pocket

Pocket Park



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parks predominantly serve immediately adjacent buildings and thoroughfares.

PORTICO is a porch or walkway supported by columns usually leading to the entrance of a building.



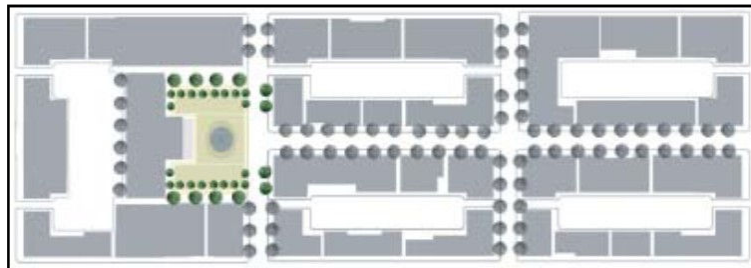
Portico

PRINCIPAL ARTERIAL - A road which accommodates higher-speed, longer-length trips, and serves at least two of seven significant trip purposes listed in federal guidelines.

PUBLIC ART TRUST FUND supports public art projects in the County and is administered by the Pinellas County Cultural Affairs Department.

PUBLIC PLAZA is an open area adjacent to a building and accessible to the public. It is generally less than half the size of a block and located at the intersection of important thoroughfares. It is devoted to civic uses and commercial activity and surrounded by buildings on all sides. Its landscape is composed primarily of durable pavement and formally planted trees. Features such as

Public Plaza

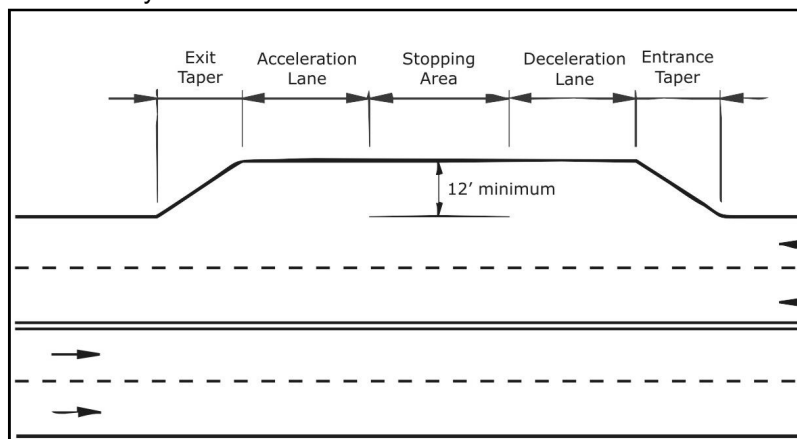


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such as fountains, statues and other vertical elements help mark the civic prominence of the plaza. These architectural features are most successful when planned in accordance with a strong visual axis, allowing the plaza to be read from a distance.

PULL-OUT BAY is a paved area adjacent to major roads that provides for buses to exit the stream of traffic to pick up and drop off passengers.

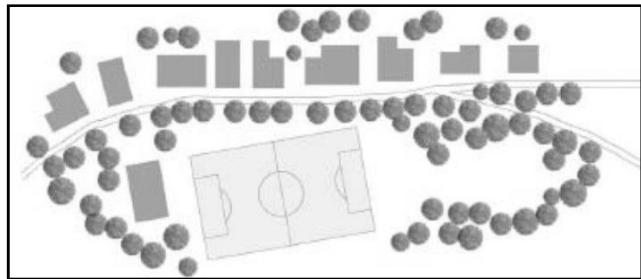
Pull-out Bay



FDOT BD 549-34 Final Report, National Center for Transit Research, Center for Urban Transportation Research, University of South Florida, November 2007, p. 42.

RECREATIONAL PARK is open public space ranging from three to 10 acres, reserved for civic gatherings as well as recreation. Recreational parks are often designed around existing natural features. Its landscape consists primarily of grassy areas, paved or unpaved walkways and shade trees. Formal playing fields may be established to service community needs.

