

BOARD OF COUNTY COMMISSIONERS

DATE: December 16, 2014

AGENDA ITEM NO. 15

Consent Agenda ☐

Regular Agenda ☒

Public Hearing ☐

 **County Administrator's Signature:**

Subject:

Approval of the Revised Agreement to Lease and Joint Participation Agreement with Duke Energy Florida, Inc. d/b/a Duke Energy for the Pinellas/Duke Energy Trail Extension Project
County PID No. 000186A

Department:

Engineering and Technical Support

Staff Member Responsible:

Kevin Becotte, P.E., Director

Recommended Action:

I RECOMMEND THE BOARD OF COUNTY COMMISSIONERS (BOARD) CONSIDER FOR APPROVAL THE REVISED AGREEMENT TO LEASE AND JOINT PARTICIPATION AGREEMENT WITH DUKE ENERGY FLORIDA, INC. D/B/A DUKE ENERGY (DUKE ENERGY) FOR THE PINELLAS/DUKE ENERGY TRAIL EXTENSION (TRAIL) PROJECT, AND AUTHORIZE THE CHAIR TO SIGN THE AGREEMENT AND THE CLERK TO ATTEST.

IT IS FURTHER RECOMMENDED THAT STAFF BE AUTHORIZED TO COMPLETE DESIGN AND CONSTRUCTION OF TRAIL SEGMENT B2, FROM ENTERPRISE ROAD/US 19 TO SR 590, AND SEEK ADDITIONAL GRANT FUNDING FOR OTHER TRAIL SEGMENTS.

Summary Explanation/Background:

In December 1999, Pinellas County entered into multiple agreements with Florida Power Corporation, which then began doing business as Progress Energy, and now Duke Energy, to build a trail within the company's electronic transmissions corridor. These agreements consisted of an Agreement to Lease, a Joint Participation Agreement and a Trail Lease. The Agreement to Lease, which is necessary to proceed with construction of further trail segments has since expired. Since its expiration, the parties have worked diligently on agreeing to a combined Revised Agreement to Lease and Joint Participation Agreement (Agreement), which will govern the construction of future trail segments within the Duke Energy corridor.

The Agreement establishes responsibilities between the parties for completion of Trail segments that are, or will be within the properties either owned or encumbered by Duke Energy. The Agreement also combines and incorporates terms from the expired Agreement to Lease and amendments thereto, as well as terms from the expired Joint Participation Agreement and amendments thereto into this current proposed revised Agreement. The terms and conditions contained in this Agreement will supersede any of the previous agreements with Duke Energy relating to this Trail project.

The Duke Energy Florida Trail will be the completion of the Trail Loop, connecting the existing Trail termini at John Chesnut Park to the north and Weedon Island to the south, along the east side of the County. The revised Agreement provides a unique opportunity for the County to utilize over 20 miles of Duke Energy's transmission corridor to connect and expand the Trail network. The cost of acquiring similar properties to accommodate completion of the Trail Loop would be insurmountable. The County and Duke Energy partnership provides Trail connectivity which will serve the transportation, recreation, and economic needs of residents and visitors while allowing Duke Energy to name segments of the Trail.



This revised Agreement contains the following terms and conditions that will result in increased costs to construction and maintenance, as well as increased future liability to the County:

1. No floodplain/wetland compensation/mitigation sites are permitted within the Duke Energy corridor. Consequently, this will require the County to mitigate for floodplain/wetland impacts offsite, which will necessitate land acquisition.
2. Duke Energy has and continues to reserve the right to modify the design/construction standards prior to the approval of a proposed Trail segment.
3. As set forth in the revised Agreement, a specific portion of Trail segment B, as well as specified crossing sections identified by Duke Energy in Exhibit C, will require a heightened construction standard to accommodate Duke Energy's heavy equipment. Duke Energy reserves the right to identify other future Trail segments which may also require a heightened construction standard for such equipment.
4. Duke Energy reserves the right to require the County to relocate its Trail facility in the future, at the County's expense, if the County's use is inconsistent with Duke Energy's primary use of the property, such as interference with construction of new electrical facilities.
5. Duke Energy's Electric Transmission Right of Way Requirements for Shared Use Path/Trails, incorporated into the Agreement by reference, may also require some additional construction and maintenance costs.
6. With the exception of Duke Energy's willful damage to the Trail, the County shall be obligated to provide repairs and/or replacements of any Trail segments damaged by Duke Energy's construction, operation or maintenance of its transmission and distribution facilities.
7. The County agrees to a \$25 million liability insurance requirement prior to commencing construction activities. The proposed cost of procuring such a liability policy may range from \$56,000 to \$80,000 in annual insurance premiums, which may vary depending on the level of risk exposure during pre and post construction activities.

Anticipated Schedule for Segment B2: The plans for Trail segment B2, the only segment currently shown in the funded Capital Improvement Program (CIP), were developed by Duke Energy's consultant to a 60% stage of completion. The County will hire an engineering consultant, utilizing the Consultant's Competitive Negotiation Act procurement process, to update, revise and finalize the plans and specifications for Trail segment B2 based on the terms contained in this Agreement. The consultant selection process, design revisions, plans completion, land acquisition, environmental permitting and construction contract preparation are anticipated to be completed within 18 months. If a design/build approach is utilized, construction could start immediately after the aforementioned 18 month process, with an estimated duration of 12 months to complete construction once it begins. Alternatively, if a standard advertising/bid construction approach is used, an additional five (5) months would be added prior to the commencement of construction.

Fiscal Impact/Cost/Revenue Summary:

On June 17, 2003, the Board approved construction funding of \$4.1 million for Trail segments B and C. Trail segment B includes sub-segments B1, B2, B3 and B4. The constructed Trail segment B4 cost \$2.2 million, leaving \$1.9 million available for completing the rest of Trail segments B and C. During the Fiscal Year 2015-2024 Capital Improvement Program (CIP) budget process, the Board approved increasing the available construction budget allocation from \$1.9 million to \$4.6 million for Trail segment B2 construction completion.

Construction of Trail segment B2 will complete segment B, since B1 was constructed by the Florida Department of Transportation, and B3 is un-constructible due to railroad crossing restrictions and made unnecessary by connection to the City of Clearwater's Long Center Spur of the Ream Wilson Clearwater Trail. Based on the requirements of the proposed revised Agreement, the estimated construction cost and associated construction engineering inspection costs of Trail segment B2 are anticipated to increase. Confirmation of the increase and its magnitude will be quantified during the design process. The total current CIP budget estimate for Trail segment B-2 is \$5,132,000. At this time, the estimated increase for Trail segment B-2 ranges from \$435,000 to \$1,153,500 above the current CIP budget estimate, depending on the alternative required to satisfy permitting requirements. This results in a revised cost estimate of \$5,567,000 to \$6,285,500. Using the smaller cost estimate for Trail segment B-2 as a reference point and extrapolating for the remaining 18 miles of trail, a total cost of approximately \$50 million is estimated for the remaining Trail segments, excluding B-2.

While previously approved by the Board, Trail segment C and other Trail segments are currently unfunded. Future construction of these Trail segments is contingent on obtaining grant funding, such as the Transportation Investment Generating Economic Recovery (TIGER) grant and/or future Penny Extension (2020-2030).

Funding for Trail segment B2 is provided by the Infrastructure Sales Tax (Penny for Pinellas): Transportation and Traffic Flow, Pinellas Trail Expansion.

Exhibits/Attachments Attached:

Contract Review Transmittal with Comment Sheet

Agreement with Exhibits

Map of Pinellas Trail Loop Segments

NON-PURCHASING CONTRACT REVIEW TRANSMITTAL SLIP

PROJECT: Revised Agreement to Lease and Joint Participation Agreement with Duke Energy Agreement	
CONTRACT NO.: N/A	ESTIMATED EXPENDITURE / REVENUE: amount (Circle or underline appropriate choice above.)

In accordance with Contract Administration and its Review Process, the attached documents are submitted for your review and comment. Please complete this Non-Purchasing Contract Review Transmittal Slip below with your assessment, and **forward to the next Review Authority on the list, skipping any authority marked "N/A."** Indicate suggested changes by noting those in "Comments" column, or by revising, in RED, the appropriate section(s) of the document(s) to reflect the exact wording of the desired change(s).

REVIEW SEQUENCE	DATE	INITIAL/ SIGNATURE	COMMENTS (IF ANY)	COMMENTS REVIEWED & ADDRESSED OR INCORPORATED
Originator: Gina Harvey Ken Jacobs Richard Coates, P.E. Ivan Fernandez, P.E. Kevin Becotte, P.E.	11/19/14 11/19/14 11/20/14 11/17/14 11/18/14	Reviewed through SharePoint		
Parks: Paul Cozzie	11/24/14	Reviewed through SharePoint		
REM: Sean Griffin	11/20/14	Reviewed through SharePoint		
Risk Mgmt: Virginia Holscher	11/21/14	Reviewed through SharePoint	Comments received via email & attached	
Finance:** Cassandra Williams	11/20/14	Reviewed through SharePoint	Comments received via email & attached	
OMB:** Bill Berger	11/21/14 via JT	Reviewed through SharePoint	Comments received via email & attached	
Legal: Michael Zas	11/24/14	Reviewed through SharePoint	See comments in the last 1/10/14 notes. See edit on BCL MEMO added a # 7 to BCL Memo	Incorporated MC
Assistant County Administrator or Executive Director: David Scott, P.E.	11/25/14	Reviewed through SharePoint		

Please return to Merry Celeste By 4-3185.

All inquiries should be made to Gina Harvey ext. 45651.



CONTRACT REVIEW COMMENT AND RESPONSE FORM

Project Name: REVISED AGREEMENT TO LEASE AND JOINT PARTICIPATION AGREEMENT WITH DUKE ENERGY

Date Issued for Review: November 13, 2014

Date Comments are Due: November 20, 2014

PID No. 000186A

Reviewer	Comment No.	Comment	Response
Cassandra Williams/Clerk's Finance	1	What phase are we in for this project? Is the provision still in place for the County to lease after the completion of Phase Five?	The project is in segments instead of phases. Per the JPA, the Trail Lease will be amended to incorporate constructed segments of the Project.
Cassandra Williams/Clerk's Finance	2	Should the amount expended thus far be included on the Board Memo?	Yes, we have captured under Fiscal section of memo.
Cassandra Williams/Clerk's Finance	3	Is there an estimate of the costs for this phase?	Costs will be quantified during design process.
Cassandra Williams/Clerk's Finance	4	Is the County still paying for the liability insurance?	Risk has a policy in place for the B-4 segment. The B-2 segment will require additional coverage for an additional cost.
Virginia Holscher - Risk	5	After speaking with Michael Zas, and because it does not appear that any further negotiations are going to happen, the comments in the Board memo that the County must provide \$25million in coverage is accurate. We have a policy with this limit for the B-4 segment and would endorse this segment onto that policy. There will be insurance premiums that will be charged for this additional segment. We will interpret the insurance requirements paragraph 15 as requiring the County to carry \$25 million. Further the County will require our contractors to carry General Liability, Bodily Injury and Property damage coverage. Duke Energy will be an additional insured on the County's insurance. And since Duke requires that the insurance covers the liability of the County and contractor, we will require our contractors to add Duke Energy as an additional insured also. The paragraph refers to all risks to all persons or entities which may sustain injury, but such a policy is not reasonably available in the insurance industry, if at all.	Response from Michael Zas, County Attorney to comments 5, 6, & 7: I have confirmed with Duke's counsel that the interpretation we took as to the ins. requirements is correct and only the County will be required to have a \$25 million dollar coverage policy and those same coverage limits will not apply to the County's contractors.
Virginia Holscher - Risk	6	Without direct negotiation with Duke, we cannot guarantee this interpretation is correct, nor that our insurance certificate(s) will be acceptable to Duke Energy. But we believe this is a reasonable interpretation given the insurance policy language that is standard in the insurance industry.	
Virginia Holscher - Risk	7	We must furnish Certificates of Insurance to Duke prior to commencing construction activities and I would assume this will be a major construction project with a contractor which will go through an RFP process. Let us know if that is not the case.	

Jackie Trainer/OMB	8	Since this is the lease/joint participation agreement, it does not speak to the amount the trail will cost or the timeframe to do so. Does in agree with the current CIP Work Plan, or will budget need updated? If the County is the lead party as outlined in this agreement for the entire project, then our project description needs to be updated, and the budget amount confirmed.	
Jackie Trainer/OMB	9	My concern with the agreement is whether the County can accept all the obligations. For example, we will be responsible for any additional rights-of-way. Another example is paragraph 27: "... Duke future use... County responsible to relocate or adjust facility.... And reimburse Duke for expenses associated with relocation.... But Duke not obligated to the County. These are just a few examples.	

REVISED AGREEMENT TO LEASE AND JOINT PARTICIPATION AGREEMENT

THIS REVISED AGREEMENT TO LEASE AND JOINT PARTICIPATION AGREEMENT made and entered into and effective this ____ day of _____, 2014, is between DUKE ENERGY FLORIDA, INC. d/b/a DUKE ENERGY formerly known as FLORIDA POWER CORPORATION d/b/a PROGRESS ENERGY FLORIDA, INC. (hereinafter referred to as "Duke Energy") and **PINELLAS COUNTY, FLORIDA**, hereinafter referred to as "County", hereinafter jointly referred to as "Parties".

WHEREAS, the Parties entered into an Agreement to Lease dated December 3, 1999 to commit to a joint project to create an extension of the Pinellas Trail, hereinafter referred to as "Project", which was amended by a First Amendment to Agreement to Lease dated December 4, 2001 and then amended again by a Second Amendment to Agreement to Lease dated June 17, 2003; and

WHEREAS, the Agreement to Lease and Amendments thereto expired on December 2, 2010 and the Parties desire to renew the agreement; and

WHEREAS, on December 3, 1999, the Parties also contemporaneously entered into a Joint Participation Agreement to provide for a funding source, master plan, surveying, engineering and construction for the Project which was then amended in the First Amendment to Joint Partnership Agreement dated June 17, 2003; and

WHEREAS, the Parties now agree to renew the agreements referenced herein and to combine the Agreement to Lease and Amendments thereto, and the Joint Participation Agreement and Amendment thereto, into one new agreement now referred to as the Revised Agreement to Lease and Joint Participation Agreement (hereinafter referred to as the "Revised Agreement"); and

WHEREAS, this Revised Agreement supplants and supersedes the Agreement to Lease and Amendments thereto and the Joint Participation Agreement and Amendment thereto and now controls this Project as it moves forward; and

WHEREAS, the Parties entered into a Trail Lease on June 17, 2003, amended on February 22, 2005, December 27, 2007 and June 2, 2009, which provides that upon completion of each segment of the Project, the Lease is to be amended, in part, to incorporate those completed Project segments into the Lease and the Parties affirm that nothing herein is intended to contravene the Trail Lease, as amended; and to the extent the terms of the Trail Lease are not consistent with this Revised Agreement, the terms of this Revised Agreement and its exhibits shall supersede the terms of the Trail Lease only if a term contained within this Revised Agreement conflicts with same; and

NOW, THEREFORE, in consideration of the mutual promises herein contained, and for other good and valuable consideration, receipt of which is hereby acknowledged by the Parties, it is hereby agreed by and between the Parties as follows:

1. All of the above whereas clauses are incorporated herein and made a part of this Revised Agreement.

2. Any reference to the term Pinellas Trail relates solely to that portion of the Pinellas Trail that is or will be physically located upon the lands owned by Duke Energy in fee simple title or encumbered by a Duke Energy easement, hereinafter referred to as "Duke Energy Property".

3. Any reference herein to the term "Project" refers specifically to segments of the Pinellas Trail to be constructed on Duke Energy Property as follows: In accordance with the Concept Plan, and Master Plan as amended, attached hereto as Exhibits "A" and "B"

respectively, a proposed trail is to be designed and constructed near the intersection of East Lake Road and Tampa Road, moving in a southerly direction to the Countryside area, along the west side of U.S. 19 south, and then east to Weedon Island in the Gateway area on public right of way or property owned or encumbered by Duke Energy and is considered part of the Pinellas Trail arterial that will provide a continuous loop within the County. Consistent with the aforementioned Master Plan and considering Pinellas County is designated by the State of Florida as a Dense Urban Land Area, the impervious surface of the Trail shall consist of a 12' continuous hard surface unless physical constraints preclude otherwise, referred to as the Trail Utilization Area, except for the segment which has already been designed by Duke Energy which portion shall be 15 feet wide flanked by a 5 foot sod buffer for a total of 25 foot wide area.

4. The Parties now agree that the construction of the Project over the portion of the Duke Energy Property herein shall be designed, permitted, bid, awarded, and constructed by the County.

5. The Parties further agree that, if necessary, the County will be responsible for obtaining additional rights of way and easements outside the fee owned boundaries of Duke Energy Property needed to complete the Project as defined herein.

6. Duke Energy shall provide the County with all existing and latest construction plans for segments of the Project already designed or partially designed by Duke Energy but not constructed in both paper and electronic format and the County shall have plans completed and/or updated to County standards and Duke Energy's Electric Transmission Right of Way Requirements For Shared Use Paths/ Trails which are incorporated herein by reference via an engineering consultant designer who may be the same or different from the original designer. These plans which identify the location and typical section, for the Trail will be considered

acceptable by Duke Energy, pending modification to incorporate specific reinforced equipment crossings as outlined in this agreement and to remove mitigation sites therein. No floodplain/wetland mitigation or floodplain compensation sites shall be permitted within the Trail Utilization Area or upon any other Duke Energy fee owned or easement property.

7. In order to complete the revised design, permit and construct in accordance with County standards and the Duke Energy standards referred to in paragraph 6, Duke Energy will allow the County, their agents and other permitting entities to enter Duke Energy Property to perform survey, engineering, permit evaluations and geotechnical work and hazmat evaluations as needed.

8. County hereby expressly understands, covenants and agrees (a) that nothing contained in this Agreement or contemplated is intended to or shall increase Duke Energy's liability for personal injury or death or for any property damage, (b) that Duke Energy does not assume any such additional liability, (c) that liability arising out of the utilization and occupancy of Duke Energy's right-of-way and/or easement by County and County's employees, agents, contractors, invitees or any representative, is hereby assumed by County and shall be at the sole and exclusive risk of County, (d) that County shall answer any and all complaints relative to County's utilization of Duke Energy's right-of-way and/or easement, (e) that County shall protect, defend and hold harmless Duke Energy, its directors, officers, employees, representatives, agents and contractors from and against any and all actions, claims, damages and/or loss including costs and attorney's fees occasioned by or growing out of any actual or claimed usage or condition of Duke Energy's right-of-way and/or easement arising in any manner whatsoever, directly or indirectly, by reason of this Agreement or County's utilization thereof, and (f) that County covenants not to interfere with Duke Energy's existing and future

high voltage electric transmission and distribution lines and related facilities in any manner whatsoever.

9. The County will send Duke Energy a Notice of Intent to Proceed prior to the County's surveying, testing, designing and inspecting the Project on any segment of the Project on Duke Energy property. The services to be performed by the County for these segments of the Project may include, but not be limited to, the survey, environmental and hazmat investigations, preliminary design, testing, final design, preparation of construction plans and specifications, applications for federal, state, and local permit, and the construction of these segments of the Project. Preparation of construction plans and specifications shall include, but not be limited to, pavement, drainage structures, erosion protection, and maintenance of traffic all of which must comply with applicable NESC standards and OSHA standards. Unless otherwise mutually agreed upon by both Pinellas County and Duke Energy, the general alignment of the Trail shall be consistent with the intent identified in the aforementioned Master Plan prepared by Progress Energy (Exhibit B). In the event environmental contamination is discovered, either during the planning/design phase or the construction phase of the project, the County reserves the right to cancel this agreement and/or modify the alignment of the Trail out of the contaminated area, of which Duke Energy's approval for a modified alignment shall not be unreasonably withheld.

10. All proposed construction plans must be approved in writing by Duke Energy which approval shall not be arbitrarily denied, delayed or conditioned.

11. Duke Energy's Asset Protection Agent or designated representative shall be notified by County thirty (30) days prior to the commencement of construction activities within Duke Energy's Property.

12. The County acknowledges and agrees that it is solely responsible for the design, implementation and/or enforcement of the County's safety program or plan and that Duke Energy is not responsible for any errors or omissions in said safety program or plan.

13. At locations where any existing Duke Energy access road, as identified by Duke Energy in Exhibit "C", attached hereto, will cross the Trail within Duke Energy's Property, the Trail shall be designed and constructed to be driven over by a 134,000 pound vehicle (83,000 pound maximum axel loading) and to accommodate a crane outrigger load of 100,000 pounds. These Trail reinforcement areas shall be a minimum of 20' long and match the width of the Trail's bicycle and pedestrian facility, with markings to indicate "heavy equipment crossing".

14. The Parties further agree that the heightened construction standards described in paragraph 13 above shall also be constructed at the crossing locations in the vicinity of Stag Run Boulevard; specifically, the County agrees to reinforce that certain segment approximately 882 feet in length that is north of the Stag Run Cul-de-Sac and within the limits of properties owned by Duke Energy depicted in the attached Exhibit "D" which is incorporated herein.

15. Prior to commencing construction activities within Duke Energy's Property, the County will furnish Duke Energy with a fully completed certificate of insurance, in duplicate, evidencing insurance coverage of the liability assumed in the amount of Twenty Five Million (\$25,000,000.00) Dollars. Specifically, the County will carry, and will cause all of its contractors to carry, policies of insurance with respect to General Liability, Bodily Injury and Property Damage insuring against all risks to all persons or entities which may sustain property damage or personal injury as a result of the combined acts and omissions of the County or its contractors, which may be related in any way to the County's use of Duke Energy Property. Duke Energy shall be named as an additional insured on the County's insurance policy.

16. Subject to Duke Energy's prior written approval, Duke Energy will afford the County or its contractors the ability to use Duke Energy property outside and/or along the Project area for initial construction purposes. Additional areas may also be added on future Trail segments considered on a case by case basis, in which written approval by Duke Energy shall not be unreasonably withheld.

17. The County agrees not to use any equipment capable of extending over twelve (12) feet in height above natural ground within Duke Energy's Property without Duke Energy written authorization which shall not be unreasonably withheld provided appropriate safety measures as required by Duke Energy are incorporated into the activity being conducted.

18. Other than Duke Energy's facilities, and those approved in writing by Duke Energy and shown on the construction plans for the County's utilization within Duke Energy Property, no overhead wires, poles, light standards, dumpsters, signs, trees, buildings, structures or obstacles shall be located, constructed or installed within Duke Energy's Property.

19. All of County's operations, activities and equipment used within Duke Energy's right of way and/or easement beneath or in proximity to any of Duke Energy's electrical facilities shall, at all times, be in strict compliance with applicable provisions of the National Electrical Safety Code (NESC), the Occupational Safety and Health Act of 1971 (OSHA) and OSHA Crane Construction Standards for Power Line Safety, Sections 1926.1408 & 1409 and other applicable Federal, State or local rules, ordinances, regulations and codes. County is further notified and hereby agrees to so notify any of County's employees, agents, contractors, representatives or other persons engaging in County's activities upon said right of way and/or easement with County's knowledge and under County's supervision or control, that extreme caution is necessary around all of Duke Energy's electrical facilities, supporting structures,

anchor guys or related appurtenances, and in the event of any damages or injuries, County shall immediately report the nature and extent thereof to Duke Energy's nearest local office.

20. The area within Duke Energy's Property which is disturbed by the County's utilization, including Duke Energy's patrol road, shall be restored by the County to a condition at least as good as that which existed prior to construction.

21. Duke Energy's Asset Protection Agent or designated representative may verify that construction of the improvements within Duke Energy's Property are constructed in compliance with the terms and conditions of the construction plans. The County's construction manager (or designated representative) will make reports to Duke Energy's inspector regarding any damages to Duke Energy's Property caused by the construction of the improvements. The County agrees to pay all expenses incurred in connection with the repair of such damages.

22. The County shall be responsible for clean-up of any and all hazardous material spills resulting from the County's utilization which may occur within Duke Energy's Property. If a hazardous material spill occurs, the County shall immediately report the nature and extent of the spill to Duke Energy's Environmental Department at 866-769-1266.

23. The County agrees to furnish Duke Energy's Asset Protection Agent or designated representative with a set of as-built drawings of the County's utilization area within sixty (60) days of completion of construction, noting the location of the County's improvements within Duke Energy's Property.

24. Upon completion of construction, the County shall notify Duke Energy's Asset Protection Agent or designated representative for inspection of Duke Energy's Property.

25. The County and Duke Energy agree that as segments of the Project are completed, the Trail Lease, as amended, will be further amended to incorporate these newly

constructed segments of the Project.

26. The County hereby waives any right to claim damages to the Trail as defined herein caused by Duke Energy's construction, operation, or maintenance of its transmission and distribution facilities, so long as Duke Energy shall not willfully cause damage to or unreasonably interfere with the County's utilization of the Duke Energy Trail.

27. Duke Energy does not guarantee that County's utilization will be completely compatible with the safe and efficient operation and maintenance of Duke Energy's existing and future high voltage electric transmission and distribution lines within Duke Energy's right of way and/or easement. In the event that County's utilization interferes with Duke Energy's present or future use of the subject real property, County agrees to relocate or adjust its facilities and/or improvements at County's sole cost and expense. County further agrees to reimburse Duke Energy for all expenses associated with the relocation of any existing Duke Energy facilities necessitated by County's utilization in the event the proposed Trail necessitates the relocation of an existing Duke Energy facility. In the event that Duke Energy exercises its rights to install additional electric facilities within the land that is the subject of this Agreement, Duke Energy shall not be obligated to repair, replace or reimburse the costs of any of County's improvements placed upon the property.

28. All notices shall be sent to the following addresses for the County and Duke Energy unless otherwise informed:

As to County:

As to Duke Energy:

Timothy Mumford
Asset Protection Agent
2501- 25th Street North
St. Petersburg, FL 33713

29. As consideration for the use of Duke Energy property as set forth herein, the Parties agree that once completed, the trail under the Project herein located on Duke Energy Property shall be named the Duke Energy Florida Trail and to the extent not prohibited by Florida Department of Transportation (FDOT) or other regulatory agencies, all references in signage, including any signs on bridges connection on either side of Duke Energy fee-held or easement property that reference the Trail, and literature for the Trail shall refer to the Trail as The Duke Energy Florida Trail and shall contain the official Pinellas County identification to indicate that it is a cooperative project. All literature on the trail will contain a reference to Duke Energy Florida's contribution in language acceptable to Duke Energy. Duke Energy and the County will coordinate in a naming event and in cooperative joint events such as dedication, groundbreaking, or grand opening that may be conducted by the County. The County will keep all future signs referencing the Duke Energy Florida Trail in good repair. Duke Energy reserves the right to change the name of the Trail and it will be Duke Energy's expense for all costs involved in changing the name of the Trail.

County shall be responsible for printing, installing and maintaining signage within the Duke Energy Property. The naming rights signage will contain language mutually developed by Duke Energy and County. The signage to be implemented on future Trail segments shall be located up to every one-quarter (1/4) of a mile along the side of the Trail and at every road crossing. Additionally, the County may provide to the public educational brochures about the Trail. The language for these brochures will be jointly agreed upon between the County and Duke Energy.

30. Duke Energy agrees to forebear encumbering that portion of Duke Energy property as depicted in the aforementioned Master Plan dated November 1, 2001, as amended,

attached hereto as Exhibit B, dedicated for the construction and collocation of the Duke Energy Florida Trail.

31. The term of this Agreement shall be perpetual unless terminated in accordance with the provisions set forth in paragraph 32.

32. This Agreement may be terminated by the earlier of mutual consent of the Parties. The County reserves the right to terminate this Agreement upon a decision of a future Board of County Commissioners to discontinue the construction of the Trail or in the event that funds are not budgeted to fund this Agreement which is subject to Sections 129.07, 129.08, 129.09, Florida Statutes. Upon termination of this Agreement, all rights of possession and use revert back solely to Duke Energy.

WITNESSES:

Ben Todd Boyer
Print Name: Ben Todd Boyer

Sam Evans
Print Name: Sam Evans

DUKE ENERGY FLORIDA, INC. d/b/a
DUKE ENERGY, a Florida corporation

By: *Daniel Hendricks*
Print Name: Daniel Hendricks
Title: Manager Land Services

(Corporate Seal)



ATTEST:

PINELLAS COUNTY, FLORIDA, a
Political subdivision of the State of Florida

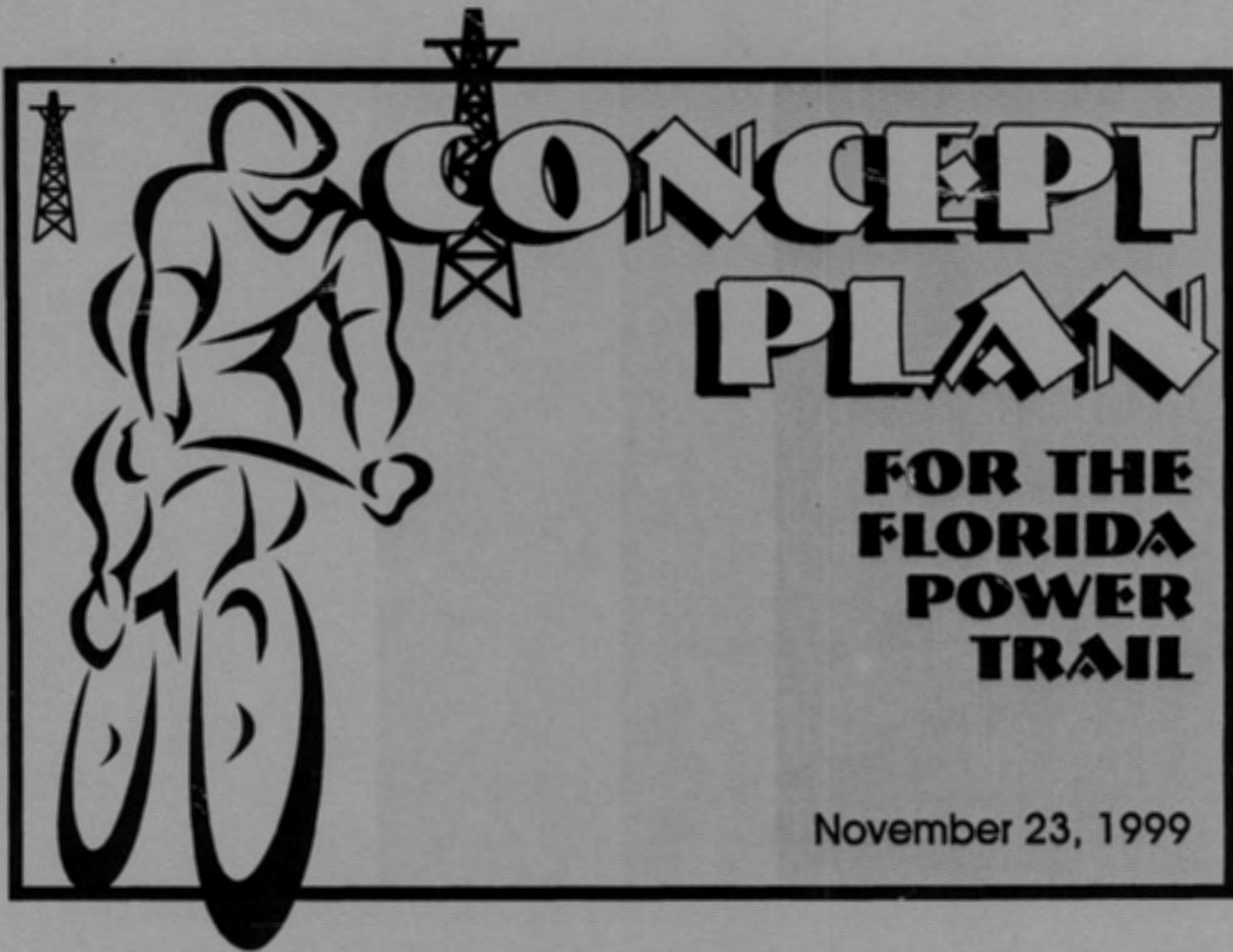
Print Name: Ken Burke
Clerk of the Circuit Court

By: _____
Print Name: Karen Williams Seel
Title: Commission Chair

Print Name: _____

**APPROVED AS TO FORM
OFFICE OF COUNTY ATTORNEY**

By: *M. Zan*
Attorney



CONCEPT PLAN

**FOR THE
FLORIDA
POWER
TRAIL**

November 23, 1999

EXHIBIT A

THE FLORIDA POWER TRAIL

PROJECT BOOK INDEX

- Project Book Index
- Task List
- Project Activity List
- Trail Utilization Area
- BAC Facilities Committee Report - March 17, 1998
- Pinellas Bikeways Plan
- Friendship Trail Map
- Meeting Summaries
- Index Map
- Area Detail Maps
- FPC Structure Maps
- Private Ownership Within Florida Power Corridor Maps
- License Agreements
- Florida Power Trail Program
- Legal Agreements
 - Agreement to Lease
 - Lease
 - Joint Partnership Agreement

FLORIDA POWER TRAIL TASK SHEET

Chart project development activities

Define alignment of Trail

- mapping
- text

Identify flyovers and other highway crossing locations and treatments

Negotiate needed agreements

Work with private properties and pursue modified agreements

Define other Trail connections

Define boardwalk treatments and flyover compact design

Negotiate for modifications where physical changes are needed to existing uses in the FPC property.