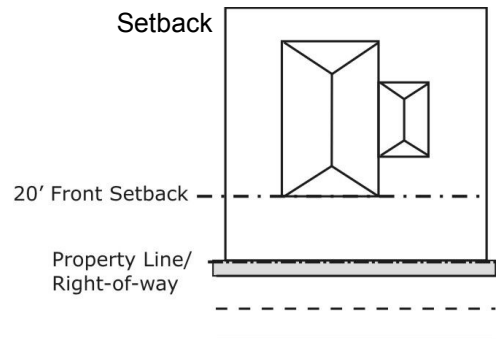
Recreational Park



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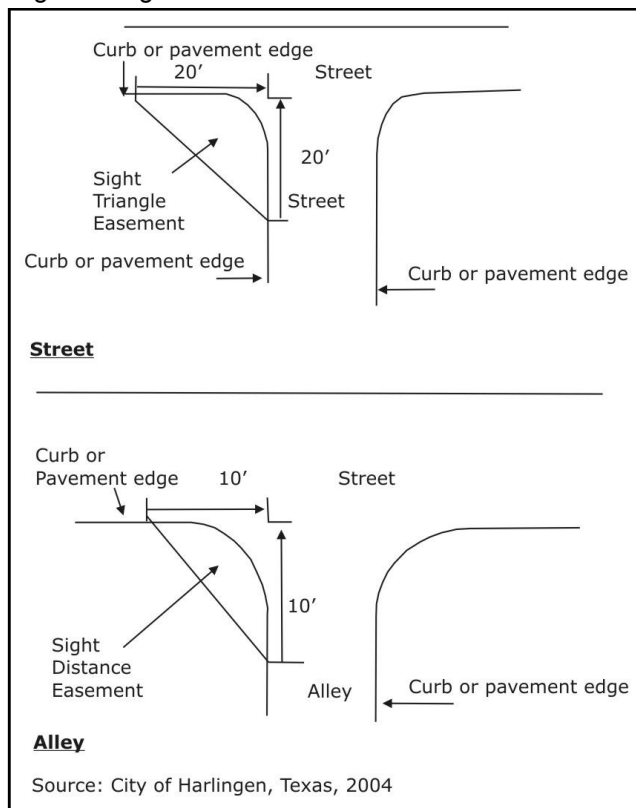
SETBACK is the horizontal distance between a structure and another structure, a property line, a right-of-way line, a body of water or other specific point.

Setback



SIGHT TRIANGLE is triangular-shaped portion of land established at street intersections where there are restrictions on the installation of signs, monuments, trees, bus shelters, etc., that limit or obstruct the sight distance of motorists entering or leaving the intersection.

Sight Triangle

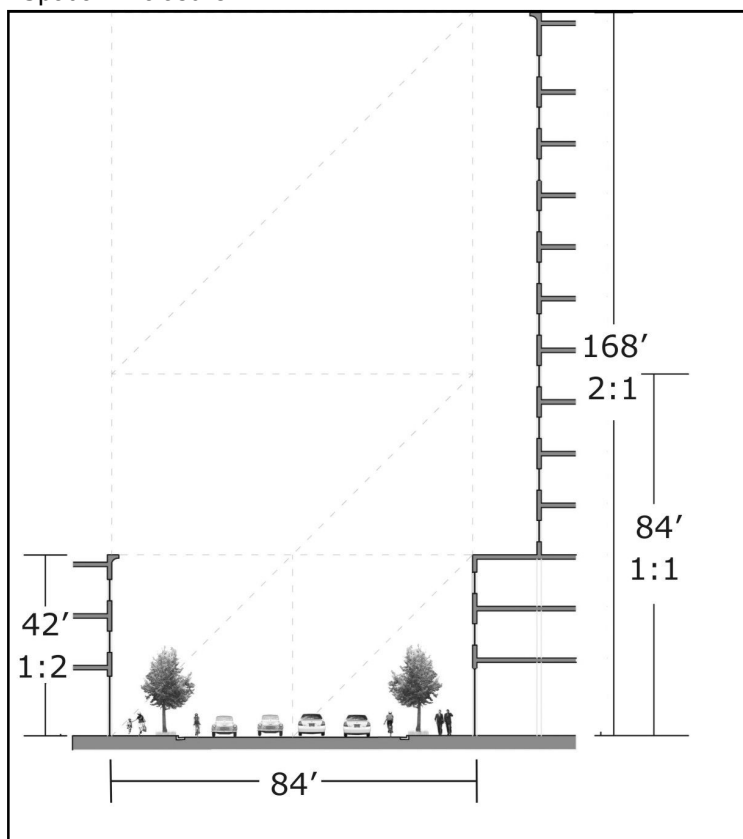


SPATIAL ENCLOSURE in the design of urban streets is an expression of building height to street width. Spatial enclosure is further described below by Architectural Graphic Standards, Seventh Edition (Ramsey and Sleeper).