Define and pursue funding for the total project

Begin project design

Florida Power Corridor Trail

September 28, 1996

The BAC initiated a series of discussions on the FPC Trail idea. (This was not a new subject to the BAC since they had been discussing this since the mid 1980's prior to the Pinellas Trail.)

October 28, 1996

BAC recommended the MPO approve Bikeways Plan with inclusion in the Long Range Transportation Plan.

October 28, 1996

BAC narrowed FPC Trail concept to two corridors.

- *West of U.S. 19 between Enterprise Road and Belleair Road.

- *North of Ulmerton Road between U.S. 19 and Largo Central Park.

November 18, 1996

BAC considering extending FPC Trail boundary north of Enterprise Road.

December 5, 1996

BAC Facilities Standing Committee toured the FPC Trail beginning at S.R. 580 and continuing north, exploring several crossover points.

January 27, 1997

BAC Facilities Standing Committee reported to BAC that they had taken a second field trip on the FPC Trail south to Belleair Road.

March 24, 1997

BAC endorsed Facilities Standing Committee report recommending the use of the FPC land as a trail project from East Lake Road near Tampa Road continuing south to Weedon Island. FPC internally reviewing policy regarding recreational uses on property.

October 28, 1997

BAC sent letter to Pete Dagostino, Vice President, Bulk Power Delivery, FPC.

November 12, 1997

BAC Facilities Standing Committee had a field trip of the FPC properties from Belleair Road to the south.

November 13, 1997

Pete Dagostino, accompanied by George Townsend and Tom Kennedy, met with Bert Valery, Brian Smith, and Dick Fitzgerald on the project.

November 20, 1997

Brian Smith sent letter to Pete Dagostino defining the project activities to follow and ongoing contacts.

Florida Power Corridor - Trail

Page 2

January 7, 1998

Bert Valery and Brian Smith met with Fred Marquis to discuss project. Mr. Marquis confirmed County support for the project as pursued by BAC.

January 22, 1998

Brian Smith and Tom Kennedy appear before MPOAC to request the inclusion of FPC utilities within controlled access facilities of FDOT.

March 2, 1998

MPO Chairman Sallie Parks conducted a series of meetings with the Property Appraiser's Office to research tax exemption or credit for FPC.

April 28, 1998

MPO/BAC met with FPC Right-of-Way Utilization Committee in Maitland.

May 12, 1998

FPC Right-of-Way Utilization Committee and MPO/BAC representatives toured FPC property from East Lake Road/Tampa Road to U.S. 19/Allen's Creek.

May 27, 1998

FPC Right-of-Way Utilization Committee and MPO/BAC representatives toured FPC property from U.S. 19/Allen's Creek to Weedon Island.

June 9, 1998

MPO staff met with FPC Right-of-Way Utilization Committee in Maitland.

July 7, 1998

MPO staff met with FPC Right-of-Way Committee in St. Petersburg.

July 20, 1998

FPC, MPO, and BAC representatives revisited the areas of concern along the corridor as noted by the FPC Right-of-Way Committee.

August 4, 1998

FPC Right-of-Way Utilization Committee and MPO/BAC representatives met in Maitland.

August 14, 1998

Brian Smith presented the FPC Recreation Trail project concept to the Pinellas County Public Works CIP Committee.

August 17, 1998

Pinellas County representatives from Public Works, Planning, and the Attorneys Office met to discuss the pending legal agreements.

Florida Power Corridor - Trail

Page 3

August 19, 1998

FPC, MPO, and BAC representatives toured the FPC Corridor to evaluate potential problem areas with private license agreements.

August 21, 1998

FPC and Pinellas County representatives met at the FPC headquarters in St. Petersburg to discuss the pending legal agreements.

September 4, 1998

MPO Chairman Sallie Parks, Brian Smith, and Victoria Bucher, FPC met with the St. Petersburg Junior College President, Carl Kuttler, to discuss the project.

October 1, 1998

FPC and Pinellas County representatives met to negotiate the agreement between Pinellas County and Florida Power and further defined the project.

October 22, 1998

FPC and Pinellas County representatives discussed certain specific issues concerning the agreement.

November 12, 1998

FPC and Pinellas County representatives discussed in further depth the draft agreement and concluded the remaining issue concerned the hazardous materials provision.

December 14, 1998

FPC and Pinellas County representatives discussed Florida Power constructing the FPC Trail. It was agreed that Florida Power would perform a more detailed evaluation and bring specifics back to the next meeting.

January 28, 1999

FPC and Pinellas County representatives discussed the legal agreements, insurance policies, and Trail design standard.

February 25, 1999

FPC and Pinellas County representatives discussed the legal agreements, details on the Trail construction, and insurance policies. Separate meetings will be held by the engineering and risk management representatives.

March 11, 1999

Pinellas County Public Works met with FPC engineers to negotiate the JPA for the engineering work of the project.

Florida Power Corridor - Trail
Page 4

March 19, 1999

Risk Management officials and insurance underwriters met to negotiate the insurance provisions and amount for the project..

August 25, 1999

Florida Power Corporation and Pinellas County met to discuss the Limitations of Liability paragraph in the Trail Lease. County Commission Chairman Sallie Parks was the moderator of this meeting.

August 25, 1999 to November 22, 1999

Ongoing discussions by legal representatives.

November 23, 1999

Florida Power Corporation and Pinellas County Board of County Commissioners signed and executed the Agreement to Lease and Joint Partnership Agreement for the development and construction of the Florida Power Trail.

Trail Utilization Area

The Trail Utilization Area is defined to be a 25-foot area consisting of a 15-foot paved surface, with an additional 5-foot buffer on each side. The location of the Trail Utilization Area is described in the Florida Power Trail Concept Plan.

The buffer area may include amenities such as water fountains, benches, informational signage and landscaping treatments. These enhancements would be in conformance with the American Association of State Highway and Transportation Officials (AASHTO) standards for trail facilities

The Trail Utilization Area may be adjusted based upon a Master Plan agreed to by both Pinellas County and Florida Power Corporation.



**PINELLAS COUNTY
METROPOLITAN PLANNING ORGANIZATION**

14 South Fort Harrison Avenue Clearwater, Florida 34616 (813) 464-4751

**REPORT ON BICYCLE ADVISORY COMMITTEE
LEGAL/FACILITIES STANDING COMMITTEE
MEETING OF MARCH 17, 1997**

The Standing Committee has performed a careful review to date of the possibility of using Florida Power Corporation lands for a trail project. The Committee has conducted two field trips where on field evaluations were made.

There were originally three segments that were considered. The first segment to the north followed the power line from the interchange of Tampa Road and East Lake Road west and then south to S.R. 580, just east of the Countryside Mall. The second segment began at the Enterprise and U.S. 19 location and proceeded south on the west side of U.S. 19 to the Belleair Road area. The third segment was an east/west link just north of Ulmerton Road that ran from U.S. 19 to the Largo Central Park area. It was concluded by the Committee that a field check for the third east/west project would not be necessary and would not be considered a candidate at this time.

The Committee, however, did add for consideration the segment west of U.S. 19 and just south of Belleair Road that would then proceed across Allens Creek and U.S. 19 on a diagonal basis through the middle of the County and then proceed east across the Interstate to Weedon Island in the Gateway area. The Committee felt this latter segment was important because it provided connectivity through a portion of the County that is not presently served by such bicycle/pedestrian friendly facilities. A map identifying these corridors is attached.

It was the general consensus of the Committee that these power line corridors were feasible for locating a trail. It was recognized that there are several roadways that need to be taken into account as crossings and there are several retention areas that are to be bypassed. The Committee recognized that the actual alignment of a trail would need to be negotiated with Florida Power Corporation so that any concerns that entity had could be accommodated. The Committee felt that the project would present a considerable good will opportunity to the Florida Power Corporation if they were to consider such a use within their lands.

The project as a total would provide bicycle/pedestrian opportunities in a portion of the County that is not served by any other facility of this type. In addition, such a trail location would provide much needed connectivity to several east/west trails that are located on the MPO Bikeways Plan. The Committee also concluded that the project should be approached in segments to make consideration by not only the Committee more manageable but also consideration by Florida Power Corporation as well. It is known that, when other proposals for such action were brought to the power company, a very defined area and project were made so that agreements can be more easily worked out, if the power company is agreeable to the concept.







Therefore, the Committee has defined the project into three separate projects. The Committee also concluded that the first step would be to submit an application for a utilization permit for each of those project segments. This would be considered the first step in the negotiation with Florida Power Corporation.

The Standing Committee, therefore, recommends that the Bicycle Advisory Committee concur with the three segments to be considered and also with the submittal of the utilization permit applications. With that Committee action taken, the MPO staff will then complete the needed paperwork on behalf of the BAC.

Attachment

MPOCorresp.ct.pg 36-37.

PINELLAS BIKEWAYS PLAN FUTURE FACILITIES

-  PINELLAS TRAIL
-  PINELLAS TRAIL EXTENSIONS
-  COMMUNITY TRAILS
-  CONNECTING BIKE LANES
-  PROPOSED TRAIL ON FLORIDA POWER ROW
-  TRAIL OVERPASS/ UNDERPASS





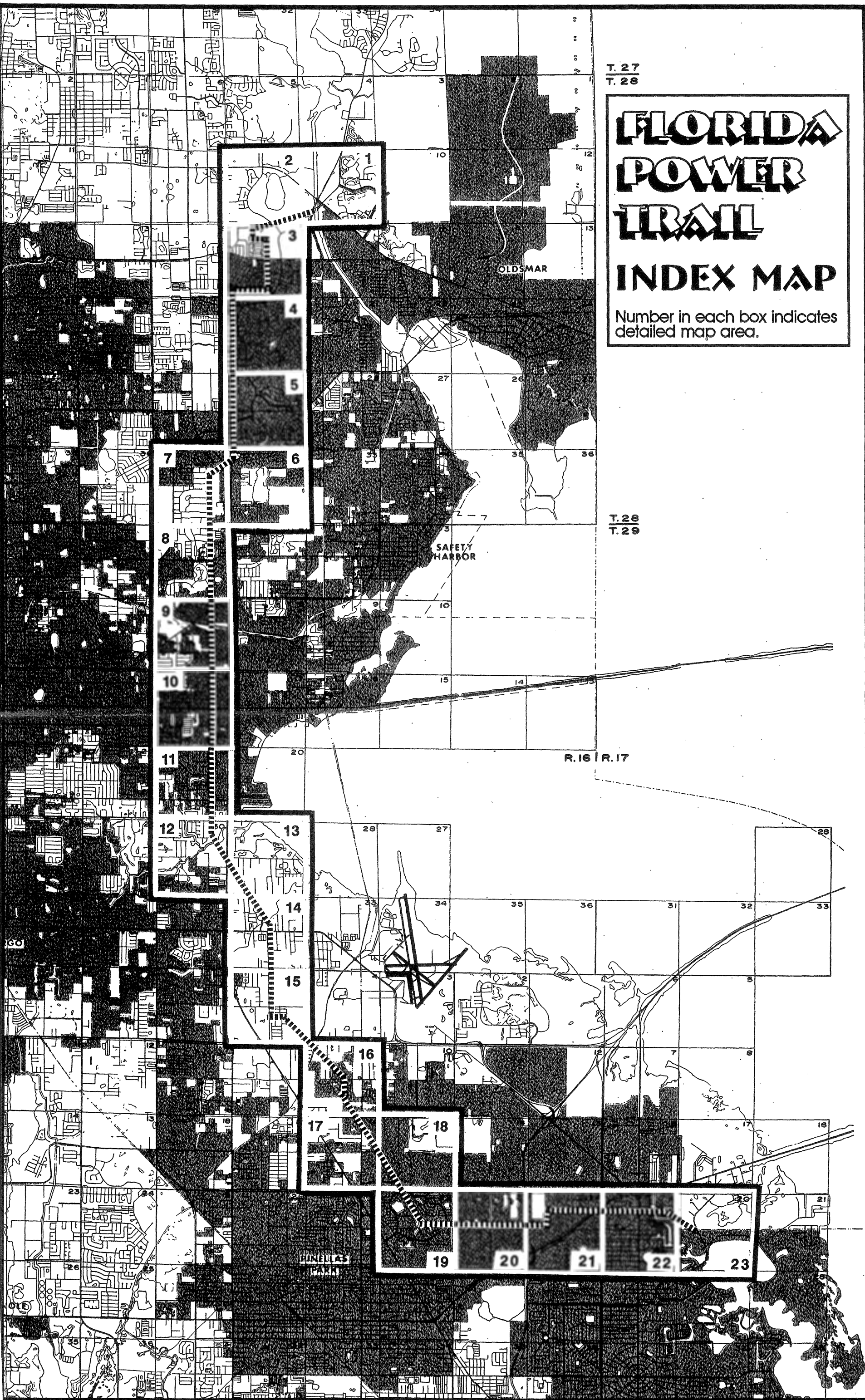
T. 27
T. 28

FLORIDA POWER TRAIL INDEX MAP

Number in each box indicates
detailed map area.

T. 28
T. 29

R. 16 | R. 17



MAP #1 WEST AREA DETAILS S 9/28/16

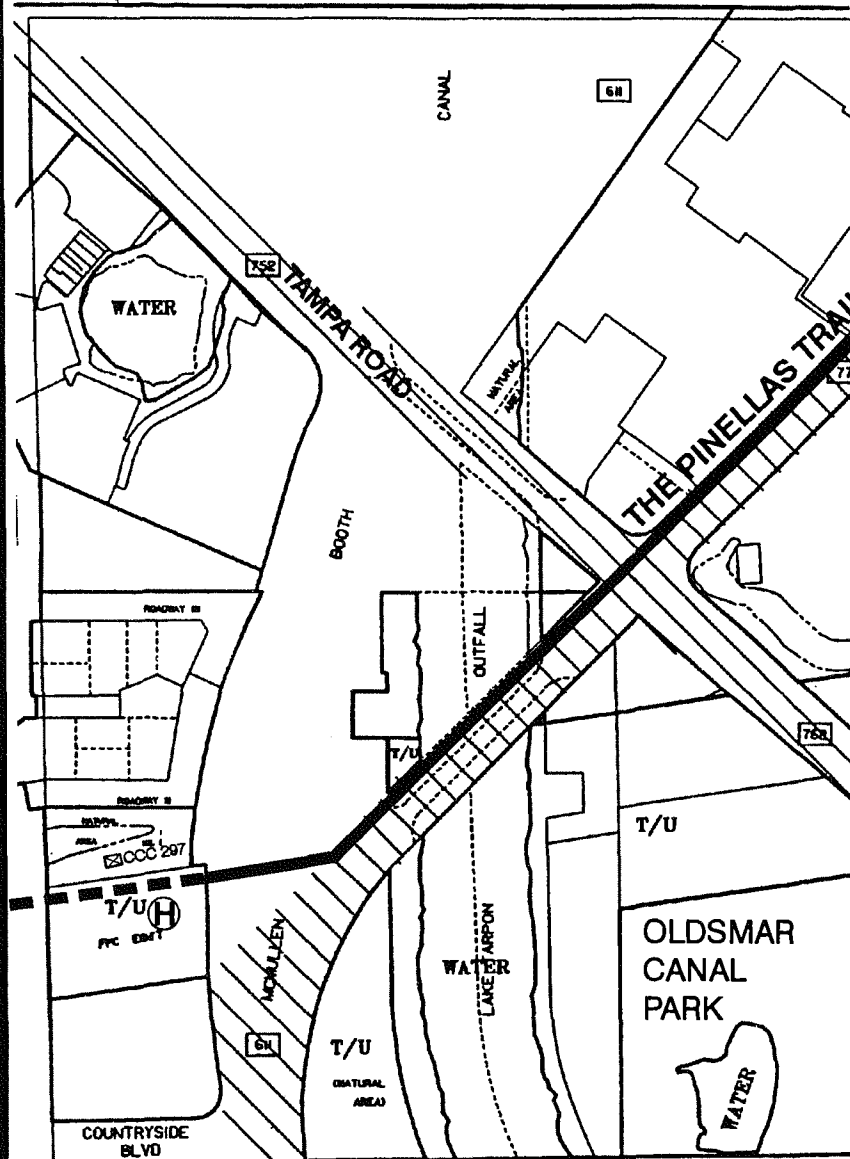
- * FPC Trail to connect with the Pinellas Trail on the west side of CR 611 south of Tampa Road.
- * FPC Trail to be located on north side of Easement between H frames and tower.
- * FPC Trail to be located at least 15 feet from tower legs.

INTERSECTIONS:

- * Trail has a major crossing at Tampa Road.

FLORIDA POWER FACILITIES:

- * Tower CCC 297 south of easement line
- ⊕ "H" pole



MAP #2 EAST AREA DETAILS S 8/28/16

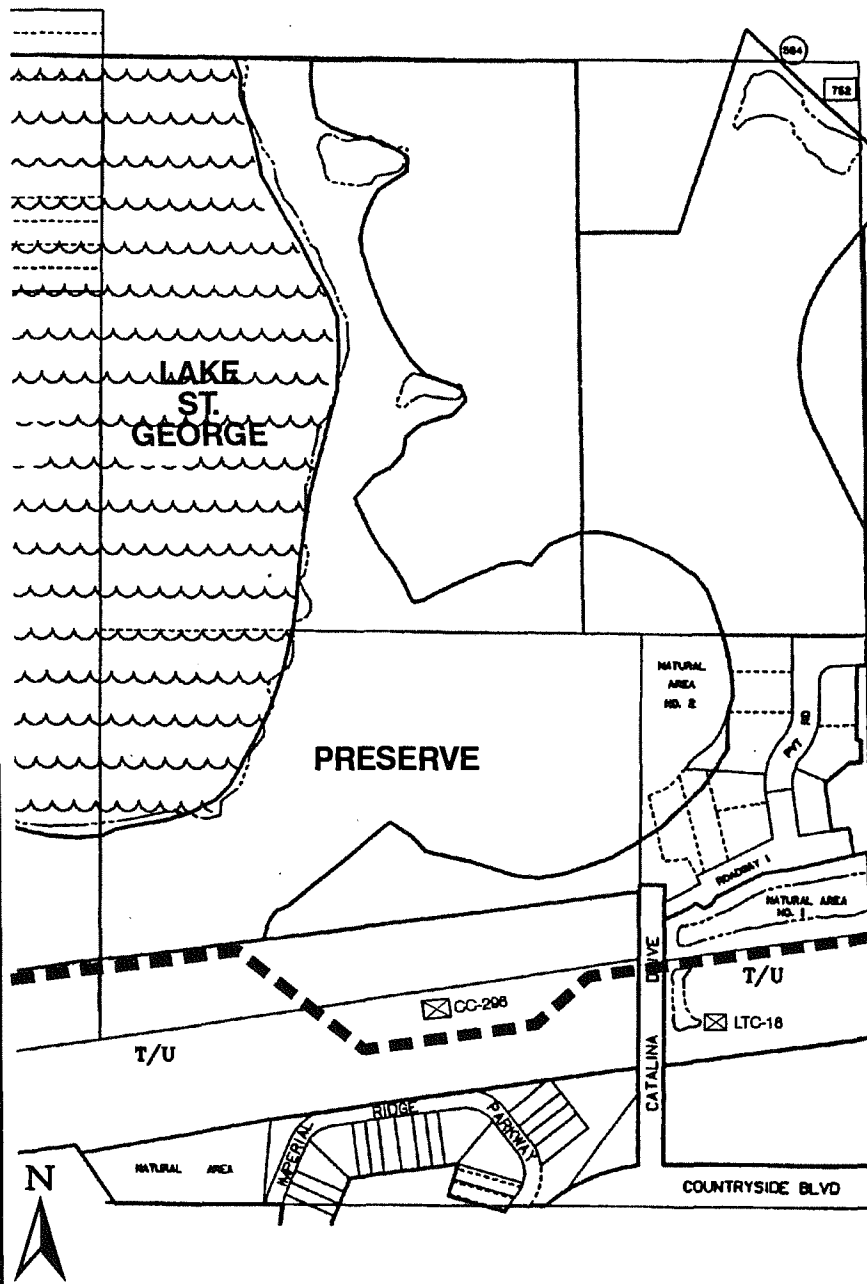
- * Trail to be located south of pond, then transitions to extreme north side of FPC easement after crossing Catalina Drive..
- * Water line on south side of easement.
- * Privately owned.
- * Dense vegetation overgrowth.

INTERSECTIONS:

- * No major road crossings
- * Minor crossing at Catalina Drive.

FLORIDA POWER FACILITIES:

- * Tower LTC-18 150' north of property line.
- * Tower CC-298 210' north of property line.
- * FPC R/W= 467.5'



MAP #2 WEST AREA DETAILS S 8/28/16

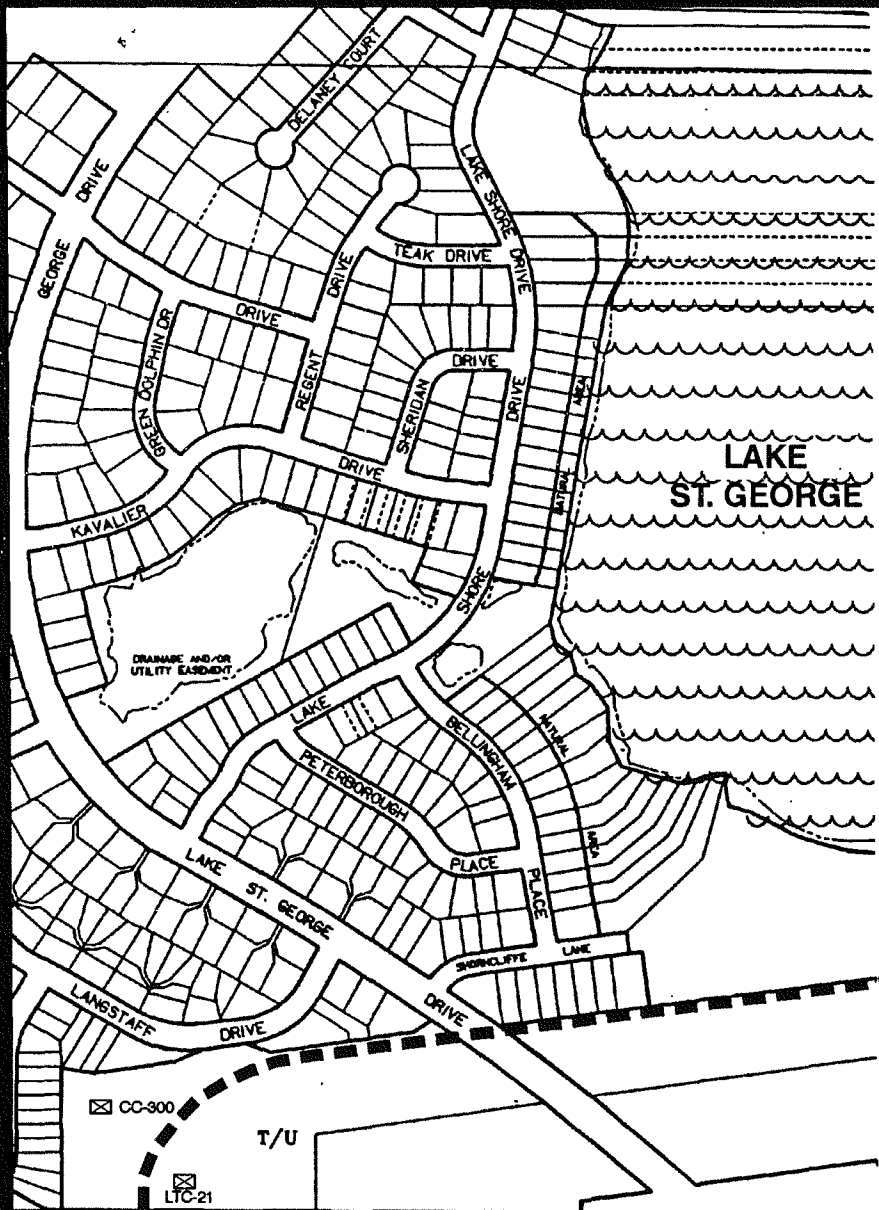
- * Trail to be located along the extreme north side of FPC easement, at least 15 feet from Tower CC 300 legs
- * Water line on south side of easement.
- * Privately owned.

INTERSECTIONS:

- * No major road crossings
- * Minor crossing at Lake St. George Drive.

FLORIDA POWER FACILITIES:

- * Tower LTC-21 317.5' south of property line.
- * Tower CC-300 210' south of property line.



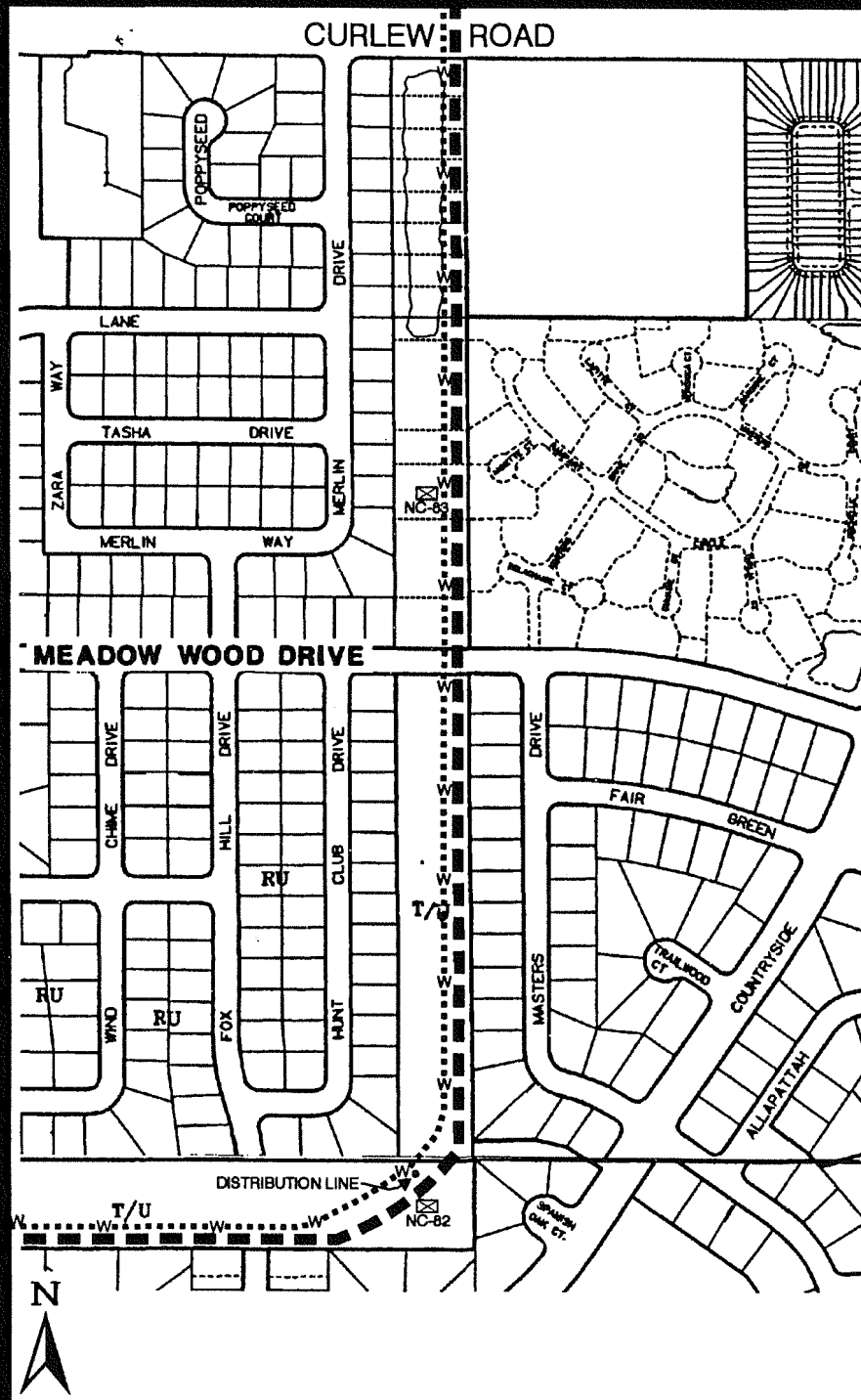
The map illustrates the layout of the Curlew Sub Station and its immediate surroundings. Key features include:

- Curlew Sub Station:** A large rectangular area in the upper center.
- Fenced Trailer Storage:** Located below the station, containing units NC-87 and NC-88.
- Pelican Lake Water:** A large irregularly shaped area to the left of the trailer storage.
- Streets:** Curlew Road runs horizontally at the bottom. Other streets include Wyndford Drive, Chatham Drive, Montclair Drive, Wesleyan Drive, and Dehaven Drive.
- Other Labels:** T/U, W, and WHITEHALL are also visible on the map.

- ## INTERSECTIONS:

- # FLORIDA POWER FACILITIES:

- * Tower NC-87 87.5' east of property line.
* Tower NC-86 87.5' east of property line.
* Tower TC-22 40' west of property line.
Ⓜ "H" Pole.
Ⓜ Water line.