As a general rule, the tighter the ratio, the stronger the sense of place and often, the higher the real estate value. Spatial enclosure is particularly important for shopping streets that must compete with shopping malls, which provide very effective spatial definition. In the absence of spatial definition by facades, disciplined tree planting is an alternative. Trees aligned for spatial enclosure are necessary on thoroughfares that have substantial front yards.

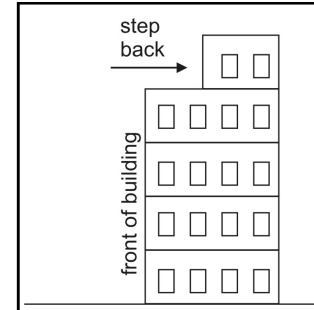
As shown in the illustration, with 84 feet of public right-of-way at the street level, the width to height ratio on a 42, 84 and 168 foot tall building would be 2:1, 1:1 and 1:2, respectively. A 1:6 ratio is the absolute minimum, with 1:3 being an effective minimum if a sense of enclosure is to result (Ramsey and Sleeper). This sense of enclosure is another key element of a livable pedestrian and transit friendly environment.

Spatial Enclosure



SPECIAL AREA PLAN requires an amendment to the Countywide Plan Map to Residential Very High, Activity Center, Community Redevelopment District, Central Business District, or the Planned Redevelopment categories of Residential, Mixed Use, Commercial, or Industrial. Special area plans shall be approved by official action of the legislative body of the local government in support of the proposed category, in a form sufficient to ensure compliance with the special area plan (Countywide Plan Rule 4.2.7.5.1).

STEP BACK a zoning regulation requiring the upper story(ies) of a building to set back from its front edge. The intention of this regulation is to reduce the canyon effect that can be felt by people walking past the building.



STOOP is a small porch, platform, or staircase leading to the entrance of a house or building.

STUB OUT refers to a street or pedestrian facility terminating at the property line of a neighboring site for the purpose of creating a future connection.

TILE MOSAIC is a picture or decorative design made up of small colored pieces such as stone or tile.



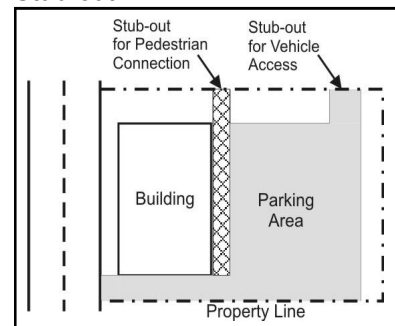
Stoop

TRANSPORTATION IMPACT FEE ORDINANCE is applied countywide by Pinellas County local governments to exact fees from development projects adding trips to the surrounding traffic circulation system based on their transportation impacts. The funds collected are utilized to fund transportation improvements necessary to provide for capacity needs resulting from traffic growth.

TROMPE L'OEIL a style of painting that gives the illusion of reality.

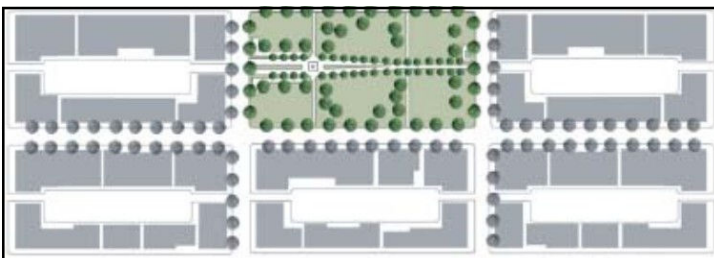
U.S. GREEN BUILDING COUNCIL (USGBC) developed the LEED rating system. They are a non-profit trade organization focused on promoting sustainable buildings in terms of their construction, design and operation.

Stub-out



URBAN PARK occupies at least a full downtown block. Its landscape consists of lawns, paved walkways and shade trees. Fountains and statues are often found in urban parks. Landscape elements can help to organize the park into a series of smaller spaces that offer diverse qualities and uses. Urban parks may be surrounded by civic buildings and residential uses. In certain instances, civic buildings can accompany the park on a shared block.