MAP #3 SOUTH AREA DETAILS S 17/28/16

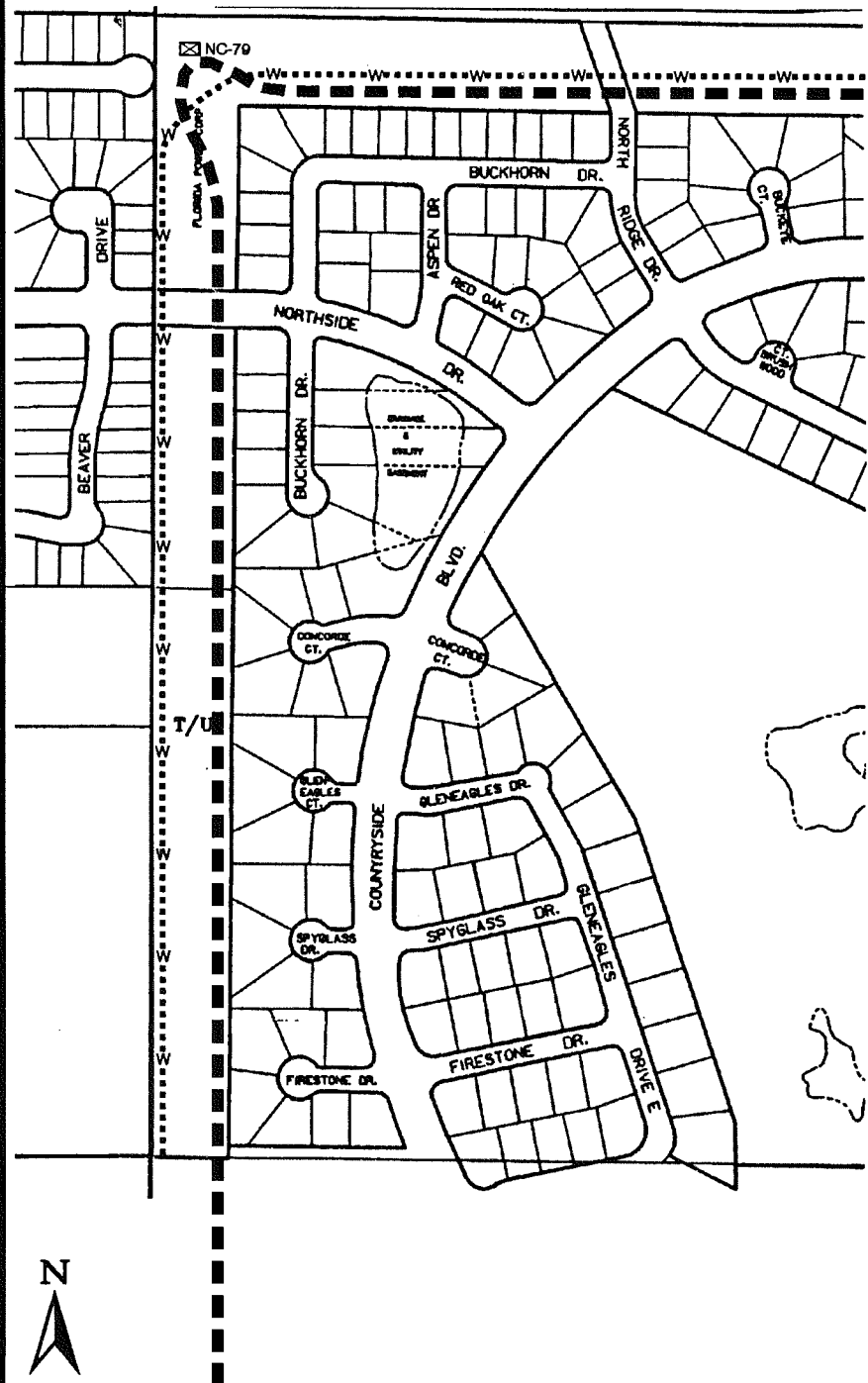
- * Trail to be located on east side at least 15 feet from tower legs.
- * Privately owned south to Meadow Wood Drive.
- * FPC ownership south of Meadow Wood Drive.
- * Water lines on east side.
- * Trail to be located between water lines and property edge.

INTERSECTIONS:

- * Minor crossings at Meadow Wood Drive.
- * No major crossings.

FLORIDA POWER FACILITIES:

- * Tower NC-83 87.5' west of property line.
- * Tower NC-82 87.5' west of property line and 87.5' north of property line.
- * Stub Pole Distribution to be moved to west side.
- * FPC R/W= 175'
- * Water line.



MAP #4 NORTH AREA DETAILS N 20/28/16

MAP #4 SOUTH AREA DETAILS S 20/28/16

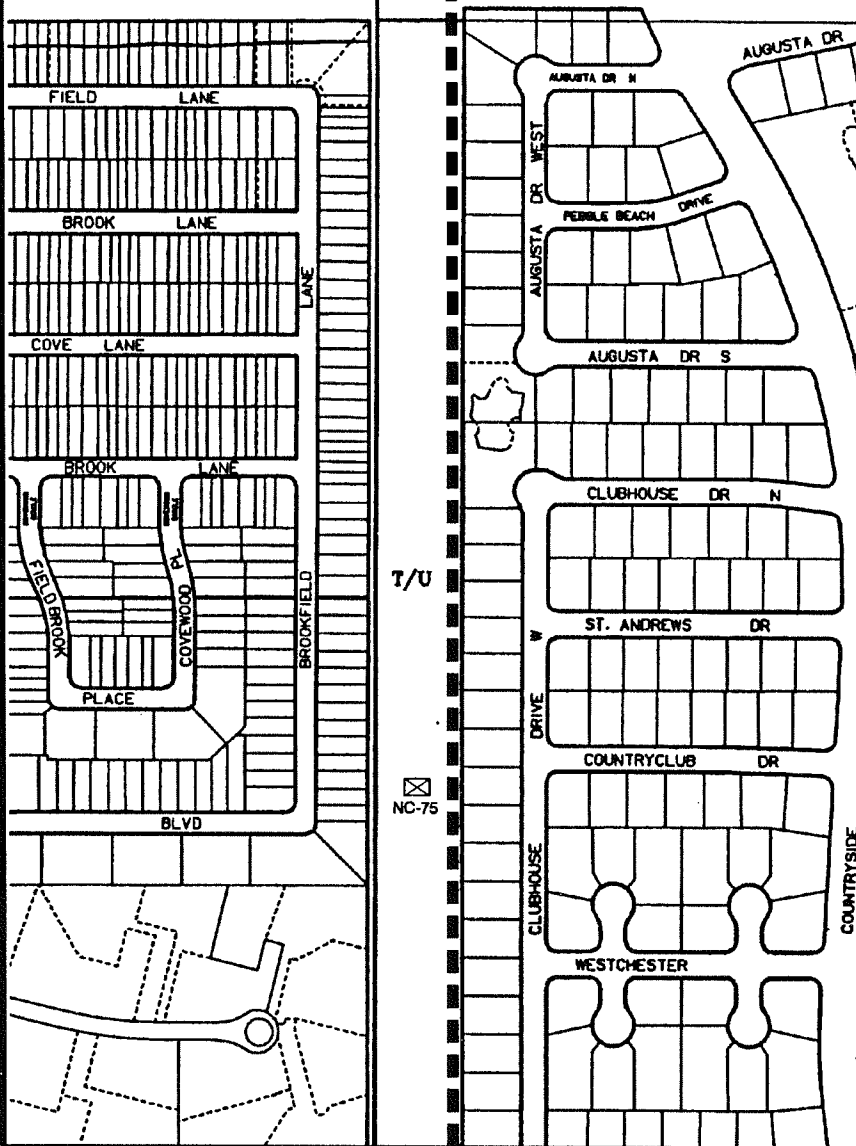
- * Trail to be located on east side at least 15 feet from hot oil line.
- * FPC owned.

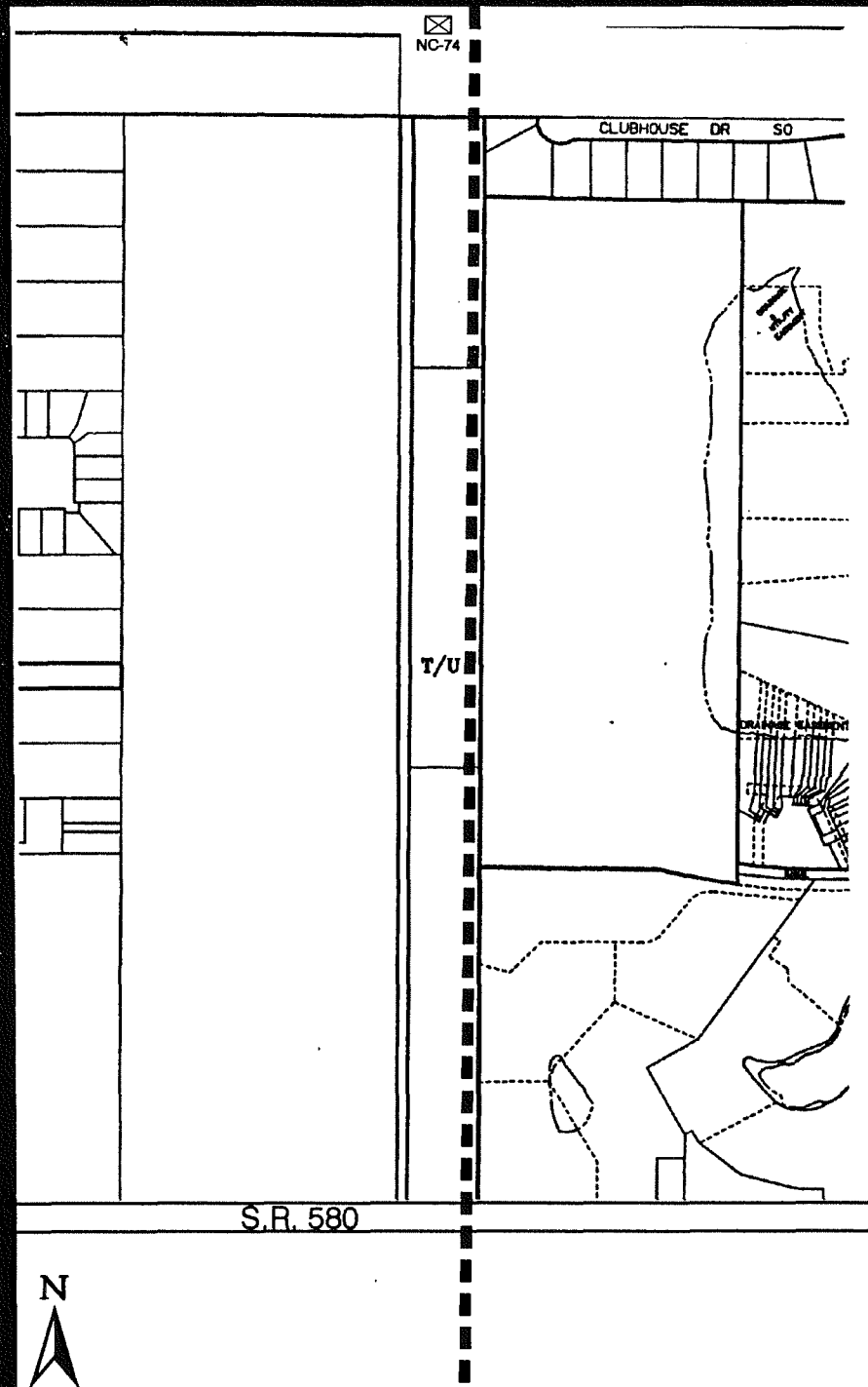
INTERSECTIONS:

- * No major or minor crossings.

FLORIDA POWER FACILITIES:

- * Tower NC-75 72.5' west of property line.
- * Hot oil pipeline 15' west of property line.
- * FPC R/W= 208'





MAP #5 NORTH AREA DETAILS N 29/28/16

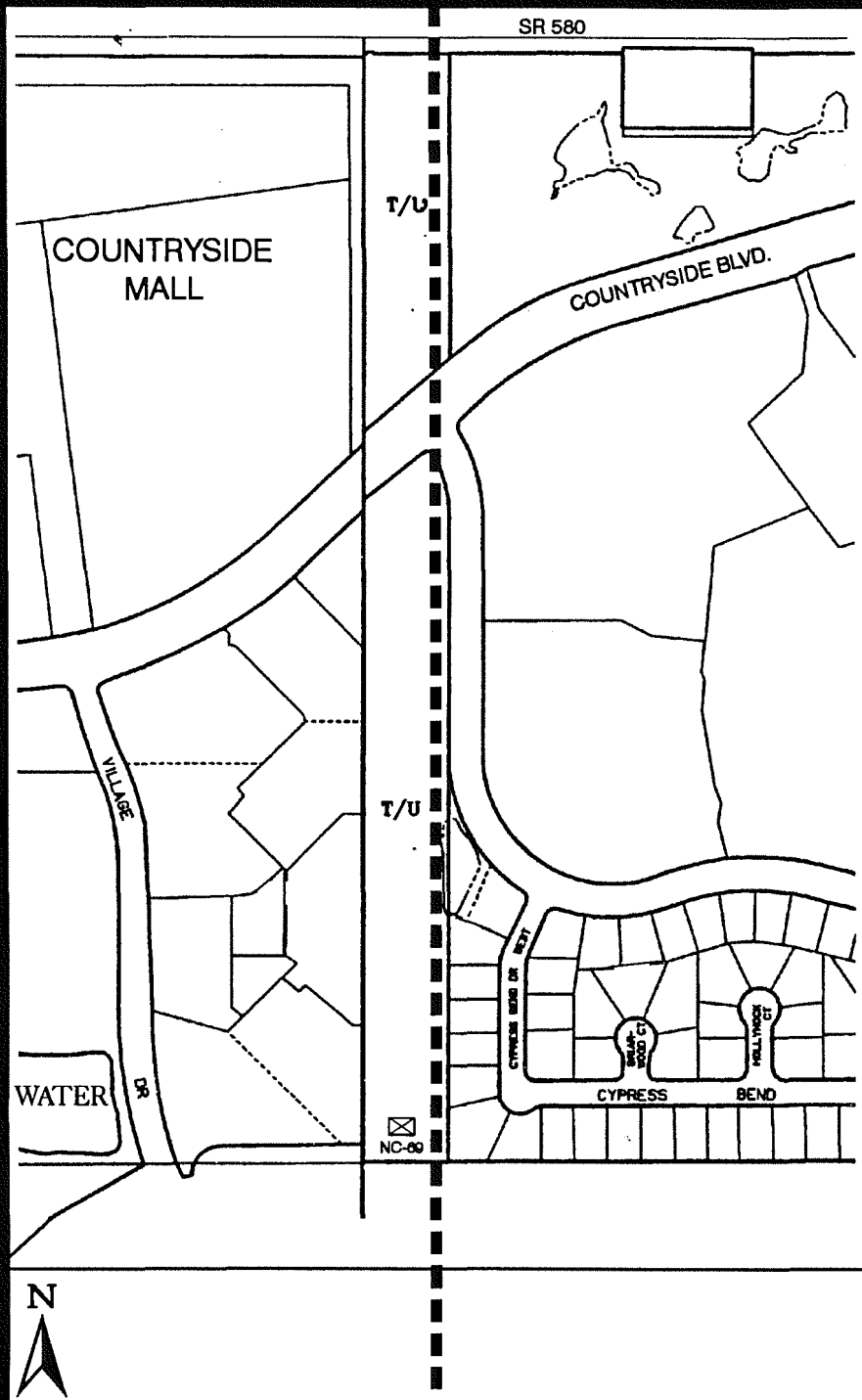
- * Trail to be located on east side at least 15 feet from hot oil line unless wavered.
- * License agreement with annual fee for Countryside Community Recreation Center for parking.
- * FPC owned.
- * North end 10' crossing easement for telephone lines.
- * Hot Oil Line / Bollards problem.
- * FPC Trail to be located at least 15 feet from tower legs.

INTERSECTIONS:

- * Major crossings at Main Street (SR 580)
(Pedestrian signal for SR 580 crossing.)

FLORIDA POWER FACILITIES:

- * Tower NC-74 87.5' west of property line.
- * Hot oil pipeline 15' west of property line.
- * FPC R/W= 208'



MAP #5 SOUTH AREA DETAILS S 29/28/16

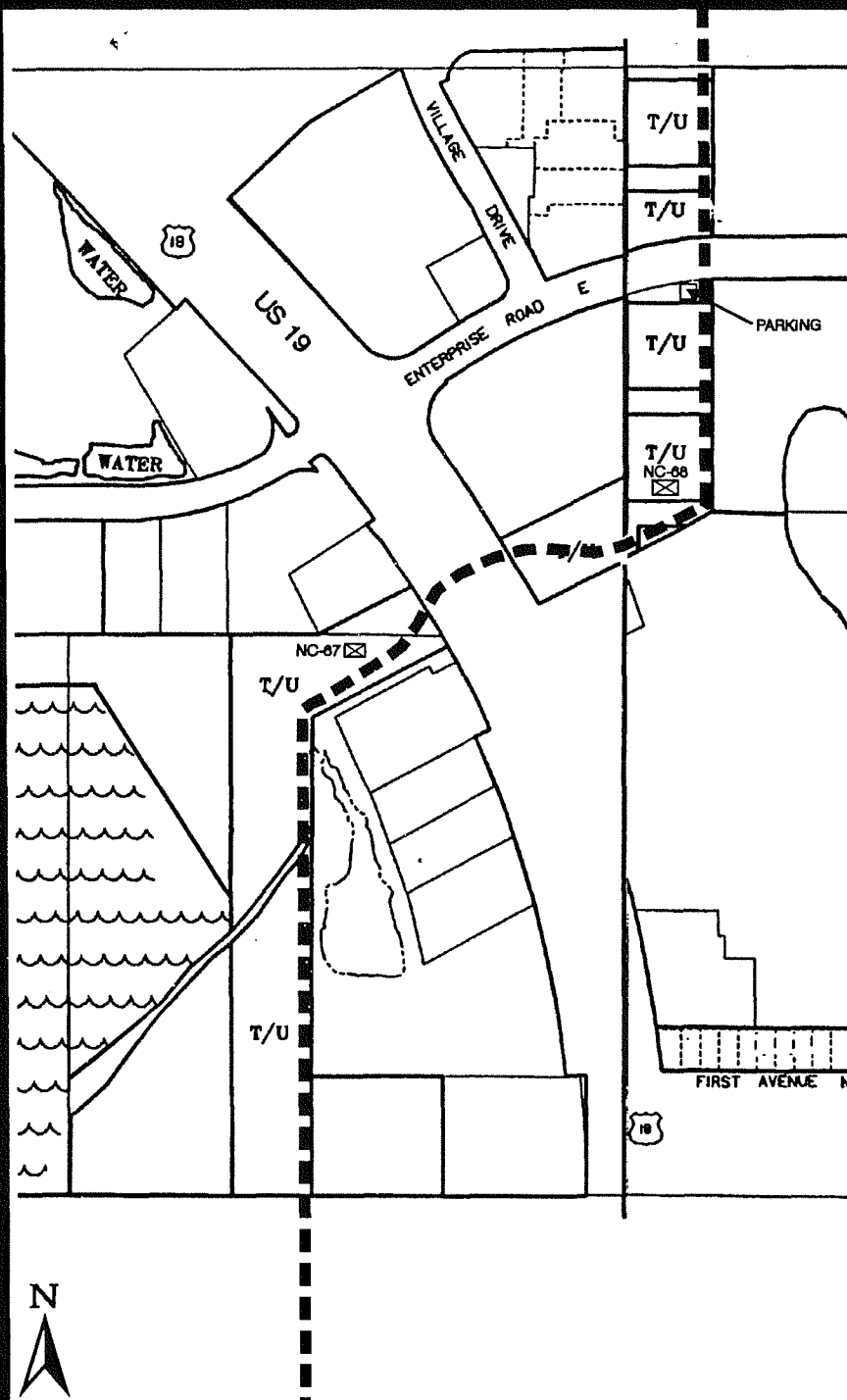
- * Trail to be located on east side at least 15 feet from hot oil line.
- * Trail on east side of Countryside Mall/AMC Theater parking lot.
- * FPC owned.
- * Distribution line conflicts at intersection.
- * Require coordination with Countryside Mall.

INTERSECTIONS:

- * Major crossing at Countryside Blvd.
- * Pedestrian signal for Countryside Blvd. crossing.
- * Blowout and thermal sag clearance requirements for overpass.

FLORIDA POWER FACILITIES:

- * Tower NC-69 87.5' west of property line.
- * Hot oil pipeline 15' west of property line.
- * FPC R/W= 208'



MAP #6 NORTH AREA DETAILS N 32/28/16

- * Trail to be located on east side at least 15 feet from hot oil line and at least 15 feet from tower legs.
- * FPC owned.
- * Distribution line conflicts at US Hwy. 19 with overpass.
- * Top of the World Golf Agreement.
- * Wire fence within FPC property northeast of golf course.

INTERSECTIONS:

- * Major crossing at U. S. Hwy. 19 (SR 55) will require overpass.
 - * Wire clearance issues with overpass.
 - * Transmission line problems.
 - * Alternative pedestrian crossing to be added at Enterprise Road on north leg.
- * Medium crossing at Enterprise Road.

FLORIDA POWER FACILITIES:

- * Tower NC-68 87.5' west of property line.
- * Tower NC-67 87.5' west of property line.
- * Hot oil pipeline is 15' west of property line.

MAP #7 SOUTH AREA DETAILS S 31/28/16

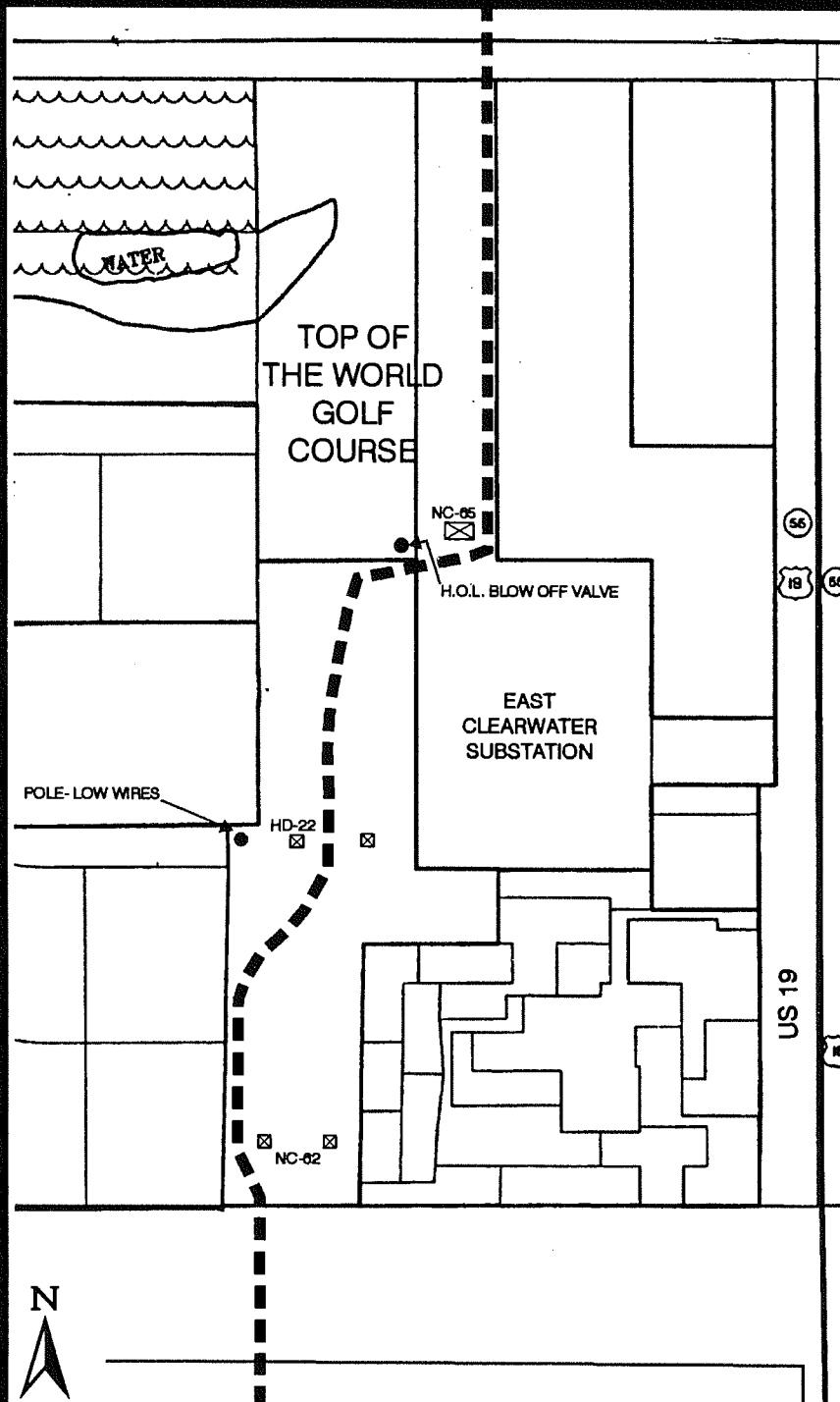
- * Trail to be located on east side at least 15 feet from hot oil line unless waved.
- * Trail east side of Top of the World golf course.
- * South of golf course Trail transitions to west side to avoid substation.
- * FPC owned- license agreement with Top of the World golf course.
- * Privacy fences 15' on FPC property.
- * Distribution line conflict needs cleared up.
- * Trail to be located at least 15 feet from tower legs.

INTERSECTIONS:

- * No major or minor crossings.

FLORIDA POWER FACILITIES:

- * Tower NC-65 87.5' west of property line.
- * Tower NC-62 87.5' west of property line.
- * Hot oil pipeline transitions to middle of ROW at south end of substation.



MAP #8 NORTH AREA DETAILS N 6/29/16

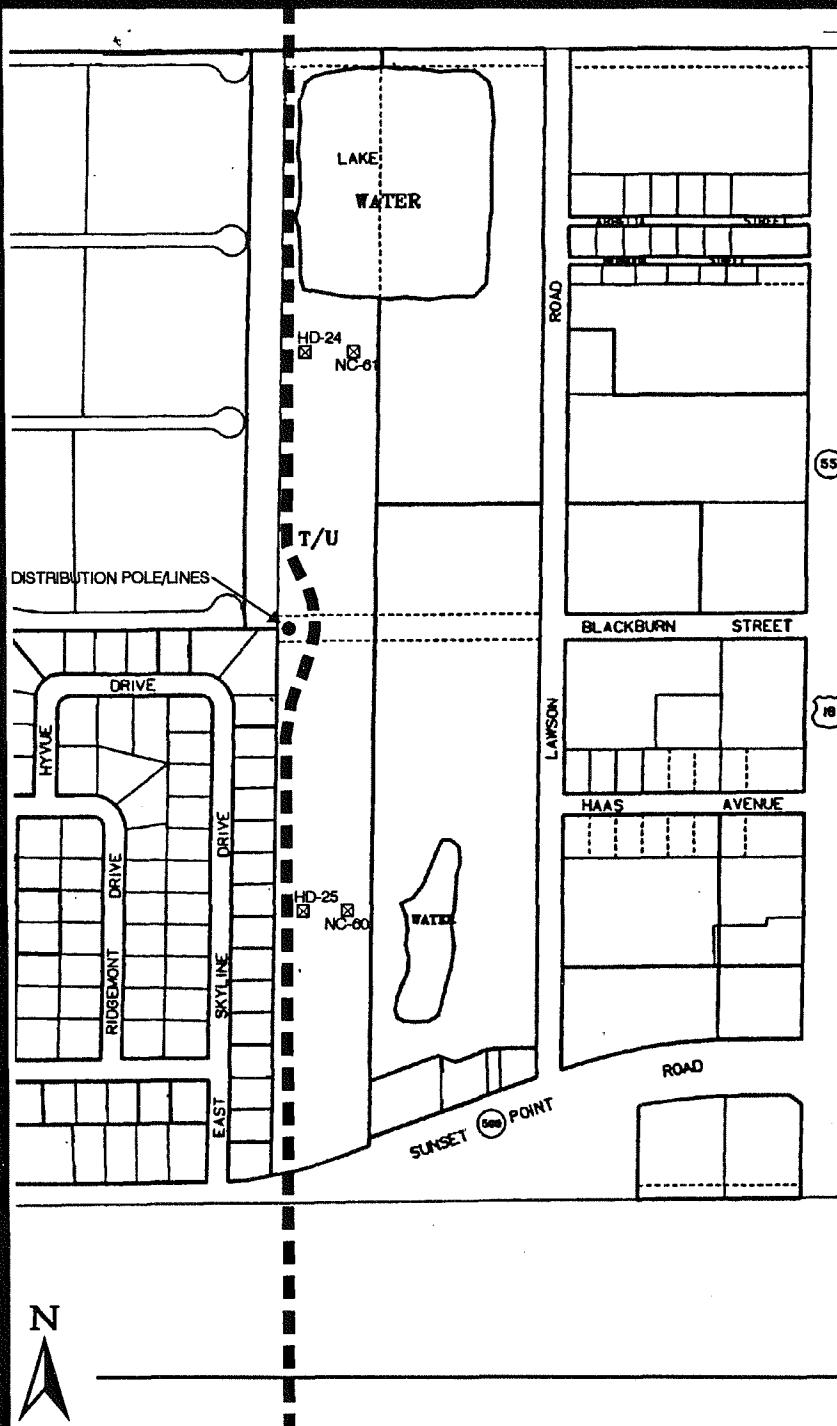
- * Trail to be located on west side at least 15 feet from tower legs.
- * At pond Trail between distribution lines on the west and hot oil pipeline on the east.
- * FPC owned.

INTERSECTIONS:

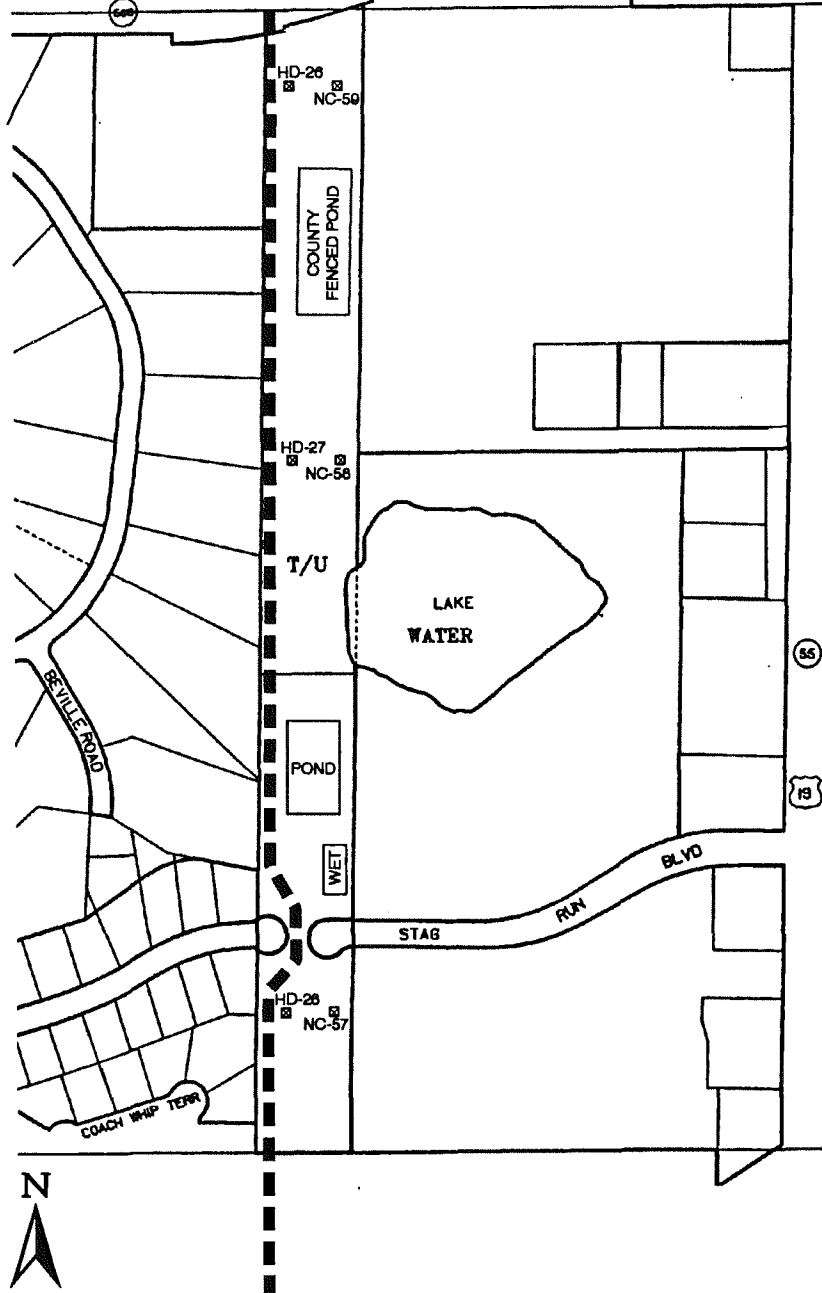
- * No minor crossings.
- * Major crossing at Sunset Point Road.

FLORIDA POWER FACILITIES:

- * Tower NC-61 87.5' west of property line.
- * Tower HD-24 125' east of property line.
- * Tower NC-60 87.5' west of property line.
- * Tower HD-25 50' east of property line.
- * Hot oil pipeline on east side of ROW transitions to west side south of Tower NC-61.
- * FPC R/W= 328' north of Blackburn Street.
- * FPC R/W= 225' south of Blackburn Street.



SUNSET POINT ROAD



MAP #8 SOUTH AREA DETAILS S 6/29/16

- * Trail to be located on west side.
- * FPC hot oil line on east side.
- * 2 County fenced retention ponds.
- * Trail splits two cul-de-sacs.
- * County owned south of Sunset Point Road.
- * Trail may be closer to tower than 15 feet- May require fence around tower legs.

INTERSECTIONS:

- * No major or minor crossings. (Except cul-de-sacs.)

FLORIDA POWER FACILITIES:

- * Tower NC-59 87.5' west of property line.
- * Tower HD-26 50' east property line.
- * Tower NC-58 87.5' west of property line.
- * Tower HD-27 50' east of property line.
- * Tower NC-57 87.5' west of property line.
- * Tower HD-28 50' east of property line.
- * Hot oil pipeline is 15' west of property line.

MAP #9 NORTH AREA DETAILS N 7/29/16

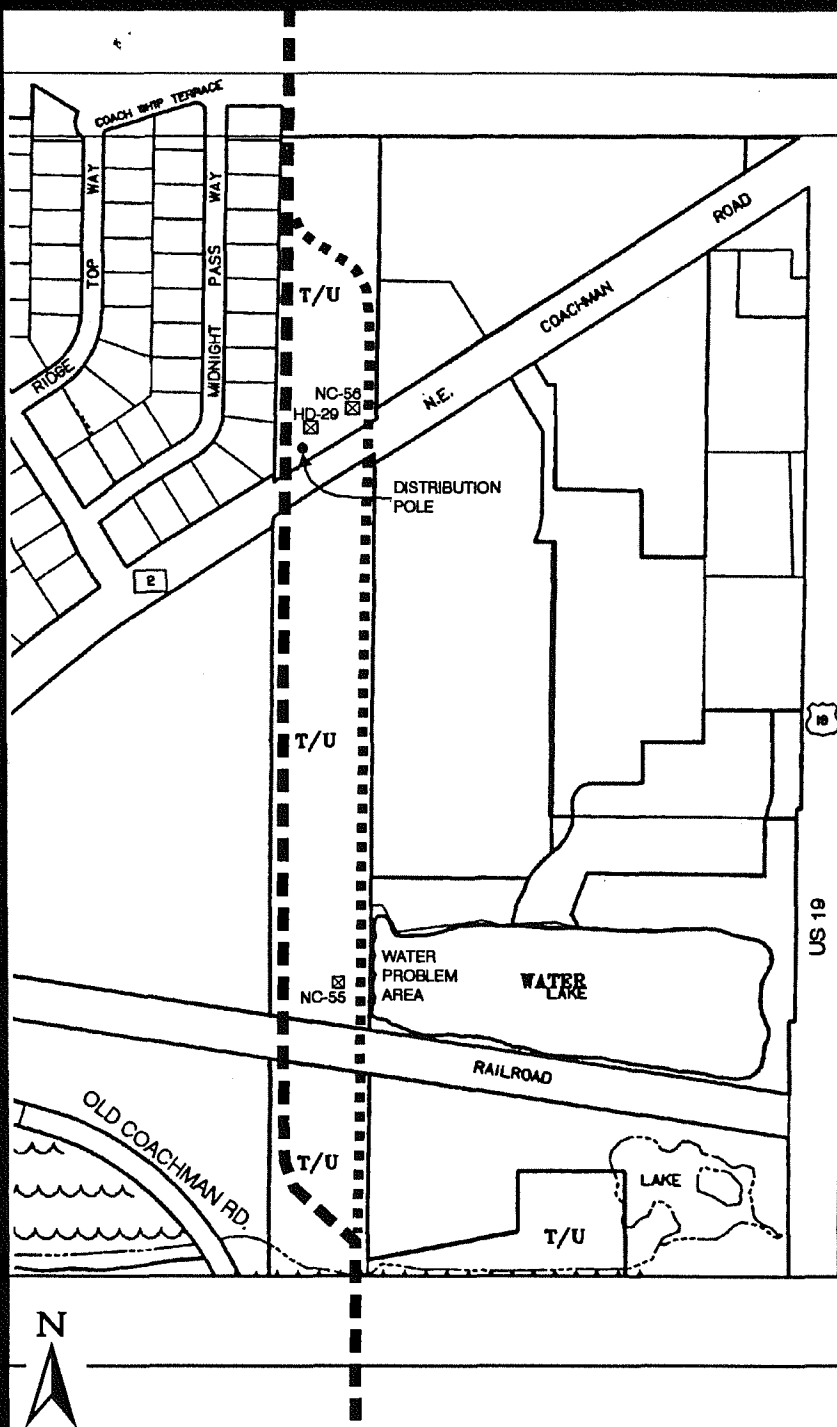
- * Trail to be located on west side from Stag Run to railroad tracks.
- * South of railroad tracks, Trail transitions to east side of FPC property to avoid road side.
- * FPC owned.
- * County to contact CSX about crossing railroad.
- * Possible wetland area.
- Possible Trail alternative location to eliminate distribution line conflict.

INTERSECTIONS:

- * Major crossing at N.E. Coachman Road and Seaboard Coast Line Railroad (CSX).

FLORIDA POWER FACILITIES:

- * Tower HD-29 50' east of property line.
- * Tower NC-56 87.5' west of property line.
- * Tower NC-55 87.5' west of property line.
- * Hot oil line 27.5' west of property line, transitions to 45' north of NC-55.
- * FPC R/W= 225'



MAP #9 SOUTH AREA DETAILS S 7/29/16

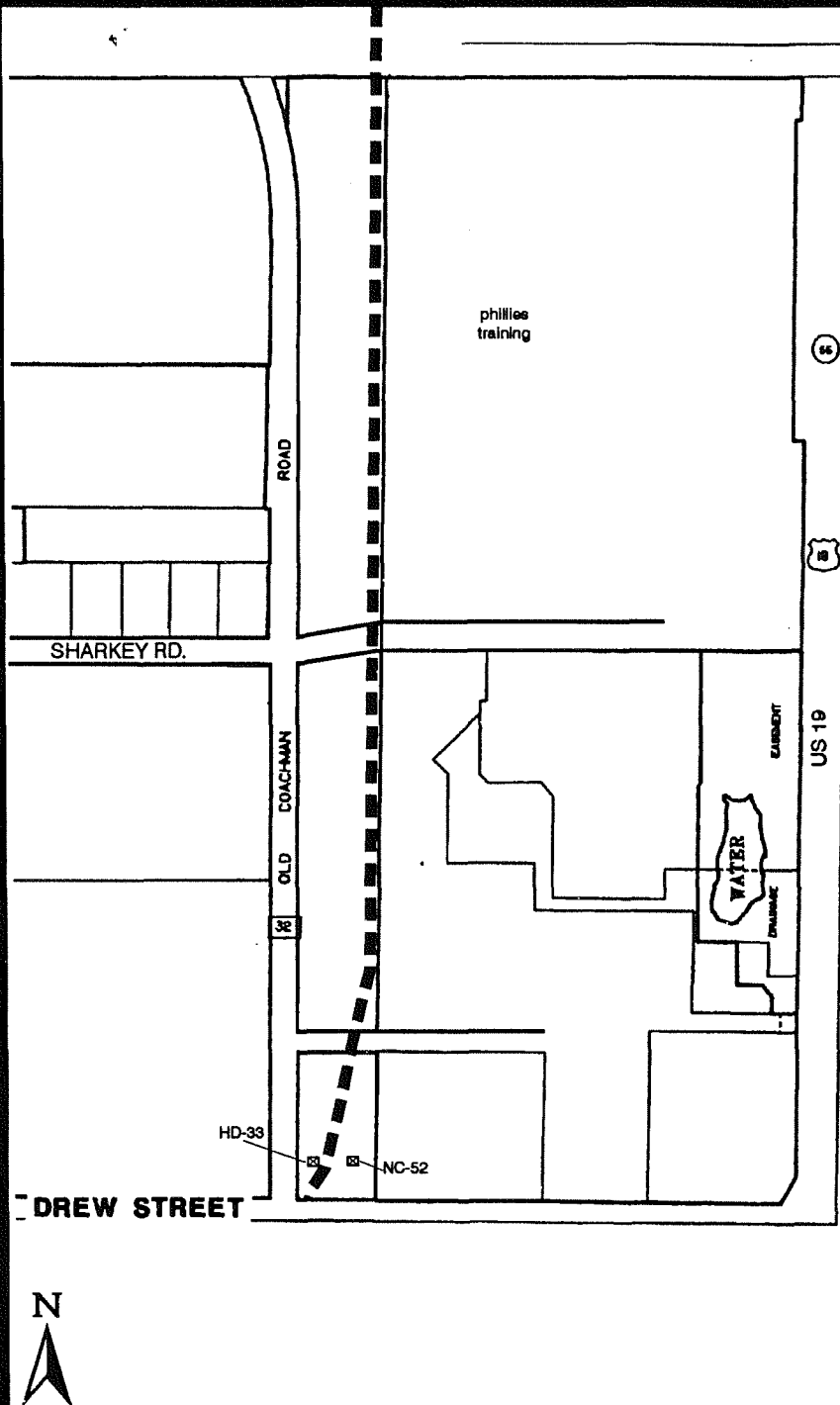
- * Trail to be located on east side of FPC property.
- * Clearwater recreation complex near Old Coachman Road has a lateral walk path on FPC property with parking.
- * Trail transitions to west side at SPJC/Drew Street signal.
- * FPC hot oil line on east side.
- * Time Warner parking agreement with FPC.
- * Trail to be located at least 15 feet from tower legs.

INTERSECTIONS:

- * Major crossing at Drew Street (signalized).
- * Minor crossing at Sharkey Road.

FLORIDA POWER FACILITIES:

- * Tower HD-33 50' east of property line.
- * Tower NC-52 60' west of property line.
- * Hot oil pipeline 45' west of property line, transitions to 27.5' south of Sharkey Road.
- * FPC R/W= 225'



MAP #10 NORTH AREA DETAILS N 18/29/16

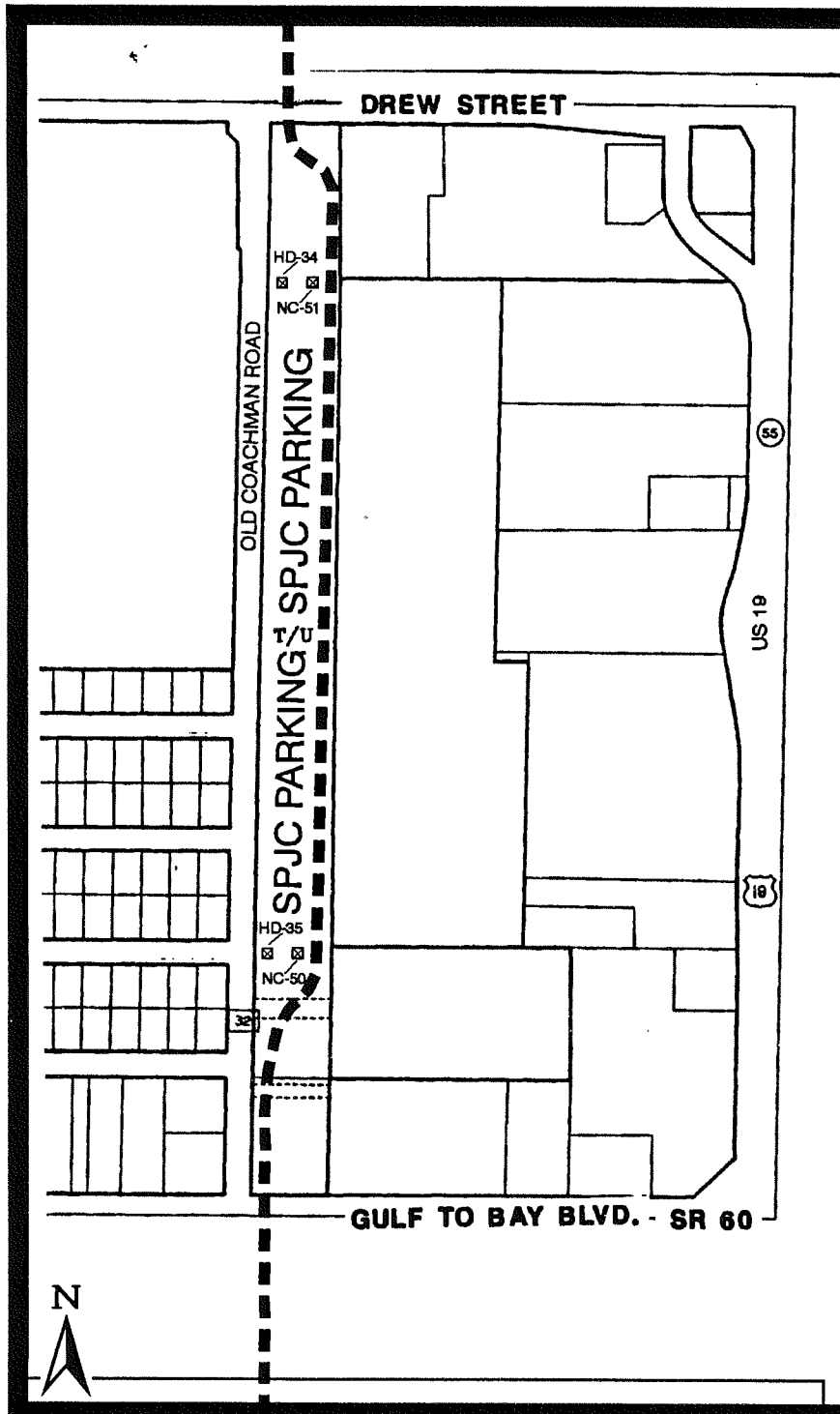
- * Problem Area / Design Issues / SPJC Cooperation.
- * FPC hot oil line on east side, transitions to west side to Gulf to Bay signal.
- * SPJC parking lot on FPC property.
- * Water wells/Clearwater.
- * Preferable Trail alignment on east side.
- * Variation to 15' hot oil line.
- * Parking relocation.

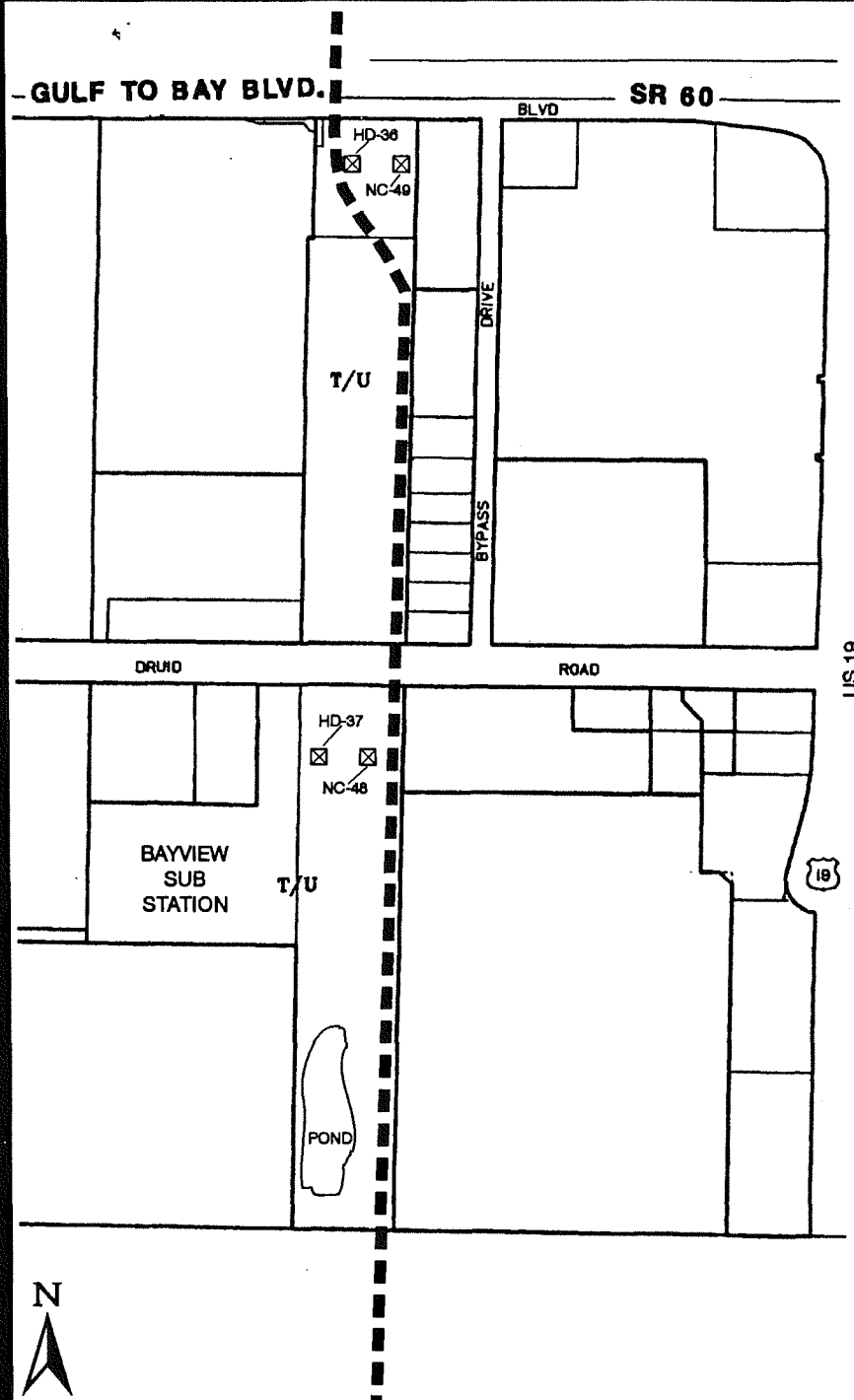
INTERSECTIONS:

- * Major crossing at Gulf to Bay Blvd. (SR 60), signalized on west side.

FLORIDA POWER FACILITIES:

- * Tower HD-34 50' east of property line.
- * Tower NC-51 60' west of property line.
- * Tower HD-35 50' east of property line.
- * Tower NC-50 60' west of property line.
- * Hot oil pipeline 15' west of property line.
- * FPC R/W = 250'





MAP #10 SOUTH AREA DETAILS S 18/29/16

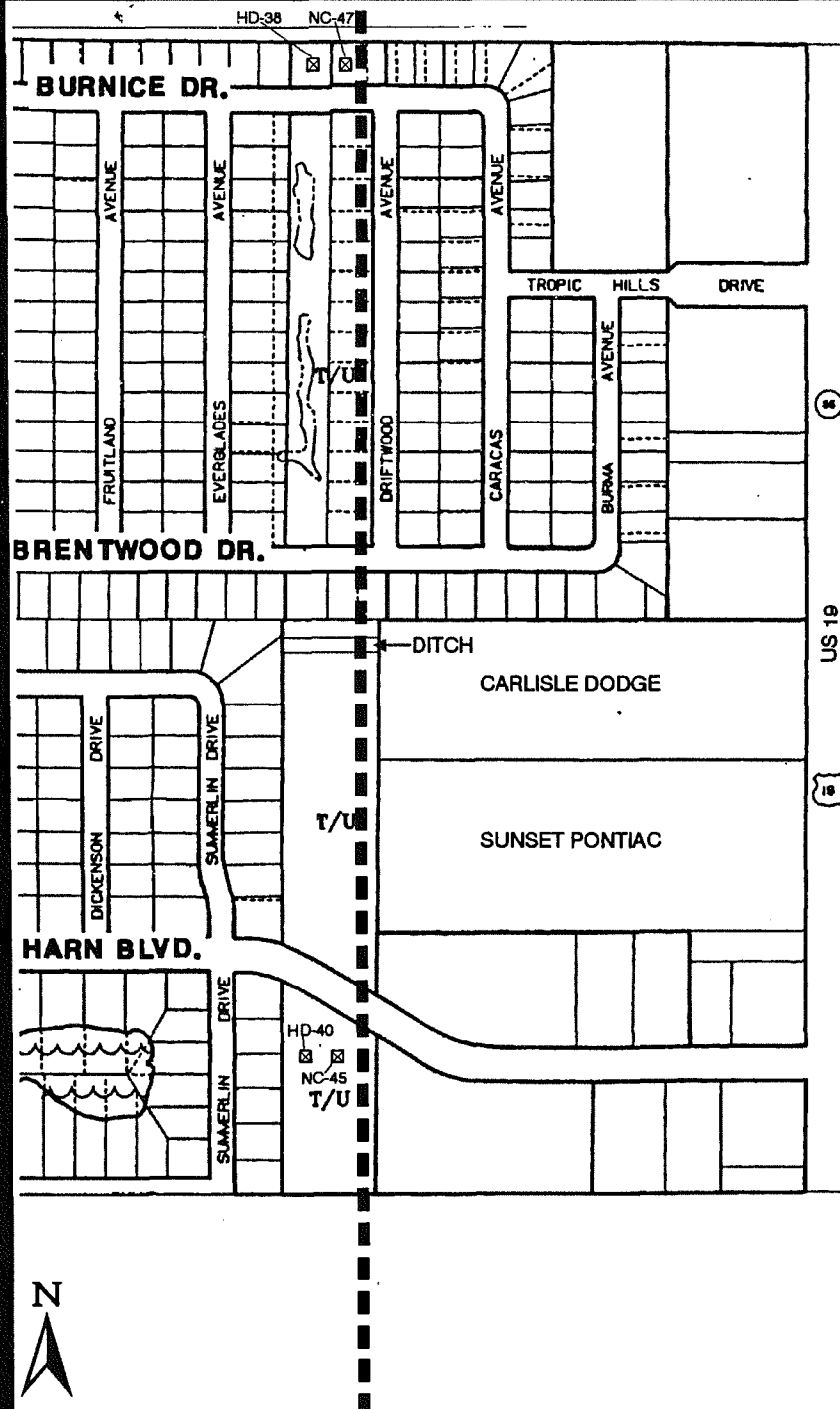
- * Trail to be located on east side, 15 feet from FPC hot oil line.
- * Ponds on south half, adequate area for Trail.
- * FPC owned.
- * Distribution line & street lights issue.
- * Trail to be located at least 15 feet from tower legs.

INTERSECTIONS:

- * Medium crossing at Druid Road.

FLORIDA POWER FACILITIES:

- * Tower HD-36 50' east of property line.
- * Tower NC-49 60' west of property line.
- * Tower HD-37 50' east of property line.
- * Tower NC-48 60' west of property line.
- * Hot oil pipeline 15' west of property line.
- * FPC R/W= 250'



MAP #11 NORTH AREA DETAILS N 19/29/16

- * Trail to be located on east side, 15 feet from FPC hot oil line.
- * Ponds on west side, adequate area for Trail exists.
- * Clearwater owned from one block north of Burnice Drive to one block south of Brentwood Drive/drainage area.
- * Relocate distribution lines to east side with street lights to make one circuit.
- * South of Brentwood Dr. ditch bridge is needed.

INTERSECTIONS:

- * Minor crossing at Burnice Drive, Brentwood Drive and Harn Blvd.

FLORIDA POWER FACILITIES:

- * Tower HD-38 53' east of property line.
- * Tower NC-47 82.5' west of property line.
- * Tower HD-40 53' east of property line.
- * Tower NC-45 82.5' west of property line.
- * Hot oil pipeline 15' west of property line.

MAP #11 SOUTH AREA DETAILS S 19/29/16

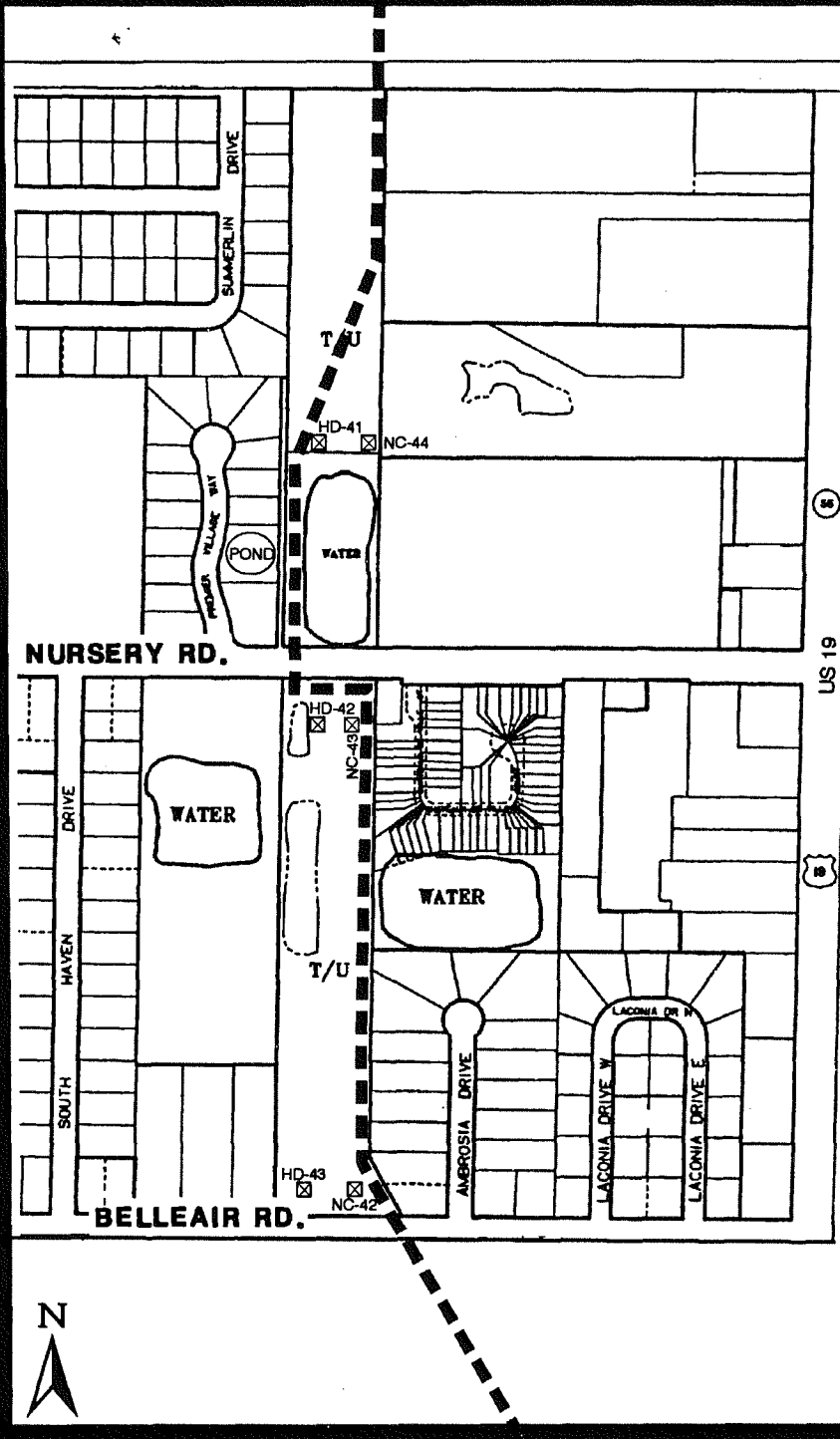
- * Trail to be located on east side.
- * Trail needs to transition with oil and water lines to west side around ponds just north of Nursery Road.
- * Parking on east side, north of lake (north of Nursery Road)- FPC to check on.
- * Possible transition for Belleair Road crossing.

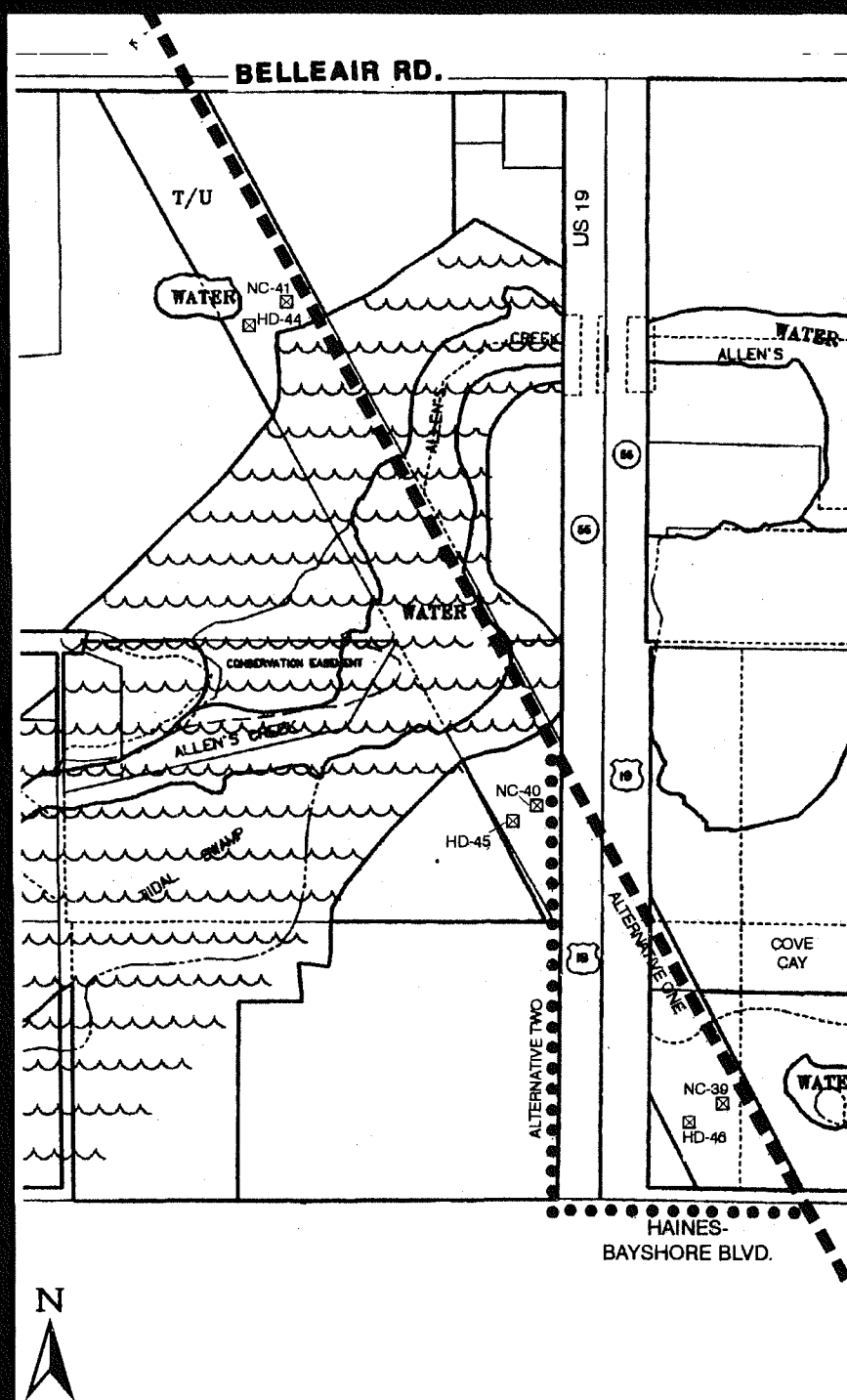
INTERSECTIONS:

- * Medium crossings at Nursery Rd. and Belleair Rd.