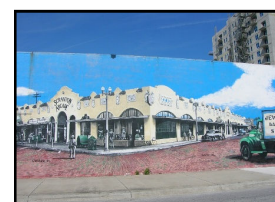
Urban Park



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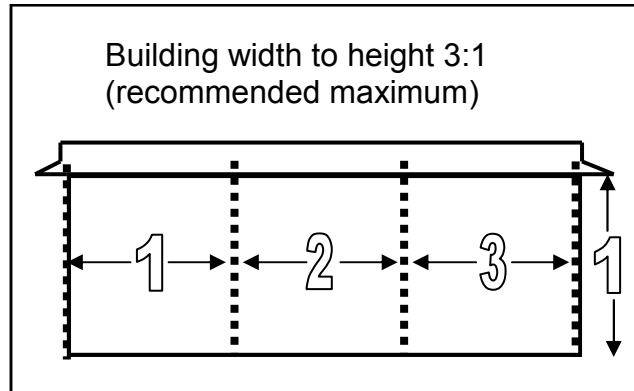


Tile mosaic



Trompe l'oeil

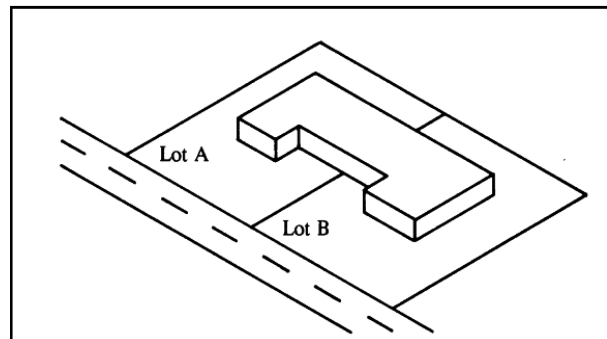
WIDTH TO HEIGHT RATIO is a measure that applies to buildings to avoid the occurrence of suburban style single story wide structures that are not consistent with livable community design standards. The Model Code advocates a width to height ratio of no more than 3:1 on commercial buildings and 2:1 on residential buildings.



WORKFORCE HOUSING refers to rental or owner-occupied housing needed for working people who do not earn enough income to rent or purchase housing at market rates. By comparison, “affordable housing” includes that for which monthly rents or monthly mortgage payments, including taxes, insurance and utilities, do not exceed 30 percent of that amount which represents the respective percentage of the median adjusted gross annual income for very low, low and moderate households, as indicated in Section 420.0004, F. S.

ZERO LOT LINE refers to a clustered form of development whereby single structures occupy separately platted lots.

Zero Lot Line



www.answers.com/topic/zero-lot-line

Common Architectural Styles (Post 1900)

Narrative in following section from Department of Planning, Geography and Recreation, Northern Arizona University (<http://jan.ucc.nau.edu/~twp/architecture>). Photographs by Thomas W. Paradis.

Period Styles

Colonial Revival. Combination of early Georgian, Federal and Greek styles including classical symmetry, two rooms deep, two rooms high (Four over Four plan), central or end chimneys, classical detailing, transom lights, pilasters around door. Hipped roof (British Georgian), or side-gable roof (American Georgian).



Tudor Revival. Many Tudor revival homes are loosely based on late medieval prototypes, identified with false (ornamental) *half-timbering*, a medieval English building tradition, often with stucco or masonry veneered walls, steeply pitched roof, cross-gabled plans. A variant of this is sometimes referred to as the *Picturesque Cottage* or *English Cottage*, which typically includes a picturesque (asymmetrical) floor plan but without the half timbering.



Beaux-Arts. Beaux-Arts style (Les beaux-arts - the fine arts - Ecole des Beaux-Arts in France) was advocated by Americans who studied at the Ecole. The style emphasized classical (Greek) forms and styles, elaborate detailing, massive plans, heavy masonry. Mostly used for grand public and institutional buildings, and the private homes of America's industrial barons.



Neoclassical. This became a dominant style for domestic buildings nationwide between 1900 and 1940. It was directly inspired by the Beaux-Arts style and the Columbian Exposition (Chicago World's Fair, 1893). The style tends to include the features of: classical symmetry, full-height porch with columns and temple front, and various classical ornament such as dentil cornices. Basically, this is the revival of the Greek Revival style that dominated the first half of the 19th century.