FLORIDA POWER FACILITIES:

- * Tower HD-43 55' east of property line.
- * Tower NC-42 82.5' west of property line.
- * Hot oil pipeline 10' west of property line, transitions to west north of Nursery Rd. And east south of Nursery Rd.
- * FPC R/W= 223'





MAP #12 NORTH AREA DETAILS N 30/29/16

* There are two route alternatives:

Alternative One: - - - - -

Along FPC property with overpass at US Hwy. 19.

A) Orange Blossom Groves is within FPC property between Allen's Creek and Belleair Rd.

B) Allen's Creek will require major boardwalk and design considerations.

C) Trail would provide access to recreation area on Allen's Creek and commercial kayak rental now in place.

Alternative Two:

Trail located south on west side of US Hwy. 19, at grade crossing at Haines Bayshore Blvd., then east on south side of Haines- Bayshore and south on east side of FPC ROW / easements .

INTERSECTIONS:

* Major crossing at U.S. 19 (SR 55) will require an overpass.

* Interim at grade crossing at Haines-Bayshore Road signal.

FPC FACILITIES:

* Tower HD-44 50' east of property line.

* Tower NC-41 87.5' west of property line.

* Tower HD-45 50' east of property line.

* Tower NC-40 87.5' west of property line.

* Tower HD- 46 50' east of property line.

* Tower NC-39 87.5' west of property line.

* Hot oil pipeline 15' west of property line, out of ROW at creek along west side of US 19 until it crosses east at Haines Bayshore Blvd.

* FPC R/W= 225'

MAP #13 SOUTH AREA DETAILS S 29/29/16

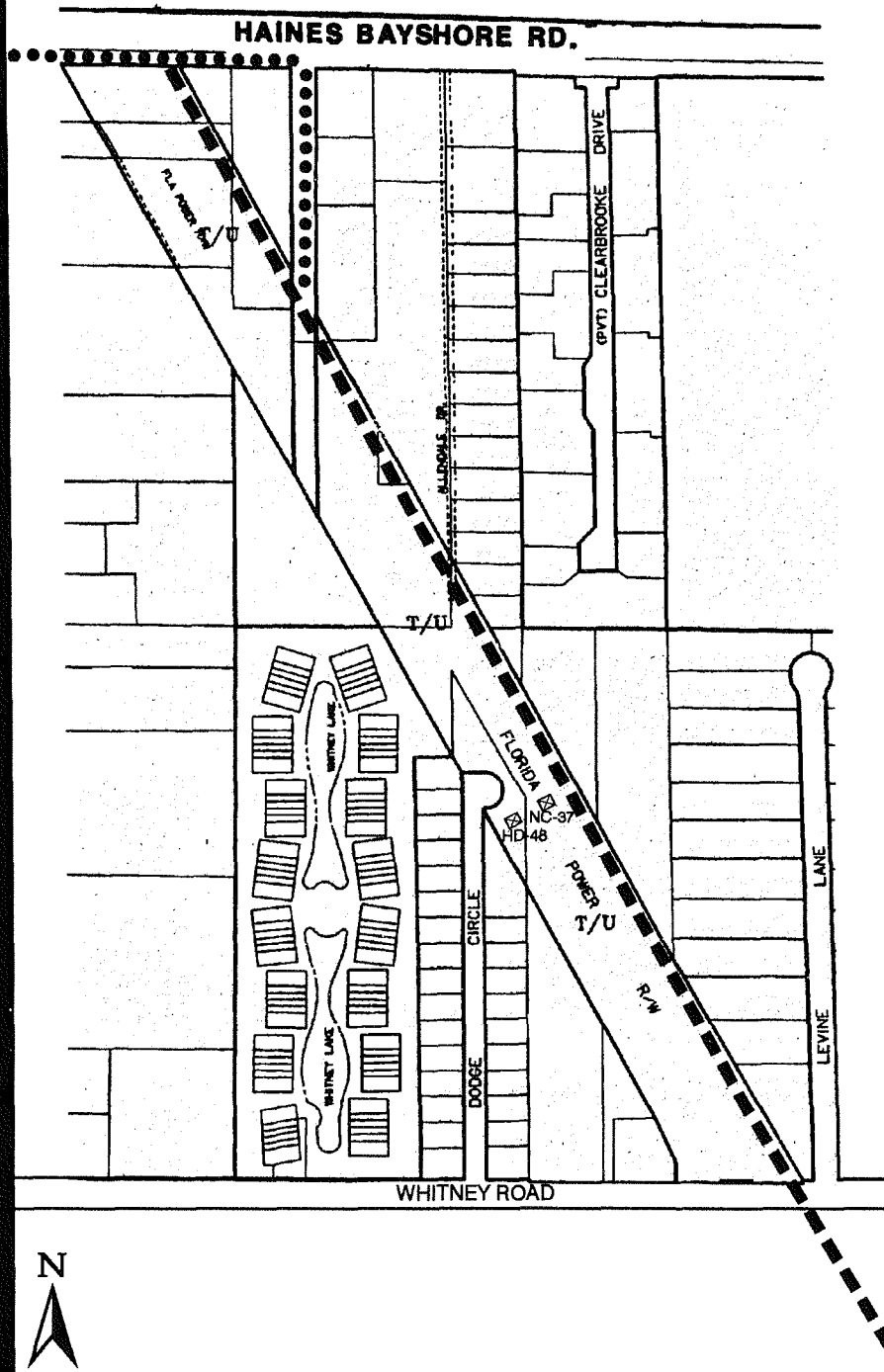
- * Trail to be located on east side at least 15 feet from FPC hot oil line.
- * Some private ownership.
- * Alternative route to use Cardinal Drive (public road) to access FPC property from Haines Bayshore.
- * Fence crossing property east to Dodge Circle.

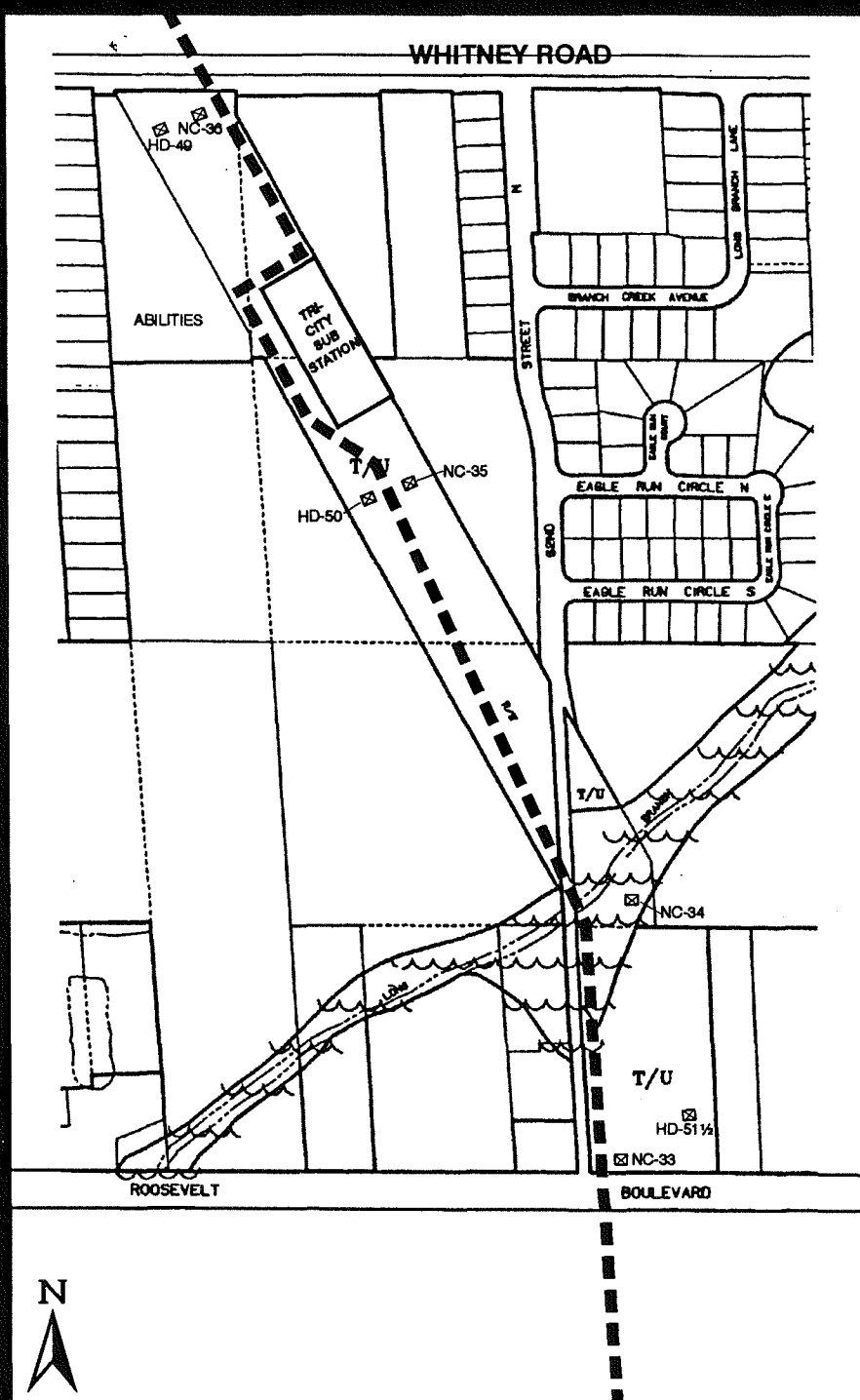
INTERSECTIONS:

- * Medium crossings at Whitney Road and Haines Bayshore.

FLORIDA POWER FACILITIES:

- * Tower HD-48 50' east of property line.
- * Tower NC-37 87.5' west of property line.
- * Hot oil pipeline 15' west of property line.
- * FPC R/W= 225'





MAP #14 NORTH AREA DETAILS N 32/29/16

- * At NC 36 Trail transitions from east side of FPC property to west side to avoid FPC tri-city substation, then around HD 50 Trail transitions to center of row at NC 34 and back to west side to cross canal.
- * Long Branch Creek crossing may require overpass.
- * Abilities uses FPC property for parking.
- * FPC owned.
- * Major distribution line conflict- prefer at grade crossing at Roosevelt Blvd.
- * Alignment change north of Tri-City Sub Station- 2/5/99.

INTERSECTIONS:

- * Major crossing at Roosevelt Blvd. (SR 686) with existing signal.

FPC FACILITIES:

- * Tower NC-36 87.5' west of property line.
- * Tower HD-49 50' east of property line.
- * Tower NC-34 87.5' west of property line.
- * Tower NC-33 87.5' west of property line.
- * Tower HD-51 1/2 50' east of property line.
- * Tower NC-35 87.5' west of property line.
- * Tower HD-50 50' east of property line.
- * Hot oil pipeline 15' west of property line.
- * FPC R/W=225'

MAP #14 SOUTH AREA DETAILS S 32/29/16

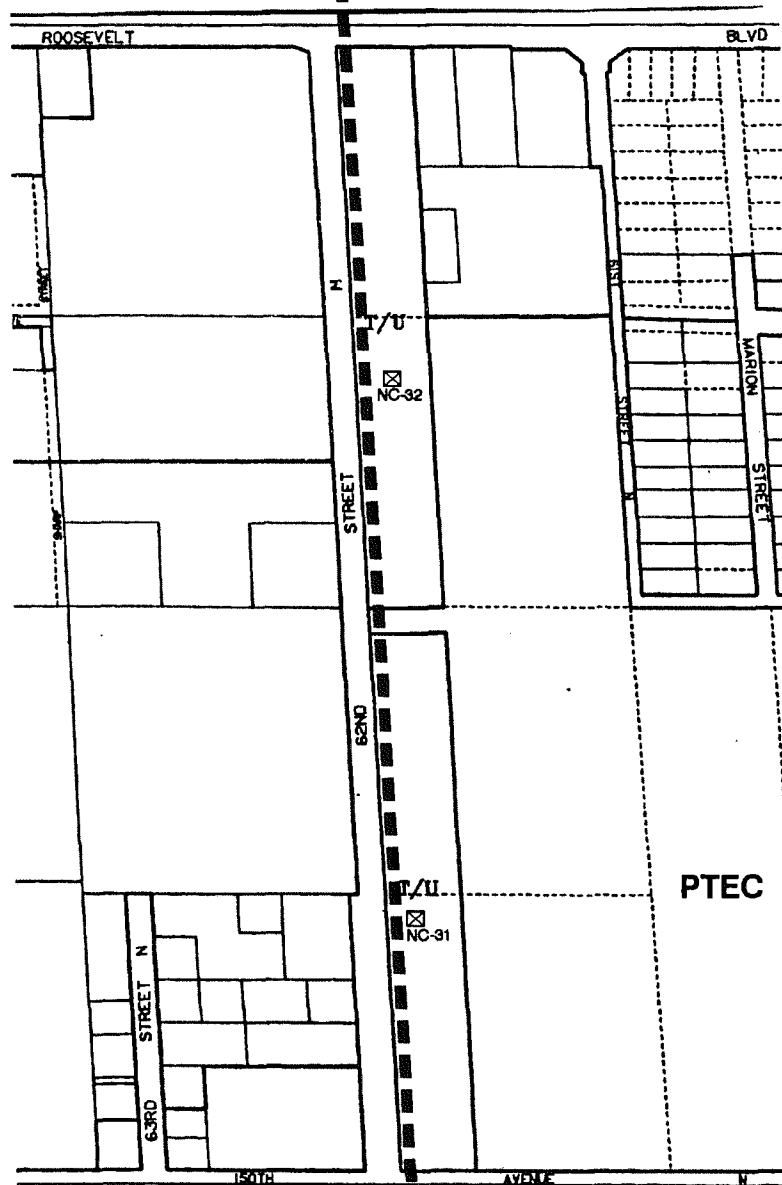
- * Trail to be located on west side.
- * PTEC currently uses FPC property for parking.
- * Hot oil line on east side.
- * Ownership?

INTERSECTIONS:

- * Medium crossing at 150th Avenue North
(Automobile Road).

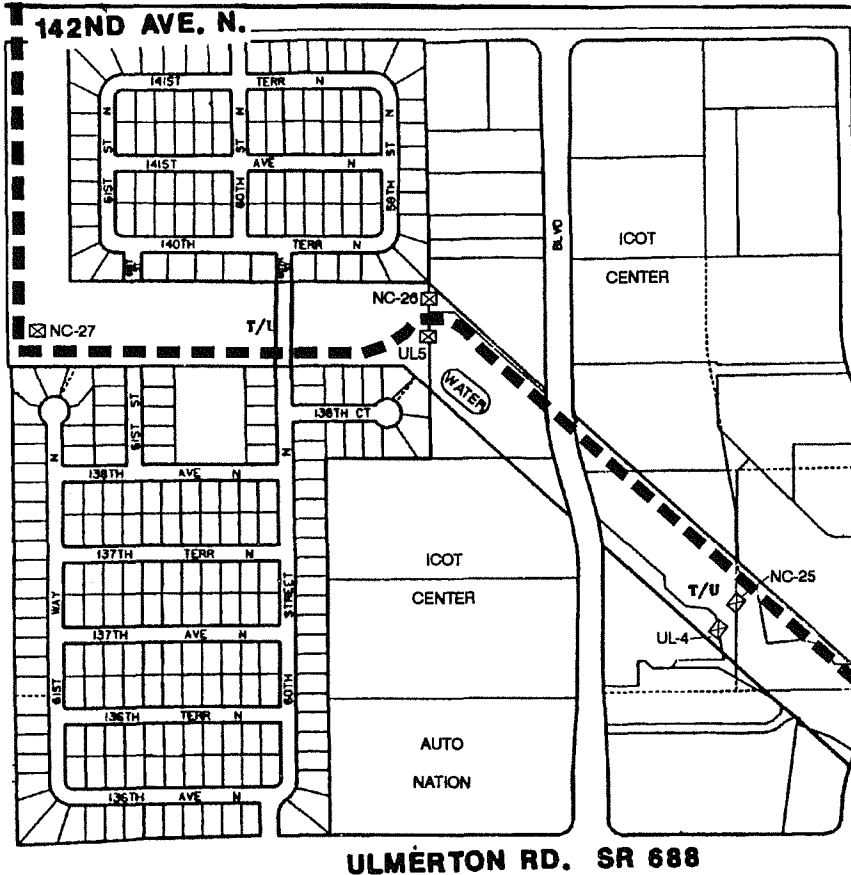
FLORIDA POWER FACILITIES:

- * Tower NC-32 87.5' west of property line.
- * Tower NC-31 87.5' west of property line.
- * Hot oil pipeline 15' west of property line.
- * FPC R/W = 175'



- * Tower NC-30 87.5' west of property line.
- * Tower NC-29 87.5' west of property line.
- * Tower NC-28 87.5' west of property line.
- * Hot oil pipeline 15' west of property line.
- * FPC R/W= 175'





* MAP IS SMALLER SCALE.

MAP #15 SOUTH AREA DETAILS S 5/30/16

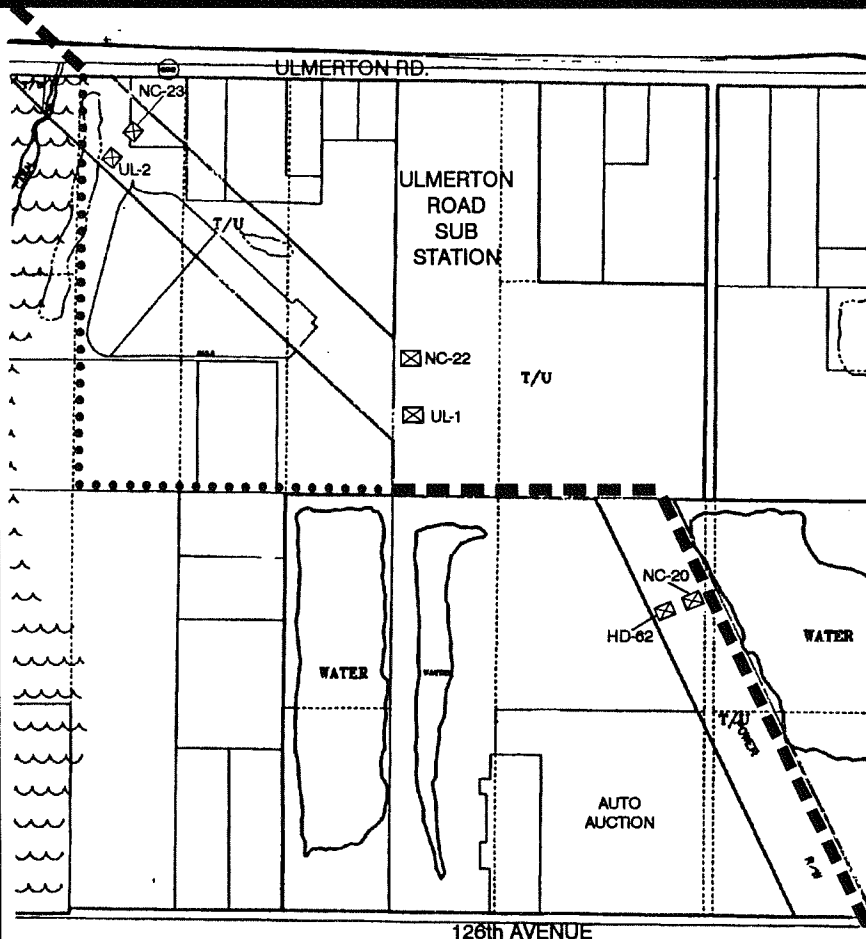
- * Trail transitions to center, then east side of FPC property to avoid ICOT ponds.
- * Private ownership (ICOT).

INTERSECTIONS:

- * Minor crossings at 60th Street, 61st Street and Icot Blvd.
- * Major crossing at Ulmerton Road (SR 688).
Ulmerton Road will require an overpass or alignment to existing signal.

FLORIDA POWER FACILITIES:

- * Tower NC-27 87.5' west of property line.
- * Tower NC-26 87.5' west of property line.
- * Tower NC-25 87.5' west of property line.
- * Tower UL-4 87.5' east of property line.
- * Hot oil pipeline 15' west of property line.
- * FPC R/W= 175'



126th AVENUE



* MAP IS SMALLER SCALE.

MAP #16 NORTH AREA DETAILS N 9/30/16

- * Area between Ulmerton Road (SR 688) and Ulmerton substation may be privately owned.
- * Trail off of FPC easement along east side of drainage canal, west of development.

INTERSECTIONS:

- * Minor crossing at 126th Avenue.

FLORIDA POWER FACILITIES:

- * Tower NC-23 87.5' west of property line.
- * Tower UL-2 87.5' east of property line.
- * Tower UL-1 on substation property.
- * Tower NC-22 on substation property.
- * Tower NC-20
- * Tower HD-62
- * Hot oil pipeline 15' west of property line.
- * FPC R/W= 262.5'

MAP #16 SOUTH AREA DETAILS S 9/30/16

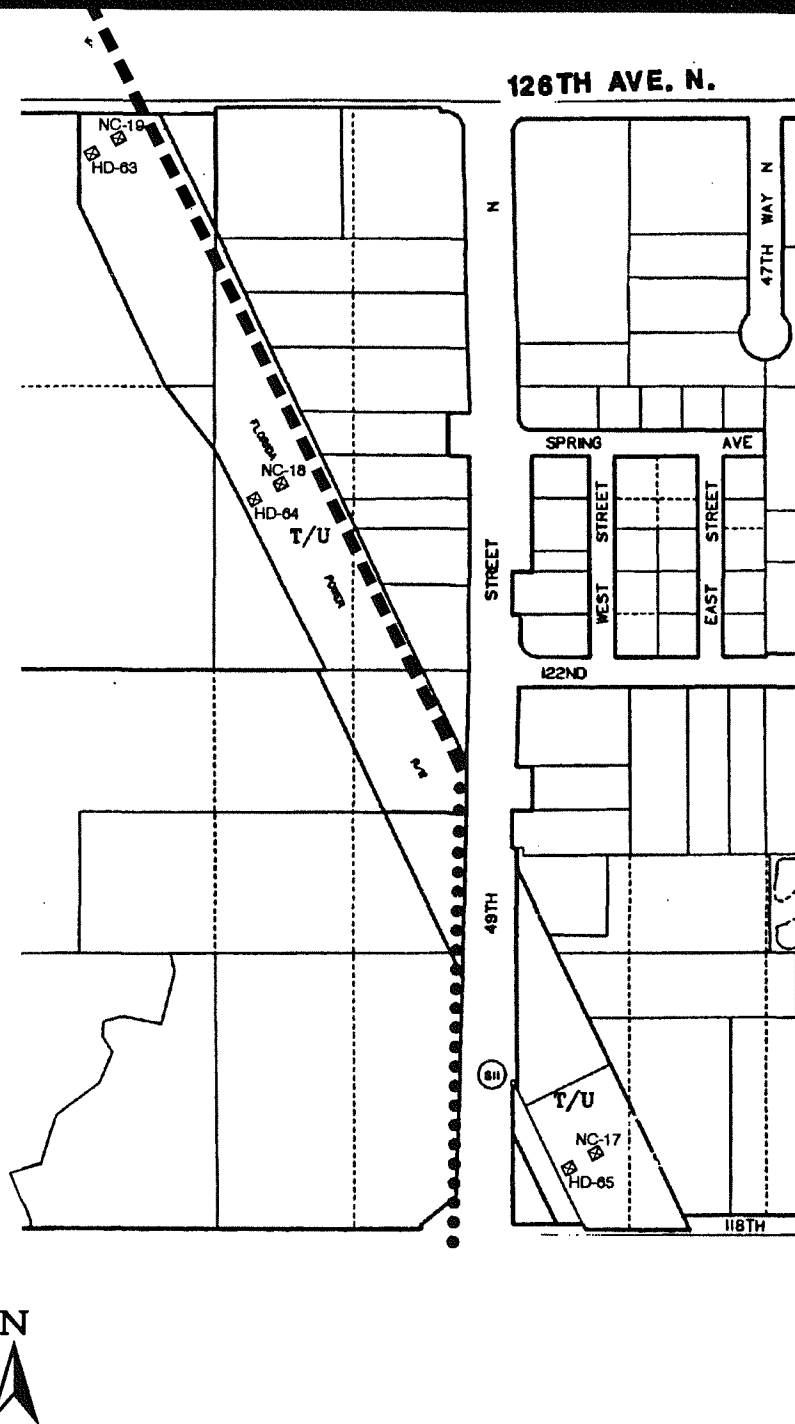
- * Trail to be located on east side of FPC property.
- * Trail will continue south along west side of 49th Street and then along the south side of 118th Avenue with an at grade crossing at the signalized intersection.

INTERSECTIONS:

- * Major crossings at 49th Street North and 118th Avenue North at existing signal.

FLORIDA POWER FACILITIES:

- * Tower NC-19
- * Tower HD-63
- * Tower NC-18
- * Tower HD-64
- * Tower NC-17
- * Tower HD-65
- * Hot oil pipeline 15' west of property line.
- * FPC R/W=



MAP #17 NORTH AREA DETAILS N 16/30/16

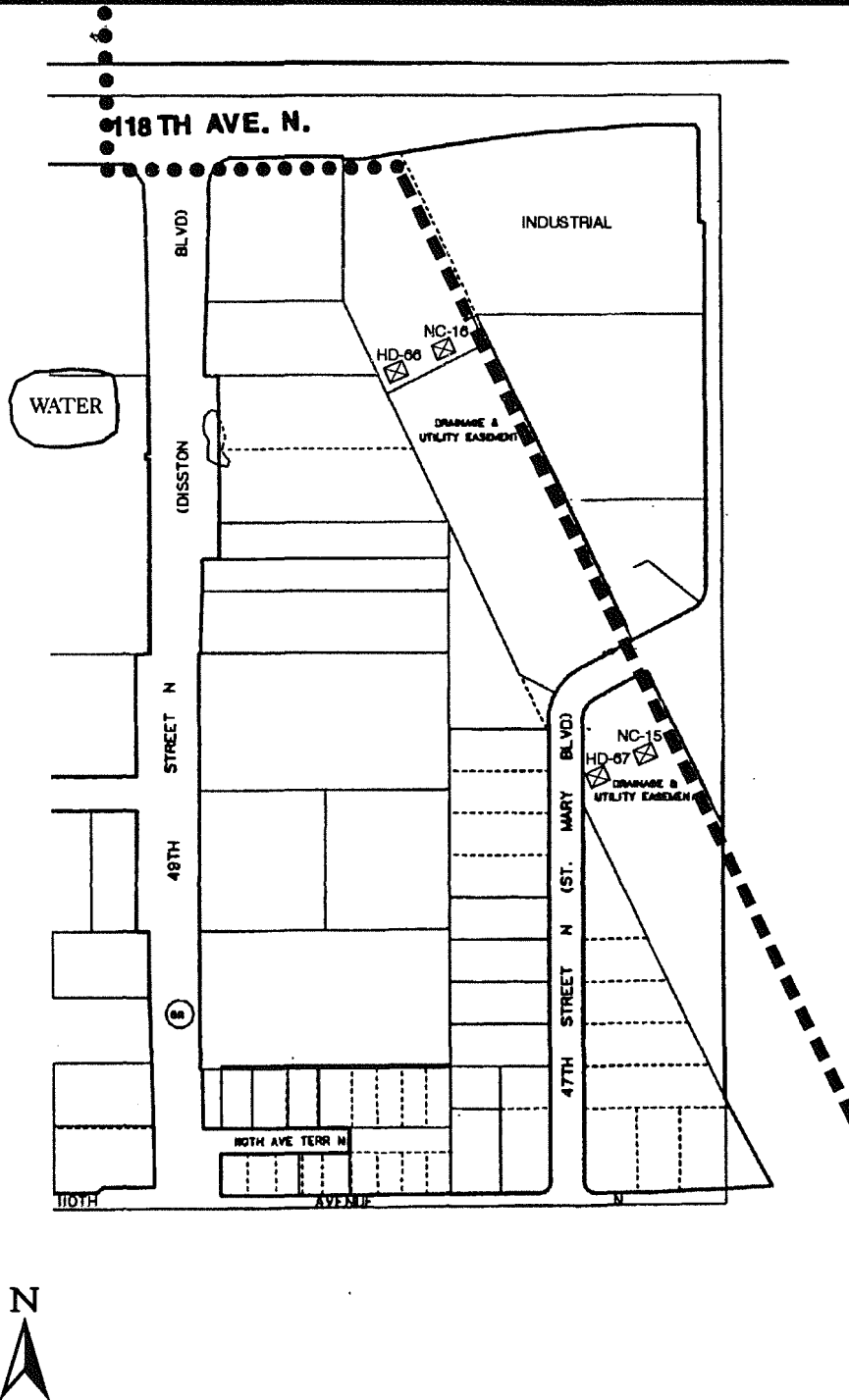
- * Trail along south side of 118th Avenue until it continues south on east side of FPC property.

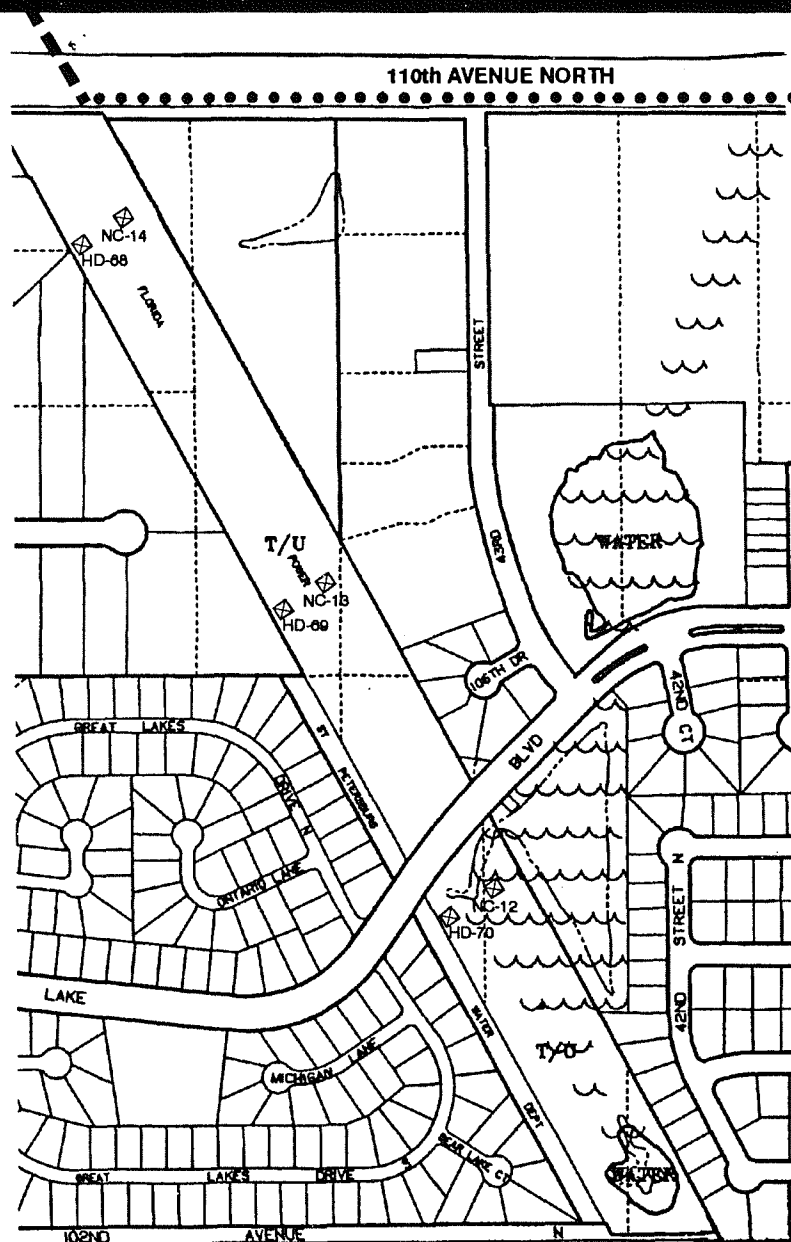
INTERSECTIONS:

- * Minor crossings at 110th Avenue North and 47th Street North.

FLORIDA POWER FACILITIES:

- * Tower NC-16
- * Tower HD-66
- * Tower NC-15
- * Tower HD-67
- * Hot oil pipeline 15' west of property line.
- * FPC R/W=





MAP #18 WEST AREA DETAILS S 15/30/16

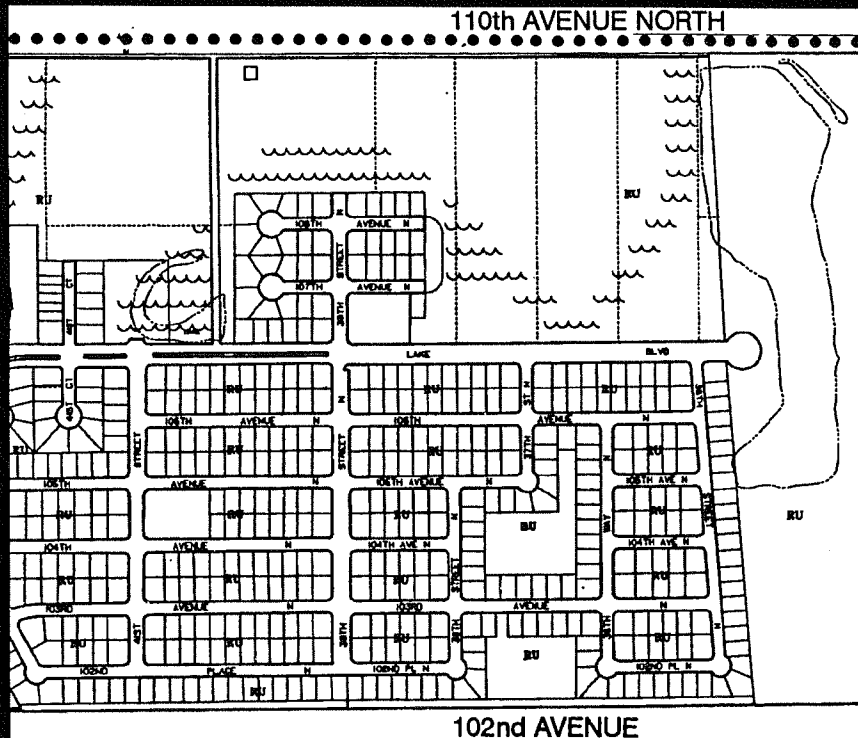
* Trail to turn east at 110th Avenue on County property.

INTERSECTIONS:

* None.

FLORIDA POWER FACILITIES:

- * Tower NC-14
- * Tower HD-68
- * Tower NC-13
- * Tower HD-69
- * Tower NC-12
- * Tower HD-70
- * Hot oil pipeline 15' west of property line.
- * FPC R/W=



MAP #18 EAST AREA DETAILS S 15/30/16

- * Trail along County property (110th Avenue alignment) to avoid Mainland and the Lakes.
- * Trail continues along County property (34th Street alignment) south until accesses FPC easement.

INTERSECTIONS:

- * No major or minor crossings.

FLORIDA POWER FACILITIES:

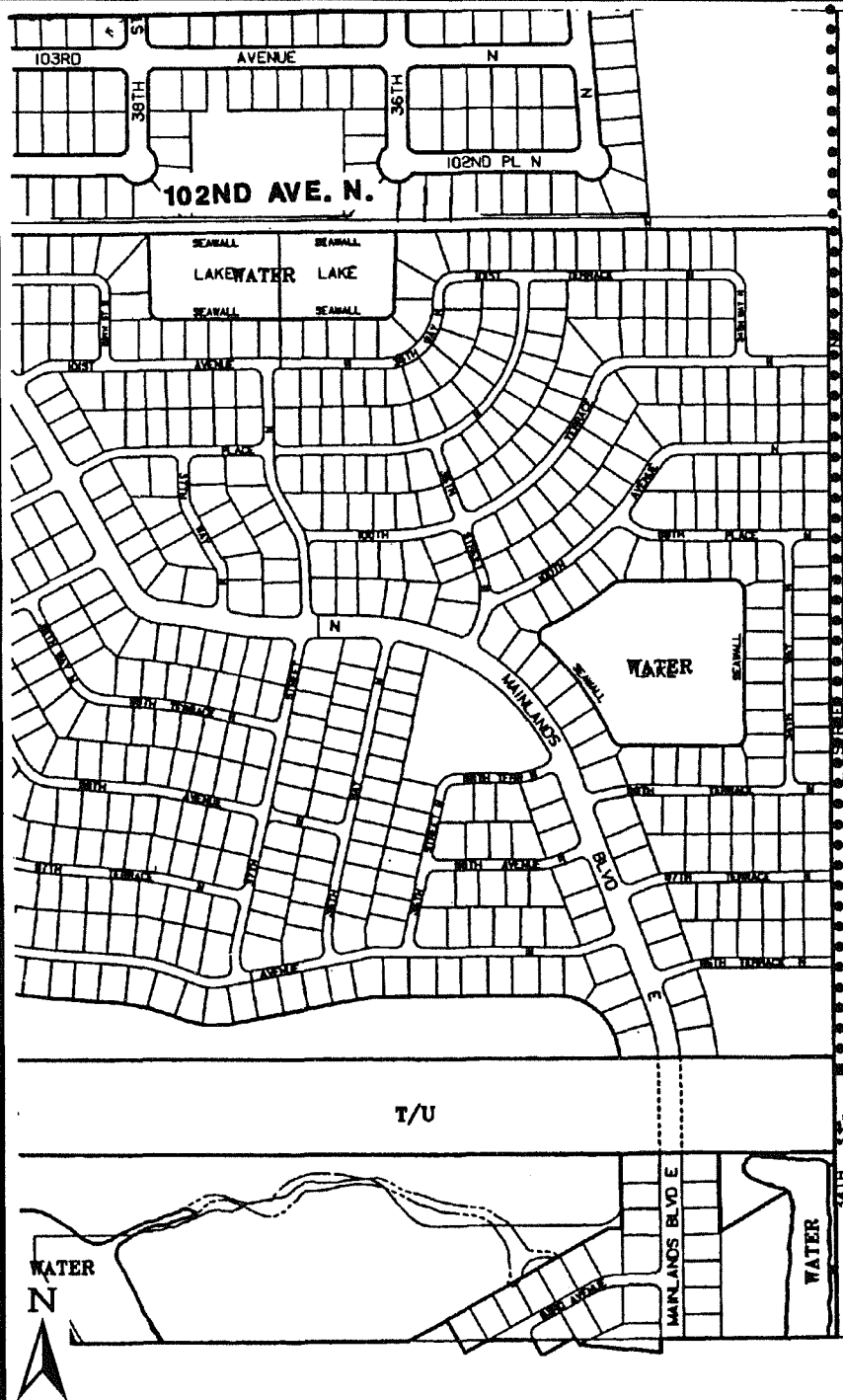
- * None.

MAP #19 NORTH AREA DETAILS N 22/30/16

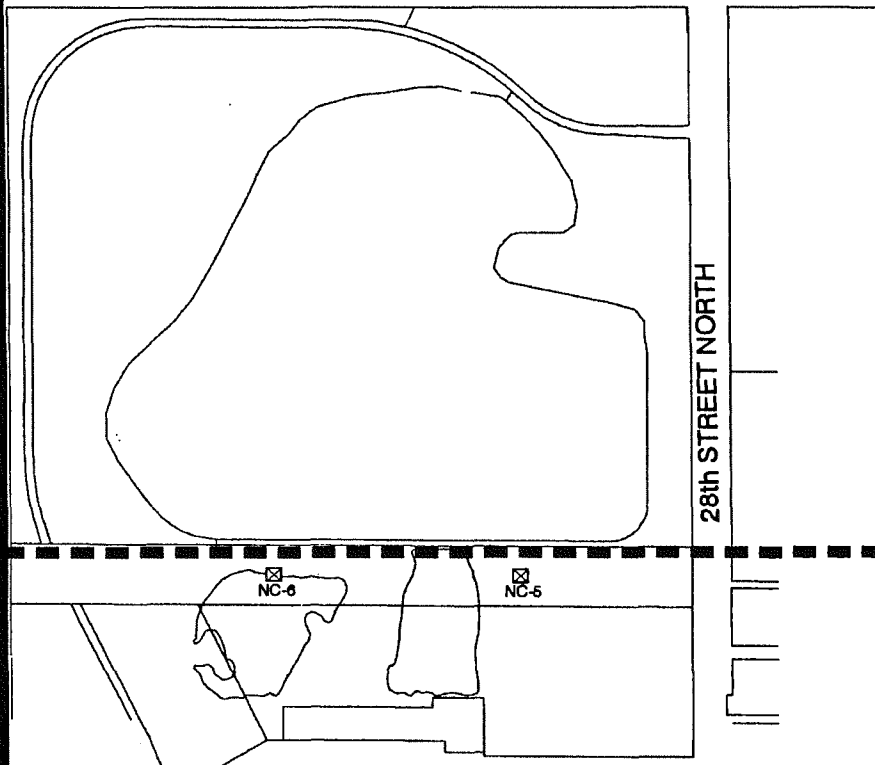
- * Trail runs alongside 34th Street back to FPC property to continue east.
- * Off FPC property around Mainlands golf community.

INTERSECTIONS:

- * No major or minor crossings.



102nd AVE. N.



* MAP IS SMALLER SCALE.

MAP #20 WEST AREA DETAILS N 23/30/16

* Trail to be located on north side of FPC
Property

INTERSECTIONS:

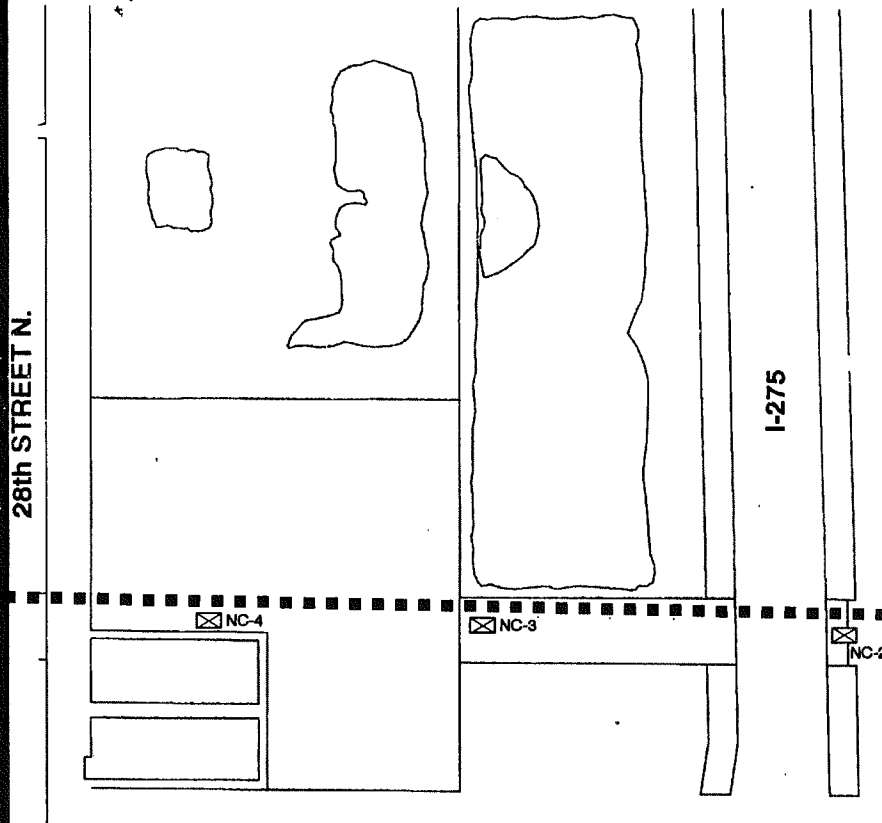
* Medium crossing at 28th Street North.

FLORIDA POWER FACILITIES:

- * Tower NC-6 87.5' south of property line.
- * Tower NC-5 87.5' south of property line.
- * Hot oil pipeline 15' south of property line.
- * FPC R/W= 225'

MAP #20 EAST AREA DETAILS N 23/30/16

- * Trail to be located north of the FPC easement along County owned 40' right of way..
- * 28th Street North to east of I-275 on additional 40' right of way.



* MAP IS SMALLER SCALE.

INTERSECTIONS:

- * Major crossing at I-275 will require overpass.

FLORIDA POWER FACILITIES:

- * Tower NC-4
- * Tower NC-3
- * Tower NC-2
- * Hot oil pipeline
- * FPC R/W=40'.

MAP #21 WEST AREA DETAILS N 24/30/16

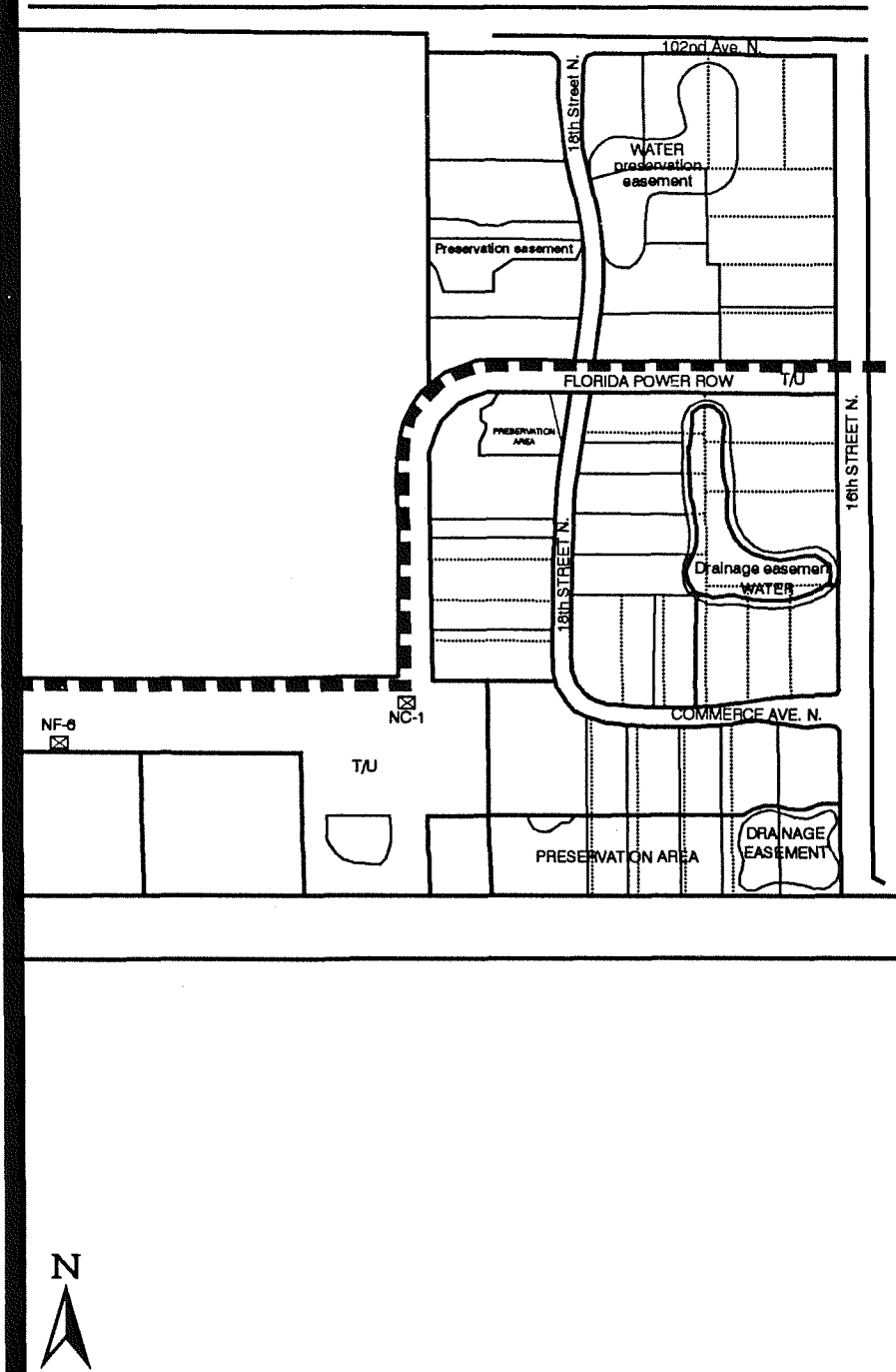
- * Trail to be located on north and west sides of FPC easement.
- * Use of Toytown as a possible Trail related park.
- * Power lines underground.

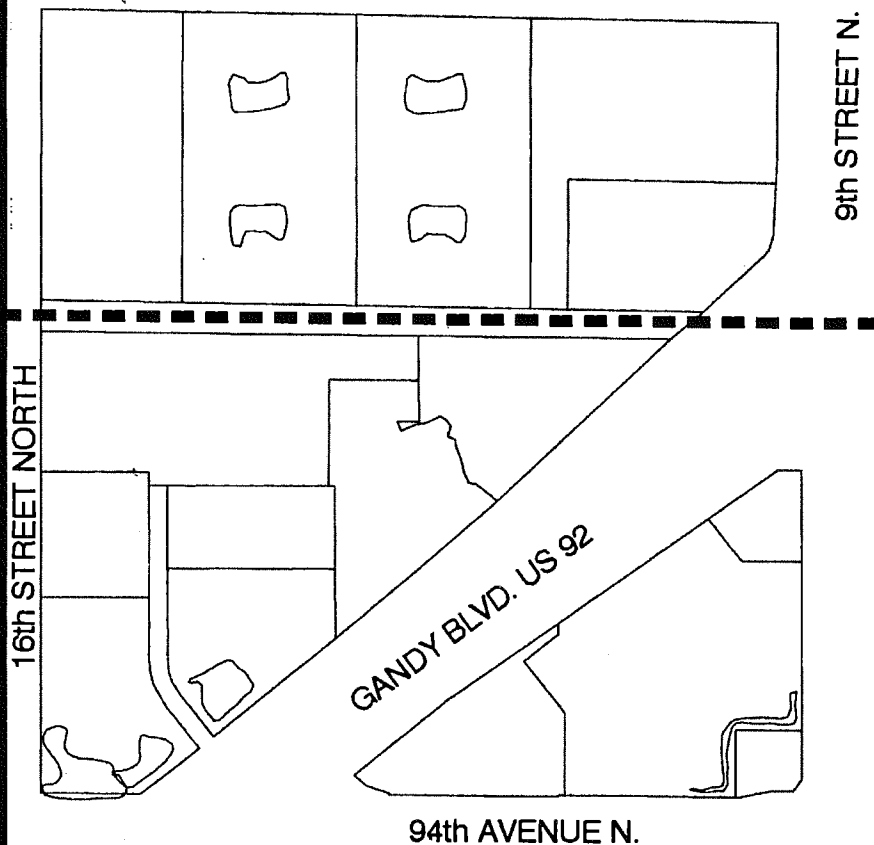
INTERSECTIONS:

- * Minor crossings at 18th Street and 16th Street.

FLORIDA POWER FACILITIES:

- * Tower NC-1
- * Tower NF-6
- * Hot oil pipeline 15' south of east of property line.
- * FPC R/W=





MAP #21 EAST AREA DETAILS N 24/30/16

- * Power lines underground.
- * Trail to be located between hot oil pipeline on north side and middle of FPC ROW.

INTERSECTIONS:

- * Major crossings at Gandy Blvd. (US 92) and 9th Street North. (Both will require special treatment.)

FLORIDA POWER FACILITIES:

- * Hot oil pipeline 15' south of property line.
- * FPC R/W=



MAP #22 WEST AREA DETAILS N 19/30/17

- * Underground power lines.
- * Trail to be located between hot oil pipeline on north side and middle of FPC ROW.

INTERSECTIONS:

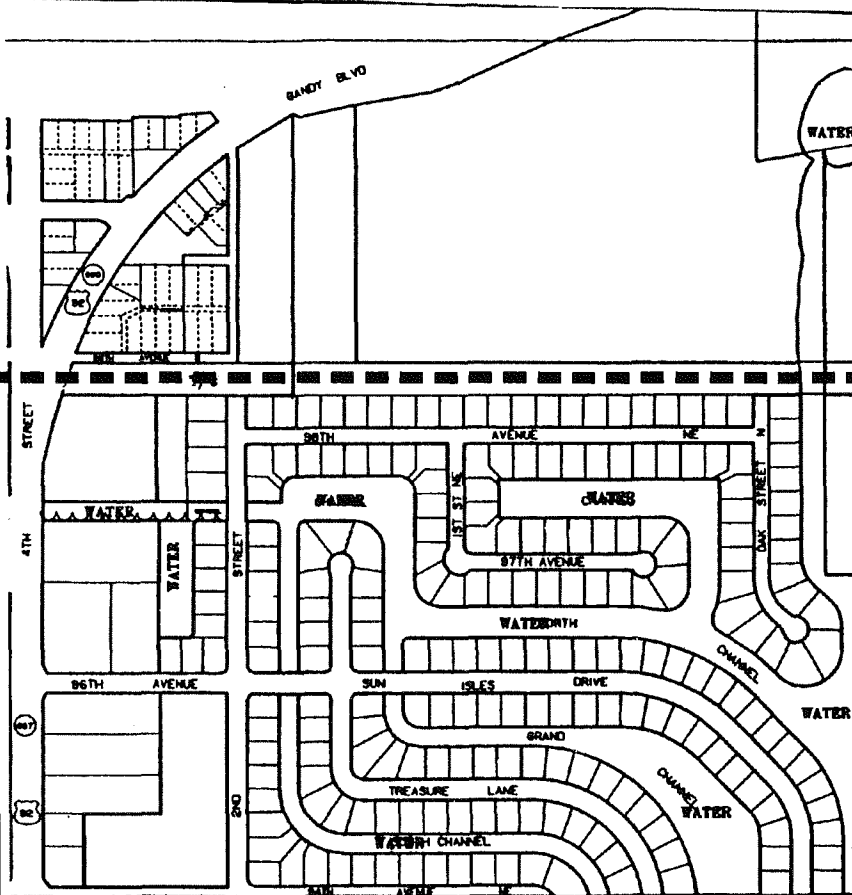
- * Major crossing at 4th Street North. (May Require overpass.)**
*** Minor crossing at 5th Street North.**

FLORIDA POWER FACILITIES:

- * Hot oil pipeline on north side of R/W.
- * FPC R/W= 100'

MAP #22 EAST AREA DETAILS N 19/30/17

- * Underground power line location.
- * Trail to be located between hot oil pipeline on north side and middle of FPC ROW.



* MAP IS SMALLER SCALE.

INTERSECTIONS:

- * Minor crossing at 2nd Street North.

FLORIDA POWER FACILITIES:

- * Hot oil pipeline on north side of R/W.
- * FPC R/W= 100'

MAP #23 WEST AREA DETAILS N 20/30/17

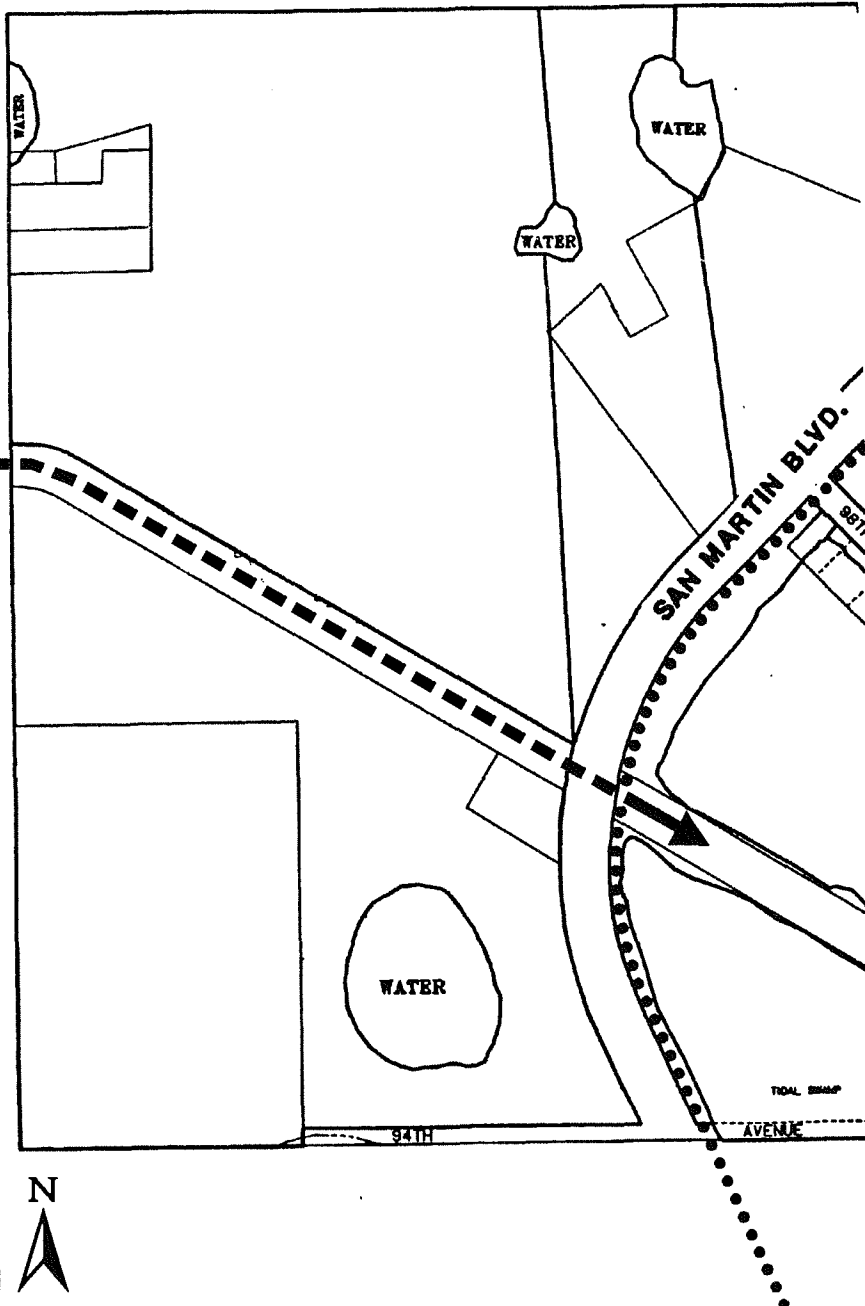
- * Connects to San Martin Blvd. to connect to the Friendship Trail to the north and the Weedon Island Preserve to the east.
- * Connects to St. Petersburg's planned 1st Street bike path which connects to the southern End of the Pinellas Trail.
- * Trail to be located between hot oil pipeline on north side and middle of FPC ROW.
- * Evaluate best access routes to Weedon Island.

INTERSECTIONS:

- * No major or minor crossings.

FLORIDA POWER FACILITIES:

- * Hot oil pipeline on north side of R/W.
- * FPC R/W = 100'



SUMMARY

Florida Power Corridor/Bicycle Pedestrian Trail
April 28, 1998

Attendees:

George Townsend	FPC Chairman ROW Utilization Committee	(407) 475-2239
Jim Talbot	Florida Power Corp, Inc.	(407) 475-2223
Ada West	Florida Power Corp, Inc.	(407) 475-2242
Victoria Bucher	Florida Power Corp, Inc. Property Agent	(407) 475-2454
Randy Strain	Florida Power Corp, Inc.	(407) 384-7953
Brian Smith	MPO Director (County Planning Director)	(813) 464-4751
Bert Valery	BAC Chairman	(813) 517-8888
Tom Kennedy	Florida Power Corp, Inc.	(813) 519-2416
Ken Nosworthy	FDOT	(813) 975-6426
Gina Harvey	MPO (County Planning Department)	(813) 464-4751

On April 28, 1998 at 1:30 p.m. at the Florida Power Corporation (FPC) Headquarters located in Maitland Florida, Pinellas County Planning staff and MPO representatives met with the FPC Rights-of-Way (ROW) Utilization Committee to discuss constructing a bicycle/pedestrian Trail within the eastern north/south FPC corridor in Pinellas County.

The FPC ROW Utilization Committee is responsible for safety and the protection of the use of their property. Due to the ever expanding development in the State, acreage is very limited for additional utility corridors, emphasizing the importance of maintaining the existing FPC property.

Pinellas County representatives briefed the ROW Committee on the events leading up to this meeting. The goal is to construct a trail within the FPC ROW which would connect the southeastern end of the Pinellas Trail, located near McMullen-Booth Road (C.R. 611) and Tampa Road, with the Friendship Trail, located on the Old Span of the Gandy Bridge. This will also connect up to the Weedon Island Preserve located south of the Friendship Trail. When completed, the trail system would effectively loop Pinellas County.

Brian Smith, Planning Director, presented to the FPC ROW Committee the documents and maps related to the project. The initial part of the presentation was the introduction of the Future Bikeways Plan, which displays existing and future bicycle/pedestrian facilities and how they would be connected to the FPC corridor. He noted Pinellas County's experience with the Pinellas Trail and how that would be useful in managing this FPC corridor as a trail. The Pinellas County Existing Land Use Map was also briefly discussed in relating land use types to the location of the FPC corridor.

Included in the packet of documents distributed at the meeting was an overview of the ownership of the FPC corridor, such as whether the property was fee owned by FPC or accessed by an easement. A regional map of Pinellas and Hillsborough Counties was made available to the Committee in order to display the importance the FPC corridor in connecting the Pinellas Trail to the Friendship Trail (Old Gandy Bridge Program).