Italian Renaissance. Usually identified with a low-pitched, hipped roof, often with ceramic tiles and sometimes flat, hinting at its Mediterranean source region; wide, overhanging eaves with large brackets under the roofline; arched doors and windows, primarily on the first floor; Italian-style entryway, often with classical columns; facade usually symmetrical, but occasionally found in asymmetrical or picturesque floor plans. Eave brackets are a distinguishing feature that are typically rare on Spanish Revival and Mission-style buildings.

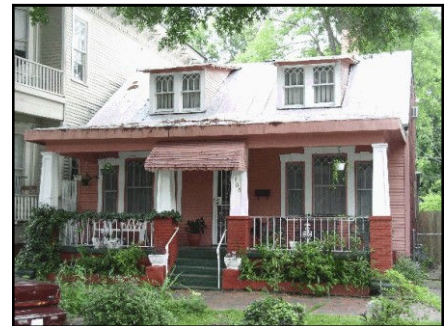


Spanish Revival. Spanish revival (also referred to as Spanish eclectic) was inspired by the architecture of Spain and Latin America, emphasizing their rich stylistic details. Due to the early influence of New Spain in the Southwest and Southeast, the style is rare outside the Southwest, Texas and Florida. It includes low-pitched roof; little or no eave overhang; red-tiled roof; prominent arch over door or window or porch; stucco wall surface; and usually asymmetrical facade.



Modern Styles

Craftsman/Bungalow. Low-pitched, gabled roof, wide overhang of eaves, exposed rafters (rafter tails) under eaves, decorative brackets (knee braces or corbels); incised porch (beneath main roof); tapered or square columns supporting roof or porch; 4-over-1 or 6-over-1 sash windows, often with Frank Lloyd Wright design motifs; hand-crafted stone or woodwork, often mixed materials throughout structure. Bungalows can either be front or side gabled.



Prairie Foursquare. Named "Foursquare" due to its characteristic boxy shape and four rooms per floor. This is one of the few indigenous American styles, developed by a group of Chicago architects known collectively as the Prairie School. Frank Lloyd Wright is essentially the "father" of Prairie style, and the acknowledged master of the prairie house. They are typically two-and-a-half stories with a large, central dormer. They often include a full-width front porch and four-room over four-room floor plan.



Art Deco. Art Deco uses a style of decoration applied to jewelry, clothing, furniture, handicrafts, buildings. Industrial designers used art deco designs to decorate streamlined cars, trains and kitchen appliances. Art Deco buildings have a smooth wall surface, often stucco; smooth-faced stone and metal; polychrome, often with vivid colors; forms simplified and streamlined; geometric designs including zig-zags, chevrons; towers and other vertical projections, presenting a vertical emphasis; machined and often metallic construction materials for decorative features.



International. Modern structural principles and materials; concrete, glass, steel the most common; occasionally reveals skeleton-frame construction, exposing its structure; rejected non-essential decoration; ribbon windows, corner windows a hallmark of the style; bands of glass as important as bands of "curtain wall"; balance and regularity admired and fostered; flat roof, without ledge. Often with thin, metal mullions and smooth spandrel panels separating large, single-pane windows.



Post-war Suburban. After World War II, when the suburban boom gained momentum, variations on the modern style became the prominent form of building for suburban neighborhoods and large tracts of standardized middle-class housing throughout the U.S. between 1950 and 1980, mirroring the modern movement and the more prominent International style. Modern houses included the California ranch, raised ranch, split-level, and "sea ranch" after the 1950s.



Post-Modern Styles

The postmodern era is most associated with architecture appearing since the 1970s, continuing through today. Often postmodern architecture is referred to as *neo-eclectic*, essentially representing a revival of period styles for houses. Postmodernism rejects modernist thought, a return to traditional, historical precedents, a re-awakened interest in history and heritage. Postmodernism coincides with both the historic preservation movement and the *new urbanism* movement.



Postmodern houses include Neo-Mansard, Neo-colonial, Neo-French, Neo-Tudor, Neo-Mediterranean, Neoclassical Revival and Neo-Victorian features. They are often referred to as “Vernacular,” meaning they incorporate architectural characteristics of these periods. Regarding commercial and institutional buildings, architectural styles have moved away from the “glass box” architecture of the International era and have incorporated more traditional features such as those found in the aforementioned periods, although they are often widely exaggerated.



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