The most significant part of the presentation was a display of aerial maps of the entire FPC corridor from the Weedon Island Preserve north to the southeastern extension of the Pinellas Trail. The aerials were numbered to correspond with a reference map for easy locating. These maps were marked with the approximate location of the edges of the FPC corridor ROW and with the location of the towers as viewed from the aerials.

The goal of the meeting was to establish an acceptable understanding for locating the trail within the FPC ROW. The attendees reviewed each of the full section aerials of the corridor, which included locating substations, parking and other structures within the ROW, drainage ponds, road crossing locations, and major trip generators that could benefit from the trail, such as St. Petersburg Junior College and the Vocational Technical Institute.

It was noted that several other uses are currently located within the FPC corridor including various city and County water lines and FPC hot oil lines necessary for the Anclote Power Plant. In order to protect the area available within the ROW for future FPC use, it was requested that any trail facility be located near or over existing users. One alternative recommended was locating the trail over the County water lines since this would not reduce land available for future FPC use and still provide a trail facility for the public within the corridor. For the majority of the corridor, the water lines are located near the edge of the ROW. This was actually the preferred location for FPC and Pinellas County in order to keep the trail away from the towers located near the middle of the corridor.

Pinellas County emphasized the need to maintain or provide a buffer area of vegetation between the neighboring areas and the trail. FPC agreed but was not receptive to any permanent structures since they could block emergency access to the corridor. It was agreed by those in attendance that FPC access to the corridor would continue to be the priority but some vegetation could be installed in areas with the understanding that, if necessary, it would be removed for FPC access.

It was agreed that the following information would be provided before the next scheduled meeting:

- FPC will provide the location of the existing hot oil lines
- Pinellas County will provide the locations and jurisdiction of the existing water lines, both active and abandoned

The cross section of the trail was also discussed with the Committee. Pinellas County had anticipated that the trail would be constructed similar to that of the Pinellas Trail. The Pinellas Trail currently utilizes two methods; a 20-foot separated 10-foot/5-foot section divided by a 5-foot grass section and a 15-foot cross section, separated by paint striping only. FPC preferred the use of the 15-foot section since the land area needed would be less than that of the 20-foot section.

George Townsend then concluded that a field trip by the FPC ROW Utilization Committee was needed for a first assessment. He suggested two weeks from this meeting date, which was May 12. Everyone was in agreement with the group to meet at 9:30 a.m. at the Publix Shopping Center located north of Tampa Road on McMullen-Booth Road to survey the corridor.

George then gave Brian Smith draft copies of a permit agreement and Memorandum of Understanding in order for the documents to be reviewed by the Pinellas County attorneys.

The meeting was concluded and adjourned at approximately 4:00 p.m.

Florida Power Easement Notes

Field Review

May 12, 1998

9:30 a.m. - 4:30 p.m.

Attendees:

George Townsend	Florida Power Corporation Right-of-Way Utilization Committee Chairman	(407) 475-2239
Ada West	Florida Power Corporation Right-of-Way Utilization Committee	(407) 475-2242
Victoria Bucher	Florida Power Corporation Land Agent	(407) 475-2454
Randy Strain	Florida Power Corporation Engineering	(813) 384-7953
Belisa Oliveira	Florida Power Corporation Legal Team	(813) 866-5110
Keith Keller	Florida Power Corporation Transmission Design	(407) 475-2247
Brian Smith	Pinellas County MPO	(813) 464-4751
Bert Valery	BAC Chairman	(813) 517-8888
Amy Dierolf	Florida Power Corporation Environmental	(813) 866-4327
Gina Harvey	Pinellas County MPO	(813) 464-4751
Ken Nosworthy	FDOT	(813) 975-6426
Joe DeMoss	Pinellas County Engineering	(813) 464-3631

Maps 1 and 2 8 or 9-28-16 (South end of Pinellas Trail to C.R. 95)

The Florida Power Corporation (FPC) Trail connects with the Pinellas Trail west of McMullen-Booth Road (C.R. 611) and south of Tampa Road (C.R. 752). The FPC Trail should be positioned on the north side of the easement to allow for future expansion of the existing FPC lines on the south side. At the pond location, the trail could be located south of the first tower before proceeding west. The water lines are located on the south side of this easement between McMullen-Booth Road and Catalina Drive. This property appears to be privately owned. No major road crossings on this segment.

Map 3 17 or 18-28-16 (C.R. 95 to Curlew Road)

FPC substation is located on the west side of the right-of-way. The substation area appears to be fee owned by FPC. The FPC Trail should be located on the east side of the property. FPC will check with design on location of any possible future substation expansion. On the south end of the substation property, a fence blocks a trailer storage area. The owner will need to be contacted and requested to relocate trailers to allow FPC Trail access and alignment. This property appears to be privately owned. As the easement turns south, water lines join and continue on the east side. The

FPC Trail should continue on the east side of the easement and east of the pond located just south of Curlew Road. Just south of the pond, the property is fee owned by FPC. When the property turns west, the FPC Trail would continue on the south side between the County water lines and the property line. Significant road crossing for the FPC Trail at Curlew Road (S.R. 586), which is to be a six-lane State Roadway.

Map 5 19 or 20-28-16
(Curlew Road to Pine Hammock Boulevard)

FPC hot oil line is located on the east side of the easement. Water line transitions to the west side of the easement. FPC policy requires all structures to be a minimum of 15 feet from the hot oil line. The FPC Trail to continue on east side along, but beyond, the 15-foot minimum from the hot oil line. No significant road crossings.

Map 8 29 or 30-28-16
(Pine Hammock Boulevard to Enterprise Road)

FPC Trail to continue on the east side of the easement. Major roadway crossing at S.R. 580, a six-lane State Roadway. The FPC Trail to be located on the east side of Countryside Mall Theater parking lot, just south of S.R. 580. A future overpass may be needed and FPC will review wire clearance due to thermal sag and blow out. The FPC Trail on the east side also crosses Countryside Boulevard, a six-lane roadway, and Enterprise Road, a four-lane roadway, both the jurisdiction of the City of Clearwater.

Map 9 32-28-16
(Enterprise Road to U.S. 19)

FPC Trail continues on the east side of the right-of-way. It should transition to the north side and FPC right-of-way turns west. This will provide adequate ramp space for a future overpass at U.S. 19. U.S. 19 is a six-lane State Roadway and a major crossing location for the FPC Trail. FPC hot oil line is located on the south side.

Map 10 31-28-16
(U.S. 19 to Union Street)

FPC Trail turns to the south and is being used by the Top of the World Golf Course. The FPC Trail should be located on the east side of the right-of-way until it reaches the southern end of the golf course. The FPC Trail should then angle to the west to continue south on the west side of the right-of-way. This will assure the trail is an adequate distance from the FPC substation located on the east side of the right-of-way. No roadway crossings on this segment.

Map 11 **6-29-16**
(Union Street to north of NE Coachman Road)

FPC Trail continuing on the west side of the right-of-way. Road crossing at Sunset Point Road, a four-lane County Roadway. FPC hot oil line located on the east side of the easement. A segment of the property south of Sunset Point Road is owned by Pinellas County, which has constructed drainage ponds to mitigate the Sunset Point Road widening. Ponds are fenced with chain link. A review of the replacement of the chain link fence to something more fitting to the Trail should be done. Adequate room between the County ponds and the west property line for the FPC Trail exists.

Map 12 **7-29-16**
(North of NE Coachman Road to Drew Street)

FPC Trail continues on the west side of the right-of-way. Significant road crossing at NE Coachman Road, a two-lane County Road. FPC hot oil line is located on the east side of the right-of-way. Railroad crossing on this segment south of NE Coachman Road. Trail approaches the Clearwater Recreation Complex located on the east side of the FPC right-of-way but has constructed walking path and parking within the FPC right-of-way. Consideration may be given to use the center of the right-of-way between the poles. As Old Coachman Road angles in, it is preferred to transition the FPC Trail to the east side of the right-of-way. North of Drew Street, Time Warner parking facility is in the FPC right-of-way. Significant road crossing at Drew Street, a four-lane County Road. A traffic signal is located on the edge of the FPC right-of-way at Old Coachman Road.

Map 13 **18-29-16**
(Drew Street to Japanese Garden Mobile Home Park)

PROBLEM AREA: Old Coachman Road is located on the west side; FPC hot oil line on the east side; and St. Petersburg Junior College parking is located between. This area will require additional review. Water lines on the east side. This segment has significant road crossing at Gulf-to-Bay Boulevard (S.R. 60), a six-lane State Road. A traffic signal is located on the west side of the FPC right-of-way at Old Coachman Road. South of Gulf-to-Bay Boulevard, the FPC Trail continues on the east side of the right-of-way and the east side of the existing ponds.

Map 14 **19-29-16**
(Japanese Garden Mobile Home Park to Belleair Road)

FPC Trail continuing on the east side of the right-of-way. Minor road crossing at Harn Boulevard, a two-lane Clearwater road. North of Nursery Road, hot oil and water lines transition to the west of the right-of-way to avoid pond, south of Nursery Road, both located back to the east side to avoid ponds on the west side of the right-of-way. If the FPC Trail is located between the pond and the west property line, it will be difficult to maintain a 15-foot clearance from the hot oil line. This may require a boardwalk area over the pond north of Nursery Road if the 15 feet cannot be reduced. Between Nursery Road and Belleair Road, water and hot oil lines on the east side of the right-of-way. The FPC Trail could continue on the east side of the right-of-way.

Map 15 **30-29-16**
(Belleair Road to U.S. 19)

Orange Blossom Grove is located south of Belleair Road within the FPC right-of-way. We will need to work with Orange Blossom Grove to possibly locate FPC trail at a beneficial location for them. This could include an alignment to eliminate tree reduction or locating the FPC trail near the building to provide refreshments to the users.

Allens Creek area will require a crossing such as a boardwalk just west of U.S. 19 over the creek. The area is a water recreation site with a canoe/kayak rental business nearby. The creek crossing vertical clearance height requirements will be researched by FDOT but should be no higher than the existing U.S. 19 creek crossing.

Florida Power Easement Notes
Field Review
May 27, 1998
Map 16 - Map 27

Attendees:

George Towensend	FPC Right-of-Way Utilization Committee Chairman	(407) 475-3339
Ada West	FPC Right-of-Way Utilization Committee	(407) 475-2242
Randy Strain	FPC Engineering	(813) 384-7953
Yvonne Ponce	FPC Micro Media Support Specialist	(813) 826-4268
Belisa Oliveira	FPC Legal Team	(813) 866-5110
Keith Keller	FPC Transmission Design	(407) 475-2247
Brian Smith	Pinellas County MPO	(813) 464-4751
Bert Valery	BAC Chairman	(813) 517-8888
Amy Dierolf	FPC Environmental	(813) 866-4327
Allison Drown	Pinellas County MPO	(813) 464-4751
Joe DeMoss	Pinellas County Engineering	(813) 464-3631
Ken Nosworthy	FDOT	(813) 975-6426

Map 16 29-29-16
(U.S. 19 to Whitney Road)

Flyover crossing U.S. 19. Retention ponds with wetland area located to the north. Ownership and maintenance of property belong to the boat marina facing U.S. 19. City of St. Petersburg 36" water main and FPC hot oil line located along the east side of the easement. FPC Trail alignment to travel between the hot oil line and the FPC towers, with a 15-foot offset from water and oil line.

Map 17 32-29-16
(Whitney Road to 150th Avenue)

City of St. Petersburg owns a 50-foot easement for 36" waterline. The FPC Trail will continue to hug 15-foot offset of hot oil line along the east side of the FPC easement. Service access road off of Whitney Road leading to FPC Tri-City substation and communication tower which is shared by Sprint. Parking lot and drainage/retention pond issues with Abilities of Florida located to the west of the easement. The FPC Trail will transition to the west side of the easement to avoid conflict with the FPC substation and gradually merge back towards the middle of the easement as it continues heading south where it runs into the Long Branch Creek. Issue of where the trail will cross the creek will depend on whether a flyover or the existing pedestrian crosswalk at 62nd Street North will be used to cross Roosevelt Boulevard (S.R. 686). If a flyover is used, the trail will merge to the east side of the easement. If the crosswalk is used, the trail will transition towards the west to line up with the crosswalk. The FPC Trail to travel south along the east side of 62nd Street between the existing

sidewalk and the Highland Nursery (which has an unverified lease agreement with FPC to encroach on the easement), Pinellas Technical Education Center, and commercial/business centers toward 150th Avenue North. There are some questions regarding parking lot encroachments and bus shelters which may have to be relocated.

Maps 18 and 19 4 or 5-30-16
(150th Avenue to Ulmerton Road)

FPC Trail to turn in a southeasterly direction, staying down the middle of the easement until it reaches the boundaries of the Rubin Icot Center. Located behind Auto Nation (facing Ulmerton Road), the trail will have to hug the parking lot of the ice skating rink to avoid getting too close to the top of the bank of the existing retention pond. This portion of the easement is privately owned. Continuing through Rubin Icot, the trail will have to stay to the east side of the easement and gravity walls will have to be built to shore up existing pond banks located throughout the park.

Map 20 9-30-16
(Ulmerton Road to C.R. 296)

FPC Trail will cross Ulmerton Road (S.R. 688) near 58th Street, which leads into a privately-owned business office park heavily congested with parking areas and landscaped islands. It was suggested to build an overpass to avoid conflicts with the parking lots but concerns arose as to how to span Ulmerton Road, clear the electrical wires overhead, and design a suitable transition back down to grade. An alternative route was scouted out, and it would involve aligning the trail along the far west side of the easement around an existing retention pond just south of Ulmerton Road and west of the business complex. The trail would then turn east and head towards the FPC training center which would lead it back out into open space and avoid the business complex altogether. Once in open territory, the trail would turn south and follow the towers staying along the east side of the easement where it would come out on 49th Street North, just north of 118th Avenue North. A communication tower with a Sprint co-location and retention pond is located on the east side of 49th Street, north of 118th Avenue. The FPC Trail to continue along the east side of the easement crossing 118th Avenue North, just east of 49th Street North.

Map 21 16-30-16
(C.R. 296 to 45th Street)

Hot oil line markers and heavy brush. The FPC Trail to continue along the east side of the easement.

Map 22 15-30-16
(45th Street to 102nd Avenue)

FPC Trail to continue on the east side of the easement. The easement is partially FPC owned and partially privately owned.

Map 23 22-30-16
(102nd Avenue to 34th Street)

Possible alternative route around golf course of Mainlands community on 102nd Avenue North. Alignment of the trail to turn east and follow the north side of what would have been the 102nd Avenue North alignment. This is a shallow drainage area with private backyards extending into the right-of-way. No apparent conflicts at this time other than the need to work with the adjacent homes as to the benefits of the project..

Maps 24 and 25 23 or 24-30-16
(34th Street to 9th Street)

FPC Trail to stay on the north side of the easement as it continues to travel east towards 28th Street. The City of St. Petersburg has an agreement with FPC to dispose of excess mulch in the easement west of 28th Street. Some concern was expressed that the elevation of the mulch may exceed the grade level of the trail if this agreement is long term. FPC representatives felt that something could be worked out with the City of St. Petersburg to eliminate any potential problems which may occur. On the east side of 28th Street, FPC has a 40-foot easement from the Suncoast Development Corporation. Manholes containing 2 - 115,000 volt underground lines run through this section and the trail will have to stay along the east side so FPC can access these manholes for maintenance purposes. There will be a flyover to cross the Interstate using the 40-foot easement located on the north side. Toytown is publicly-owned and is a good possibility for a trail-related park. The power goes underground at this point, with the FPC Trail to be on the east side of the south and north segment. As the alignment turns east, the FPC easement narrows with no judgement on the location of the trail.

Maps 26 and 27 19 or 20-30-17
(9th Street to Weedon Island Preserve)

Easement is open territory. There was some discussion regarding the crossing of Gandy Boulevard, 9th Street North, and 4th Street North. A flyover will be needed but other alternatives will have to be investigated. There were some concerns over the road leading into the preserve not being bicycle or pedestrian friendly. Should consider a separate pathway into the preserve. A good connection to the Friendship Trail is also needed from the Weedon Island Preserve at San Martin Boulevard.

Florida Power Corridor/Bicycle Pedestrian Trail

Summary

June 9, 1998

1:30 p.m. - 4:30 p.m.

Attendees:

George Towensend	FPC Right-of-Way Utilization Committee Chairman	(407) 475-3339
Ada West	FPC Right-of-Way Utilization Committee	(407) 475-2242
Randy Strain	FPC Engineering	(813) 384-7953
Keith Keller	FPC Transmission Design	(407) 475-2247
Brian Smith	Pinellas County MPO	(813) 464-4751
Gina Harvey	Pinellas County MPO	(813) 464-4751
Victoria Bucher	Florida Power Corp Land Agent	(407) 475-2454

On June 9, 1998, at 1:30 p.m. at the Florida Power Corporation (FPC) Headquarters in Maitland, Florida, Pinellas County staff met with the FPC Right-of-Way (ROW) Utilization Committee to review the proposed FPC Trail location within the FPC ROW.

The County presented a report that contained a task list, area maps with notations, private owner mapping and individual listings, along with a past events list. The task list was agreed to and the events list needed more inclusions such as the initial project door opening meeting. The mapped sheets were then discussed.

Based on the field visits held on May 12 and May 27, maps had been developed to display the preferred location of the trail. This process involved locating water lines, the hot oil line, tower locations, private ownership of right-of-way, and intersection crossings.

Those in attendance approved the location maps, with exception to the areas which are to be revisited due to complications. There is a possibility this revisit could occur before the July 7 meeting.

It was agreed that the report submitted would continue to change with several corrections and additions to be made to the Area Detail maps. These corrections and additions include:

- FPC structures and identification numbers to be added to the Area Detail maps to assist in documenting the exact FPC Trail location;
- Ownership listed on the Area Detail maps;
- Road crossings expanded to major, medium, and minor;
- Area Detail maps renumbered to eliminate missing sections;

- Locations with FPC Trail off FPC property to be designated by different line pattern;
- Area Detail maps to expand on roadway crossing treatments; and;
- Area Detail maps to have roadways labeled and unnecessary clutter removed.

FPC will require the FPC Trail to be closed during FPC maintenance of equipment. The time of closure for routine maintenance is usually not extensive. This was agreed to be a reasonable request to assure safety of the FPC Trail users.

FPC will also require the FPC Trail facility to be limited to 15 feet in width. This should provide adequate space for the FPC Trail without significantly limiting the FPC of future expansion space. Since the 15 feet will be needed for the Trail surface, landscape provisions or initiatives need to be discussed if they were to be allowed for

FPC usually does not allow for impermeable surfaces to be constructed within their property. The FPC Trail will be allowed with the understanding the County will be responsible for stormwater and retention requirements off the FPC fee owned property.

FPC stressed that their priority would be to protect those who have license agreements to access and utilize the FPC property, such as for parking, etc.

The group then briefly reviewed the private properties listings that were involved in the corridor.

FPC is willing to work for the County in contacting the private owners regarding the FPC Trail on their property. FPC would provide this service for an hourly fee if the County felt it to be necessary. It was felt that the Bicycle Advisory Committee members or County agents would be willing to contact the owners and FPC services would probably not be needed for this task. However coordination was viewed as an appropriate consideration here.

The two agreements for the project were discussed. Regarding legal indemnifications between FPC and Pinellas County, FPC representatives identified sources are available for reasonably priced indemnification policies up to \$10 million. It is expected FPC to approve the policy with payment made by the County.

The attendees agreed to meet on July 7, 1998 at 1:00 p.m. at the Florida Power Corporation facility in St. Petersburg. This meeting will include an additional review of the report and field revisits to the necessary sites.

FLORIDA POWER CORRIDOR/BICYCLE PEDESTRIAN TRAIL SUMMARY

JULY 7, 1998
1:00 P.M. - 3:45 P.M.

Attendees:

George Townsend	FPC ROW Utilization Committee Chairman	2600 Lucien Drive, Maitland	(407) 475-2239
Ada West	FPC ROW Utilization Committee	2600 Lucien Drive, Maitland	(407) 475-2242
Randy Strain	FPC Engineering	6565 38 Avenue N, St. Petersburg	(727) 384-7953
Keith Keller	FPC Transmission Design	2600 Lucien Drive, Maitland	(407) 475-2247
Victoria Bucher	FPC Land Agent	2600 Lucien Drive, Maitland	(407) 475-2454
Jim Talbot	FPC	2600 Lucien Drive, Maitland	(407) 475-2223
Belisa Oliveria	FPC Legal	34 Street, S. G.O.C.	(727) 866-5110
Kent Hendricks	FPC Environmental	3201 34th Street, S., St. Petersburg	(727) 866-4281
Amy Dierolf	FPC Environmental	3201 34th Street, S., St. Petersburg	(727) 866-4327
Joe DeMoss	Pinellas County Public Works	440 Court Street, Clearwater	(727) 464-3631
Brian Smith	Pinellas County MPO	14 S.Ft. Harrison Ave., Clearwater	(727) 464-4751
Gina Harvey	Pinellas County MPO	14 S.Ft. Harrison Ave., Clearwater	(727) 464-4751

On July 7, 1998 at 1:00 p.m. at the Florida Power Corporation's (FPC) Central Station Office located at 125 5th Street South in St. Petersburg, Florida, Pinellas County, staff met with the FPC Right-of-Way (ROW) Utilization Committee to review and discuss FPC Recreation Trail project.

The attendees briefly reviewed the reports, minutes, task list and other documents contained in the project book. Changes to these documents included:

- 1.) FPC Trail Date List - Add Dick Fitzgerald to attendees of November 13, 1997 meeting
- 2.) FPC Trail Criteria Report - This report will be rewritten based on the recent agreements and input
- 3.) Task List - add task to work with private property owners

The Committee and the County reviewed the Detail Maps outlining the location of the recreational trail. Several areas were noted as needing additional review and a revisit by both County and FPC Committee members. The areas flagged for additional review were:

- 1.) Top of the World Golf Course Area (7S)
 - Distribution Line Conflicts
 - License Agreement
 - Private privacy fences on FPC property

FPC/Bicycle Pedestrian Trail Meeting Summary

July 7, 1998

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- 2.) Pond north of Sunset Point Road (8N)
 - Distribution Line Conflict
 - HOL variance required due to pond
- 3.) Stag Run (8S)
 - Double cul-de-sac/fence/landscaping
- 4.) South of RR Crossing (9N)
 - Need to avoid roadside
 - Possible wetland area
 - Identify best route of trail
- 5.) SPJC (10N)
 - HOL variance required due to parking lot location
 - Design considerations with respect to changes in parking to accommodate FPC Trail
- 6.) Burnice Drive to Brentwood Drive (11N)
 - Elevated area/ponds
 - Clearwater drainage ROW
- 7.) Nursery Road (11S)
 - HOL variance required due to pond location or construct boardwalk
- 8.) Allen's Creek Area - south of Belleair Road (12N)
 - Design considerations/boardwalk
 - U.S. 19 Crossing
- 9.) South of Haines Bayshore (13S)
 - Obstacle in ROW
- 10.) Ulmerton Road to 126 Avenue (16N)
 - Alternate Route east of canal, west then south of development
 - Auto auction parking in FPC ROW
- 11.) 49th Street/118th Avenue area (16S)
 - Revisit intersection
 - Determine best route to crossing roadways
- 12.) I-275 east (21W)
 - Underground utility review needed to locate the FPC Trail

FPC/Bicycle Pedestrian Trail Meeting Summary

July 7, 1998

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Several additions, modifications and corrections were also noted to be included in the detail maps. These will be completed in the next bound report.

FPC representatives emphasized that the recreation trail concept with FPC ROW in Pinellas County is a pilot project, with Pinellas County supporting FPC in their efforts of installing transmission lines on limited access roadways.

The attendees discussed intersecting roadways and how to best handle. The concept of a Priority One and Priority Two listing of intersections best resolved with flyovers was agreed upon. It was noted that distribution lines at these intersections will need careful consideration since they are difficult and costly to relocate. FPC related that any structure in the FPC ROW such as a flyover would require the approval of the Transmission Design Group (contact Keith Keller) to assure all electrical safety standards are satisfied. This is in addition to the concern with the location of distribution lines at the intersections.

The attendees also discussed the width of the FPC Trail within the ROW. The Trail would be a 15-foot wide paved surface. Landscaping could be permitted, if maintained. The issue of maintenance was discussed briefly and will be defined during the negotiation process. The County may maintain/mow a maintenance area as part of the agreement. This maintenance area could include at least the linear area along the 15 foot trail site.

The issue of the Weedon Island entrance point will need to be discussed with both County and FPC staff. The route and entrance point to the Weedon Island Preserve will need to be closely coordinated with the County. FPC agreed to discuss internally with the Weedon Island Power Plant manager to determine FPC's preference in the trail location.

The status of the legal agreements was discussed. The two draft agreements were provided to the County. The County Attorney is in the process of reviewing the documents.

It is intended that the Detail Maps will become part of the actual contract agreement as an exhibit. FPC legal will word the contract to allow for the Detail Maps exhibit to be updated/modified as more information is available. This understanding may be necessary if the agreements are to be acted on by the end of August 1998.

Several outstanding items which need to be resolved were identified and individuals requested to schedule necessary meetings. In response, a meeting will be set with Florida Power Corporation, and Pinellas County attorneys to discuss the legal agreements. The noted revisit sites will be field checked by FPC and County staff to identify the preferred alternatives to resolve conflicts prior to the next Right-of-Way Utilization Committee meeting.

FPC/Bicycle Pedestrian Trail Meeting Summary

July 7, 1998

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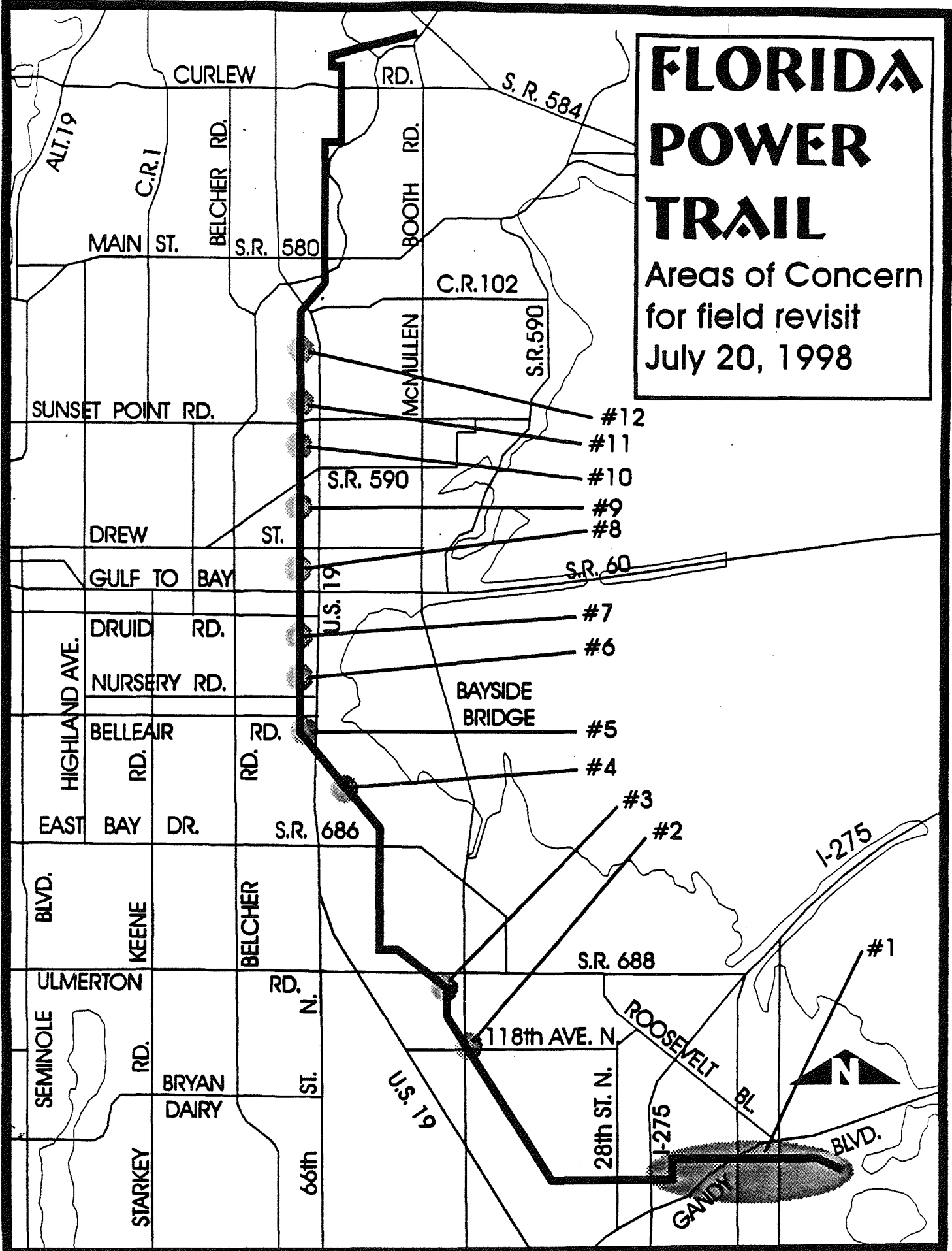
The next meeting was agreed to be held on August 4, 1998 at 1:00 p.m. The agenda for this meeting will include:

- 1.) Conclusions of final field visits
- 2.) Committee review of FPC right of way agreements
- 3.) Underground utility review with Paul White (HOL). Discussion on variance to the 15' setback requirement (Yvonne Ponce)
- 4.) Status of Agreements
 - A.) Detail Maps
 - B.) Two-party agreement
 - C.) Multi-part agreement
- 5.) Public Awareness Plan
(Ann Marie, Public Relations - FPC)
- 6.) Status of Weedon Island Entrance
 - A.) Pinellas County
 - B.) FPC

The meeting was concluded at approximately 3:40 p.m.

MIN/FPCTRAIL/AF

Areas of Concern for field revisit July 20, 1998



**FLORIDA POWER CORPORATION TRAIL
AREAS OF CONCERN
FIELD REVISITS
JULY 20, 1998**

- 1.) I-275 east (21W)
 - Underground utility review needed to locate the FPC Trail
- 2.) 49th Street/118th Avenue area (16S)
 - Revisit intersections - how to best cross roadways?
- 3.) Ulmerton Road to 126 Avenue (16N)
 - Alternate Route east of canal, west of development, then south of development
 - Auto auction parking in FPC ROW
- 4.) South of Haines Bayshore (13S)
 - Obstacles in ROW - large trees
- 5.) Allen's Creek Area - south of Belleair Road (12N)
 - Design considerations/boardwalk
 - U.S. 19 Crossing
- 6.) Nursery Road (11S)
 - HOL variance required due to pond location or construct boardwalk
- 7.) Burnice Drive to Brentwood Drive (11N)
 - Elevated area/ponds
 - Clearwater drainage ROW
- 8.) SPJC (10N)
 - HOL variance required due to parking lot location
 - Design considerations with respect to changes to parking to accommodate FPC Trail
- 9.) South of RR Crossing (9N)
 - Need to avoid roadside
 - Possible wetland area
 - Identify best route of trail
- 10.) Stag Run (8S)
 - Double cul-de-sac/fence/landscaping
- 11.) Pond north of Sunset Point Road (8N)
 - Distribution Line Conflict
 - HOL variance required due to pond
- 12.) Top of the World Golf Course Area (7S)
 - Distribution Line Conflicts
 - License Agreement
 - Private privacy fences on FPC property

**Florida Power Corporation
Corridor Recreation Trail
Field Meeting
July 20, 1998
9:00 a.m.**

Attendees:

Brian Smith, MPO
Gina Harvey, MPO
Joe DeMoss, County Engineering
Amy Dierolf, FPC
Keith Keller, FPC
Yvonne Ponce, FPC
Paul White, FPC

The Right-of-Way Subcommittee met at the K-Mart located at 9th Street and Gandy in St. Petersburg at 9:00 a.m. to complete the field revisit sites.

1) I-275 East

Paul White, FPC, discussed the FPC area to the south. He agreed the Recreational Trail would be best located generally on the north and west sides of the right-of-way. The hot oil line is located on the extreme north of the right-of-way and the other underground utilities are on the south side. Between these underground utilities, adequate space exists for the FPC Recreation Trail, which would be located near the middle to north side of the right-of-way. There was discussion regarding the access point to the Weedon Island area but no final determinations were made. Several washout areas and canals cross the FPC right-of-way and could necessitate engineering to provide water crossings. It was agreed several alternative access points are available.

2) 49th Street/118th Avenue Area

The second area to revisit was the intersection of 49th Street (C.R. 611) and 118th Avenue (C.R. 296). The FPC property crosses 49th Street north of 118th Avenue, then 118th Avenue east of 49th Street. A large retention pond is located on the northeast corner of the intersection. It was agreed that the FPC Trail would turn south along the west side of 49th Street cross the signalized intersection, then along the south side of 118th Avenue until it continues south on the east side of the FPC right-of-way.

3) Ulmerton Road to 126th Avenue

The FPC easement south of Ulmerton Road is privately owned and developed as an industrial area with walls, parking, and other structures. It was agreed the best route would be for the Trail to be located from Ulmerton Road south

along the east side of the drainage canal on the west side of the development, then east, until it merges back with the FPC property.

4) **South of Haines Bayshore**

- a) South of Haines Bayshore is apparently privately owned with very limited access. The best route was determined to be from Haines Bayshore along Cardinal Circle to access FPC property.
- b) North of Haines Bayshore is the Cove Cay Golf Course. Just south of U.S. 19 is privately owned with the Golf Course's main entrance, signage, and landscaping (see #5 below).

5) **Allen's Creek Area - South of Belleair Road**

The FPC property between Belleair Road and U.S. 19 consists of Orange Blossom Groves and Allen's Creek. This area will require a boardwalk to span the creek and to provide access to the existing recreational area, which includes kayak/canoe rentals. The preferred route is for the FPC Trail to follow the FPC property. This would provide the most aesthetic environment and also provide access to the recreational area of Allen's Creek. This route will involve somewhat complicated engineering and design. If the preferred route is determined to not be feasible, other alternatives are available for consideration.

6) **Nursery Road**

A pond is located north of Nursery Road towards the east side of the right-of-way. On the west side of the right-of-way are residential properties. The hot oil line is located at the western edge of the pond, approximately 25 feet east of west property line. While adequate space exists for the FPC Trail to be located 15 feet west of the hot oil line, the Trail would then be directly behind the bordering homes. With FPC approval, additional review in the engineering stage may allow the Trail to be located closer to the hot oil line to provide FPC improved access to the line and to avoid the private homes.

7) **Burnice Drive and Brentwood Drive**

Area revisited due to distribution line conflicts. North of Brentwood Drive, it was agreed the distribution lines would be relocated to the east and connected with the street lighting to make one circuit.

8. **St. Petersburg Junior College**

Currently, the hot oil line is next to the pavement of the St. Pete Junior College parking lot. The FPC Trail would be located on the existing pavement but the St. Pete Junior College parking spaces would be shifted slightly to the west. The existing drainage swale would be replaced with an ex-filtration system to allow the vehicles to park over. Three City of Clearwater potable water wells are also in this area but they should not be of concern to the Trail location.

- 9) **South of the Railroad Crossing**
The FPC Trail to cross at grade at the Drew Street signal on the east side of the right-of-way. North of the wet area and south of Sharkey Road, the Trail transitions to the middle of the right-of-way. South of the railroad crossing, the route shifts to the west side.
- 10) **Stag Run Cul-de-sac**
Two cul-de-sacs split the FPC property with a small area between separated by landscaping and a guardrail. It was determined that the best FPC Trail route would be to continue directly between the two cul-de-sacs.
- 11) **Pond North of Sunset Point Road**
Narrow area between the edge on the pond on the east and edge of right-of-way on the west. Hot oil line is located on the west side of the pond and distribution lines on the west side of the easement.* It was agreed adequate space existing for the Trail to be located between the hot oil line and distribution lines.
- 12) **Top of the World Golf Course**
Based on the revisit, no apparent problems exist with the distribution lines.

The field revisit concluded at approximately 12:15 p.m.

* Minutes revised August 4, 1998

Meeting Summary
Florida Power Right-of-Way Utilization Committee
Tuesday, August 4, 1998
1:00 p.m.

Attendees:

George Townsend	Florida Power Corporation	(407) 475-2239
Victoria Bucher	Florida Power Corporation	(407) 475-2454
Ada West	Florida Power Corporation	(407) 475-2242
Keith Keller	Florida Power Corporation	(407) 475-2247
Jim Talbot	Florida Power Corporation	(407) 475-2223
Randy Strain	Florida Power Corporation	(727) 384-7953
Brian Smith	Pinellas County MPO	(727) 464-4751
Joe DeMoss	Pinellas County Engineering	(727) 464-3631
Kent Hedrick	Florida Power Corporation	(727) 826-4281
Gina Harvey	Pinellas County MPO	(727) 464-4751
Belisa Oliveira	Florida Power Corporation	(727) 866-5110
Amy Dierolf	Florida Power Corporation	(727) 866-4327

Pinellas County MPO and Bicycle Advisory Committee representatives met with the Florida Power Right-of-Way Utilization Committee at the Maitland headquarters on August 4, 1998 at 1:00 p.m. Brian and George gave a brief status on the project's history for new attendees of the Right-of-Way Committee.

The attendees reviewed the areas of concern previously noted for revisit to determine the best FPC Trail route location. These locations were field reviewed on July 21, 1998, by FPC and Pinellas representatives.

The full Committee reviewed the 12 revisit locations and concurred with the route determinations, with the understanding that the St. Petersburg Junior College Site (8) would need additional evaluation due to the complex license agreements, parking, and drainage issues. In addition, the wording on the pond area north of Sunset Point Road (9) would be changed to read "Hot oil line is located on the west side of the pond and distribution lines on the west side of easement." In the area north of Nursery Road (6), private residences are on the west side, with a pond on the east and the hot oil line on the west side of the pond. While adequate space exists to construct the FPC Trail beyond the 15-foot minimum clearance from the hot oil line, it was agreed to pursue a variance to allow the FPC Trail to be located 7 ½ feet from the hot oil line. This would provide adequate clearance to the hot oil line, improve FPC access to the hot oil line, and increase the distance between the FPC Trail and residences on the west side of the right-of-way.

The attendees discussed the area of Allen's Creek in regard to alternative routes and the complexities of environmental permitting for a possible boardwalk construction. It was determined that the area of Allen's Creek is fee owned by FPC. It was recommended that contacts with the Department of Environmental Protection be utilized because they are multi-party agreement participants, to assist

in fast tracking environmental pre-permitting process. Therefore, it could be predetermined that the Department of Environmental Protection would cooperate in this process.

FPC provided Pinellas representatives a list of easement/license/lease agreements in place along the proposed FPC Trail. The listing includes location, nearest structure number, type of agreements, etc. It was agreed that, in order to determine what the existing uses are within the right-of-way, a field check would be needed.

The Committee briefly discussed the underground utility issues in the area east of 28th Street as noted during the revisit field trip. It had been agreed that the FPC Trail would be located toward the north and west sides of the right-of-way between the hot oil line located on the extreme north side and the underground utilities on the south side. No outstanding items of concern were noted on the underground utility issues.

Several agreements are in the process of execution. The multi-party agreement has been executed, with the three party agreement and actual right-of-way utilization permit outstanding. FPC legal and the Pinellas County Attorney's Office are in the process of working on the terminology of these agreements. It was agreed that the goal was to have these agreements executed within one to two weeks. FPC legal was optimistic on this time frame based on the cooperative efforts by FPC and Pinellas County on this project.

The attendees discussed the publicity campaign with the FPC Public Relations Coordinator. FPC will be holding a dedication/ribbon cutting ceremony for the Weedon Island opening in early September. This could also be a good opportunity to begin publication of the FPC Trail. It is necessary for Pinellas County to make initial contact with the private property owners of the corridor prior to any public discussion on the project. FPC representatives recommended Pinellas County send out introduction letters to the private owners defining the proposed project and requesting their acceptance of a supplemental easement.

Individual indemnity policies may be needed for the private property owners separate from the policy with the FPC. The County may also be able to purchase a bond or other policy to cover the private property owners or could purchase the property and provide FPC with an easement.

The Committee briefly discussed the Weedon Island entrance. FPC was not supportive of the FPC Trail entering the Weedon Island area on the FPC power line alignment east of San Martin Boulevard. The area is secured and closed at dusk. Therefore, the FPC Trail entrance should be located at a secured point. Pinellas representatives were also in favor of this concept. It was further agreed additional coordination with other entities are necessary to establish an acceptable entrance point.

The field visit to verify private uses along the corridor was scheduled for Friday, August 14, with a back-up date of Wednesday, August 19. It was agreed to meet at 7:00 a.m. at the coffee shop near Tampa Road and Boot Ranch.

The meeting was adjourned at approximately 4:00 p.m.

Meeting Summary
Florida Power Corporation Recreation Trail
Field Meeting
Wednesday, August 19, 1998

Attendees:

Brian Smith	Pinellas County Planning	(727) 464-4751
George Townsend	Florida Power Corporation	(407) 475-2239
Victoria Bucher	Florida Power Corporation	(407) 475-2239
Marc Hanger	Pinellas County Planning	(727) 464-4751
Joe DeMoss	Pinellas County Public Works	(727) 464-3631
Ivan Fernandez	Pinellas County Public Works	(727) 464-3631
Amy Dierolf	Florida Power Corporation	(727) 866-4327
Keith Keller	Florida Power Corporation	(407) 475-2247

The attendees met at the Publix located in the East Lake Woodlands Shopping Center at 7:00 a.m. The purpose of the field trip was to review and evaluate potential problem areas caused by private license agreement holders within the FPC corridor.

FPC had an organized file on the field trip of all known license agreements. This provided the attendees with adequate information to determine which are areas of concern.

The field review began at the north leg and ended near the southern tip of the FPC corridor. Several corrections/modifications to the "Detail Maps" were noted in order to provide additional information on existing conditions. These included labeling such as "SPJC parking" and redrawing minor roadways to show actual configuration such as the Stage Run cul-de-sacs.

Within these changes were two FPC proposed Trail realignments at Drew Street and Gulf-to-Bay Boulevard, the FPC Trail will transition to the west side of the property in order to use the existing signals at Old Coachman Road for crossings.

Based on this field visit, several areas were noted as locations that need additional research and attention to help assure any potential problems with the FPC Trail alignment are eliminated. These areas include the Countryside Mall parking area, Chilis, the Clearwater SPJC Campus parking, Orange Blossom Groves, and the Icot Center.

Pinellas County Planning has provided copies of the license agreements to the County Right-of-Way and Survey Divisions for their information and research. The County Planning also discussed making initial contacts with some of the properties to notify them of the proposed project. This would probably be coordinated through the Pinellas MPO Chairman's office.

The field meeting was completed by approximately 3:00 p.m.

Meeting Summary
Florida Power Corridor/Bicycle Pedestrian Trail
Friday, August 21, 1998

Attendees:

George Townsend	Florida Power Corporation	(407) 475-2239
Belisa Oliveira	Florida Power Corporation	(727) 866-5110
Brian Smith	Pinellas County Planning	(727) 464-4751
Keith Wicks	Pinellas County Public Works	(727) 464-3631
Bob Powell	Pinellas County Right-of-Way	(727) 464-3602
David Sadowsky	Pinellas County Attorney's Office	(727) 464-3354
Carol Abernathy	Pinellas County Attorney's Office	(727) 464-3354
Sarah Richardson	Pinellas County Attorney's Office	(727) 464-3354
Jan Herbst	Pinellas County Public Works	(727) 464-3829
Gina Harvey	Pinellas County Planning	(727) 464-4751

On August 21, 1998 at 1:00 p.m., Florida Power Corporation (FPC) and Pinellas County representatives met at the FPC offices in St. Petersburg to discuss several legal agreements pending for the FPC Recreation Trail.

Brian Smith presented an introduction of the project to the attendees to provide a brief history and latest interconnectivity projects being considered by the local municipalities.

The first agreement, which is being coordinated by Tom Kennedy for FPC, is the Memorandum of Understanding (MOU). The MOU has been signed by the County Administrator with several noted modifications. The MOU will be signed by other agencies, including the Department of Environmental Protection; the Florida Department of Transportation, Suwannee River Water Management District, Southwest Florida Water Management District, and Florida Power Corporation. Pinellas County legal representatives requested a copy of the MOU signed by all parties and were referred to contact Tom Kennedy, FPC, for the document. Brian Smith agreed to coordinate this effort.

The second document, the Memorandum of Agreement, was discussed in detail by the attendees. Most issues were resolved through this process. Outstanding issues that will be further discussed are the FPC Trail width and the defined area to be maintained by the County.

The third agreement was originally a FPC Right-of-Way Utilization Permit but it has been requested by Pinellas County to be changed to a Lease Agreement. This concept was acceptable to those present. A draft Lease Agreement was distributed by the Pinellas County Attorney's Office for FPC legal to review. It was decided that the legal representatives review and discuss the terms of the Lease Agreement. If needed to resolve any outstanding issues, the other participants will assist.

The meeting was adjourned at approximately 4:00 p.m.

Meeting Summary
Florida Power Corporation
Thursday, October 1, 1998

Attendees:

George Townsend	Florida Power Corporation	(407) 475-2239
Belisa Oliveira	Florida Power Corporation	(727) 866-5110
Brian Smith	Pinellas County Planning	(727) 464-4751
Keith Wicks	Pinellas County Public Works	(727) 464-3631
Jan Herbst	Pinellas County Public Works	(727) 464-3829
David Sadowsky	Pinellas County Attorney's Office	(727) 464-3354
Gina Harvey	Pinellas County Planning	(727) 464-4751

On October 1, 1998, at 3:00 p.m., Florida Power Corporation (FPC) and Pinellas County representatives met at the County Planning Department to discuss several outstanding issues regarding the FPC Trail.

The meeting opened with a discussion of the current situation. FPC briefed the attendees on the status of the Memo of Understanding (MOU), which is an administrative agreement. The intent of the MOU is to verify the spirit of agreement between the involved parties.

At the previous meeting, concerns were raised as to the proper format of the legal agreement, such as a license, permit, or lease. Based on continued discussion, all parties agreed that a long-term (99 year) lease would be acceptable. FPC attorneys merged the draft lease provided by Pinellas County and the FPC license agreement into a new draft "Trail Lease." This document was intended to include the necessary and requested details from both parties needed for implementation. The lease agreement will include several exhibits as needed, including the FPC guidelines for construction within the corridor, FPC standard guidelines for co-location regarding the Bartow/ Anclote Hot Oil Pipeline, liability insurance policy, and the detailed Trail location maps.

The Detail Trail Location Maps are to be included as Exhibit A and illustrate the approximate physical location and configuration of the Trail within the corridor. These maps are based on review, discussion, and several field visits by FPC and Pinellas County representatives. Exhibit A will be updated as the project proceeds, such as the Detail Maps replaced by Preliminary Design Plans, which will eventually be replaced with As Built Plans.

Based on additional discussion, Pinellas County and FPC's attorneys agreed to add a section to the draft lease on special exception areas. FPC will make the changes to the agreement and forward to the County Attorney for review.

The attendees also discussed the two major outstanding issues regarding the Trail: pavement width and maintenance.

A pavement width of 15 feet had been agreed to based on FPC concern for maintaining the limited access to property they have available for future utility needs. A discussion ensued which resulted in a better understanding on the parties' part of this concern. Based on the discussion, it was agreed the pavement width would not exceed 15 feet unless a special exception was granted by FPC for unique situations. It was agreed the Trail should be constructed as flat as possible with respect to the adjacent or shoulder areas to avoid creating drainage issues. FPC must have easy access to the corridor for the "Condor", which is a large vehicle used to construct and maintain electric facilities. Pinellas County may maintain the area surrounding the Trail as they feel necessary, such as sod and plantings. Trail features such as drinking fountains would need to be approved by FPC. It was also discussed that strategically placed kiosks describing the Trail and FPC's support of it would be incorporated as part of the progress of Trail development.

Maintenance of the corridor has also been an item of concern for both FPC and Pinellas County. During preliminary negotiations, FPC indicated Pinellas County could take over maintenance responsibility for the entire corridor in which the Trail is to be located. After additional discussions and negotiations, it was agreed that, if Pinellas County would maintain the corridor, the costs associated with this would be deducted from the payments from the County to FPC for the Trail use. An estimated maintenance cost of \$30,000 annually was based on the corridor's 369 acres, of which approximately half are routinely mowed quarterly. The estimated cost used for a mowing contract was \$50 per acre, which totals to approximately \$37,000, from which FPC agreed to accept the figure of \$30,000 per year regardless of what the County actually pays for the maintenance. FPC usually mows the corridor as required by the local government and, based on this agreement, would not be held responsible for the maintenance.

Pinellas County representatives were agreeable to this request and noted they would probably mow the Trail area more frequently. The County now usually mows the area of the Pinellas Trail every six weeks in order to provide the users and neighbors with a more appealing and safer area.

Previous discussions involved FPC allowing the use of their corridor for a trail and the County would allow FPC to use the mitigation value of the Brooker Creek area. This concept was becoming very complex; therefore, it was determined the best and most acceptable method was not mitigation but straight payment for the property use. FPC was willing to negotiate a one-time payment by the County or an annual payment plan.

FPC provided a figure of \$2.2 million to the County as an acceptable value for the use of the property. Utilizing a loan rate of 7%, the annual payment would be approximately \$150,000 that could be minus the maintenance figure of \$30,000 plus the insurance premium. This amount would be adjusted annually based on an acceptable CPI.

The draft lease agreement provided by FPC includes the provisions for both a lump sum payment and an annual payment. Once the parties agree on a payment plan, the agreement can be modified.

The attendees briefly discussed the funding sources available for this project, including Congestion Mitigation/Air Quality (CMAQ) monies and/or the Penny for Pinellas.

Pinellas County representatives agreed that the development of a project Master Plan should be initiated quickly, since it will be the key part of the project.

FPC had forwarded a draft insurance policy to the County for review and comment on the Weedon Island project. The County noted the policy had been forwarded to their Real Estate Department and they were waiting on their comment. Since a similar policy could be used for the Trail project, the County will follow-up on the status of the policy approval.

The attendees agreed to meet on Thursday, October 22, at 1:30 p.m. in the Planning Department Office. This would allow time for the attorneys to complete the draft lease agreement.

MEETING SUMMARY
FLORIDA POWER CORPORATION
OCTOBER 22, 1998
1:30 P.M.

Attendees:

George Townsend
Belisa Oliveira
Carol Abernathy
Keith Wicks
Jan Herbst
Joe DeMoss
Jim Collins
Brian Smith

Florida Power Corporation
Florida Power Corporation
Pinellas County Attorney's Office
Pinellas County Public Works
Pinellas County Engineering
Pinellas County Engineering
Pinellas County Engineering
Pinellas County Planning

On October 22 at 1:30 p.m. representatives of Florida Power and Pinellas County met to discuss the Florida Power Trail project and the agreement. The following points were covered during the course of the discussion.

- A) Pinellas County indicated that a lease could not be finalized by agreement until a legal description was available of the exact properties involved within the Florida Power lands. It was recognized the generalized location is known but that this may vary when the engineering is performed. It was agreed however that all of the negotiations were appropriate for the lease document but at this time the official action to be concerned with is an agreement for an intent to lease with the proposed lease document then attached to that.
- B) The lump sum vs. yearly payment procedure was discussed. It was agreed by both Florida Power and Pinellas County that the yearly payment procedure was the chosen option. Apparently Pinellas County could not do the lump sum procedurally and Florida Power preferred the yearly procedure. There was agreement that the \$150,000 per year with a CPI adjustment was acceptable.
- C) The tax credit or exemption was discussed. It was first agreed to check on the amount that was involved that would apply to the proposed utilization area. From the discussion, it appeared there would not be much gained by using this procedure and therefore it need not be pursued.
- D) The utilization area was discussed. It was recognized that there would be a 15' wide paved area with then the provision of a narrow area on each side for sodding and landscaping. It was noted this wouldn't be more than 2' to 5' on each side, depending on the engineering standard and design. It was further recognized there will be identified exception areas to accommodate a bench, kiosk or some other trail feature.

- E) The proposal for Pinellas County to maintain the entire Florida Power Line was discussed with Pinellas County indicating that was not something they were interested in. Pinellas County stated they preferred to maintain the utilization area. Pinellas County was also worried about having the maintenance people under their direction functioning in the Florida Power area involving Florida Power facilities that were beyond the utilization area. Florida Power had suggested this concept was one of efficiency and that while the County was maintaining the utilization area they could extend that contract to include maintaining the full power corridor and then get a credit for that against the yearly payment. It was recognized that Pinellas County was only interested in maintaining the utilization area, thereby not triggering the other issues.
- F) It was agreed that this agreement will not involve the environmental mitigation credits as was earlier discussed for the previous draft agreements.
- G) The insurance policy was discussed and indicated to be pretty much complete. It was recognized that Beth of Florida Power should get with Dominick of Pinellas County to work out any details.
- H) Florida Power pointed out that various amenities or features along the trail such as benches, water fountains or kiosks should be of non-conductive materials.
- I) As the project proceeds into design, Florida Power indicated that Keith Keller would be a good contact to work with concerning these types of specifics. It was noted by Pinellas County that a master plan for the corridor would be initiated very soon that would use local funding, not federal dollars, but that a full program has been identified that would utilize federal dollars for construction.
- J) It was agreed that some further refinements are needed on the agreement and that that work would be performed by the Attorneys with another draft to follow. There would also be an attachment based upon a document produced by George Townsend. This attachment appears to be referred to as Attachment B.
- K) It was agreed that a working manual should be developed for the maintenance of the trail and that the same document would also have the procedures that Florida Power could use when they may need to maintain their facility which would have an effect on the Trail.
- L) It was agreed that the meeting was productive and that further discussion and finalization was needed with a follow up meeting. That meeting was set for November 12 at 1:30 in the same location (Pinellas County Planning Department).

MEETING SUMMARY
FLORIDA POWER CORPORATION/PINELLAS COUNTY
NOVEMBER 12, 1998 1:30 P.M.
PLANNING DEPARTMENT

Attendees:

George Townsend
Belisa Oliveira
Carol Abernathy
Keith Wicks
Jan Herbst
Joe DeMoss
Brian Smith
David Sadowsky

On November 12, 1998 at 1:30 pm. representatives of Florida Power Corporation and Pinellas County met to discuss the Florida Power Trail project and the agreement. The highlights of that discussion are as follows:

- a) Agreement status: the draft agreement was re-reviewed based upon follow up wording that had been developed by Belisa. On the first page, paragraph 1 was redrafted at the meeting to reflect the annual payment procedure and other understandings. There was also discussion concerning the insurance paragraph on Page 3. This was also redrafted at the meeting. It was reconfirmed that this would be an agreement to lease with the various understandings spelled out in that agreement. This would then also serve as the intent to lease once it was executed by both parties. Once the lease was then acted on, this agreement would be already in place and referenced. As part of this discussion, George Townsend clarified that his draft of specific guidelines for the project would be incorporated into the main exhibit B entitled, "Guidelines" that would be referenced in the agreement.
- b) There were specific matters where discussion was needed to get resolution. The first of these was a provision in the insurance paragraph stating that Florida Power shall have the full discretion to increase policy limits if determined insufficient. During the discussion it was agreed that this would be rephrased. It would now state, "Florida Power Corp. and the County shall mutually agree to increase." The second subject concerned a provision for hazardous materials in the corridor. There was no agreement on how this provision should be approached in the agreement. Generally speaking, Pinellas County was seeking a full indemnification in the utilization area, while Florida Power felt it could only indemnify what it had done or was responsible for. Therefore, it was concluded this was a matter that would take some more specific negotiating between the parties between now and the next meeting.

- c) Brian indicated that the Orange Book was being updated with new information on the project.
- d) Pinellas County provided an update on the project development indicating that a master plan was to be locally funded while a full application was being submitted for federal funding for construction.
- e) Brian indicated that the need for a project liaison approach was close and that discussion should be initiated as to how this would be carried out. It was indicated that George Townsend would continue as the project contact for Florida Power with Ann Marie Vargar being the community liaison person. It was agreed this would be the subject of a separate meeting to insure that the public liaison for the project is appropriately handled.
- f) It was agreed the next meeting would be on January 14, 1999, which is the second Thursday in January at 1:30 in the Pinellas County Planning Department. It was further understood there would be discussion between the parties to work out the hazardous materials provision and further refinement of the agreement text based upon discussions at this meeting.

TO: George Townsend
Belisa Oliveira
Sam Nixon
David Sadowsky
Carol Abernathy
Keith Wicks
Jan Herbst

FROM: Brian Smith, Director
Planning Department

SUBJECT: Florida Power Trail Meeting

DATE: December 15, 1998

As a follow up to our December 14 meeting, it has been our practice to summarize what was discussed for our future reference. The people in attendance are listed above in this memo.

The meeting involved the review of Florida Power considering the idea of constructing the trail instead of the previously considered scenario of Pinellas County doing that. As the concept developed it made sense that Florida Power do the engineering and master plan work as well as the construction. This would make sense since they would then have a good knowledge of the project as they proceed into construction. It was understood that Pinellas County would have personnel involved to insure compatibility with its interests as well. At this point Florida Power is considering the idea and are now looking further into how this could be done. They indicated they had not done a project of this specific type before but felt that it was feasible for them to do it. It was recognized that they would be contracting out this work and therefore would be managing the activities. Sam Nixon, Director of Energy Delivery Services, is now going to proceed to put some details together that would define the approach for Florida Power. It was agreed that the next meeting on January 14 did not allow enough time for this to occur. Therefore we set our next meeting between Pinellas County and Florida Power for January 28, 1999 at 1:00 in the Pinellas County Planning Department.

During the discussion, Pinellas County agreed to provide Florida Power with two documents. One was the joint program agreement and the second was the Master Plan that was used for the Pinellas Trail. There was also interest in providing Pinellas County's GIS documentation of the alignment on aerials. It was agreed this would be updated to match the Orange Book alignment maps and then provided to Florida Power. The Orange Book, edition dated December 14, 1998, was distributed at the meeting. The Orange Book contains all of the current information on the project. The relationship of this project to the Weedon Island project was discussed. It was noted that the agreement for Weedon Island was proceeding forward and that Pinellas County's status on that project should be checked on with respect to its relationship with the Trail project. Brian

Smith agreed to check into this.

Finally, it was agreed that the Master Plan for the Trail project should include how the trail would connect into the Weedon Island area and, for that matter, the Friendship Trail. It was recognized that Florida Power would not be doing the engineering work in this regard but that there was the need for a defined concept as to how the Trail connection would work within the active area of the Preserve.

The meeting then adjourned with the understanding that Pinellas County would convey to Florida Power the materials noted and that there would be other contact as needed to be ready for the January 28, 1999 meeting. As noted earlier, that meeting will be at 1:00 in the Pinellas County Planning Department.

\\USER\BCE\DOCS\BCC\FLORPOW.G

**Meeting Summary
Florida Power Corridor/Bicycle Pedestrian Trail
Thursday, January 28, 1999**

Attendees:

George Townsend	Florida Power Corporation	(407) 475-2239
Belisa Oliveria	Florida Power Corporation	(727) 866-5110
Brian Smith	Pinellas County Planning	(727) 464-4751
Keith Wicks	Pinellas County Public Works	(727) 464-3661
David Sadowsky	Pinellas County Attorney's Office	(727) 464-3354
Joe DeMoss	Pinellas County Public Works	(727) 464-3631
Ken Nosworthy	FDOT	(813) 975-6426
Larry Buster	Parsons Brinckerhoff	(727) 289-5300
Kenneth Lord	Florida Power Corporation	(407) 475-2488
Mick Vander Ploeg	Florida Power Corporation	(407) 475-2261
Troy Buis	Florida Power Corporation	(407) 475-2446
Gina Harvey	Pinellas County Planning	(727) 464-4751

On January 28, 1999 at 1:00 p.m., Florida Power Corporation (FPC), Pinellas County, and Florida Department of Transportation (FDOT) representatives met at the Pinellas County Planning Department in Clearwater to discuss the latest draft agreements and status of the project.

The "Agreement to Lease" was modified by FPC to reflect changes as previously requested. The Pinellas County Attorney's Office will review and work directly with FPC legal on the final agreement. The next step in the process will be finalizing the Joint Project Agreement (JPA). A draft version was distributed at the meeting for Pinellas County attorney's approval.

The JPA also contains three exhibits:

Exhibit A	FPC Detail Maps (Salmon Book)
Exhibit B	Co-location Guidelines
Exhibit C	Bartow-Anclote Hot Oil Pipeline Guidelines

It was understood that Exhibit A will be changed as the actual design of the FPC Trail is completed. The group briefly discussed specifics of the project, including FPC constructing the FPC Trail outside their property limits to assure connectivity on the north and south ends of the FPC Trail into East Lake and Weedon Island. FPC was agreeable and noted Pinellas County was their client.

Pinellas County has developed and forwarded to FPC the standards for Trail design as established for the Pinellas Trail. It was noted FDOT had also developed standards (Feb 9B) for off-road trail systems which should be taken into consideration during the design of the FPC Trail. This was agreeable to FPC.

Also, the insurance premiums for the FPC Trail had been negotiated with the underwriters and those figures may change if an agreement is not reached by February 18. The figures would need to be renegotiated after that date.

The next major step for the project after BCC approval is the development of the Scope of Services and schedule. The Agreement to Lease will go before the Pinellas County Board of County Commissioners (BCC) for approval. It was agreed that the JPA would go to the BCC at the same meeting. Preferably, FPC would execute first with the BCC executing last, with a ceremony surrounding this meeting action. Both FPC and Pinellas County representatives seemed comfortable in establishing a working relationship and developing the necessary milestones for the project.

In conclusion, the attorneys will finalize the agreements for execution and the group will reconvene via conference call on February 18 at 1:00 p.m. at the Planning Department phone number - (727) 464-4751. The call's purpose is to assure the agreements are completed and acceptable, as well as scheduling the project to go before the BCC. It was agreed that the agreements would be ready for BCC approval in early March.

**Florida Power Corridor/Bicycle Pedestrian Trail
Legal Agreement Meeting
Conference Call
February 25, 1999
1:30 p.m.**

Participants:

Keith Wicks	Pinellas County Public Works
Jan Herbst	Pinellas County Public Works
Joe DeMoss	Pinellas County Public Works
Brian Smith	Pinellas County Planning Department
Gina Harvey	Pinellas County Planning Department
Carol Abernathy	Pinellas County Attorney's Office
David Sadowsky	Pinellas County Attorney's Office
George Townsend	Florida Power Corporation
Belisa Oliveria	Florida Power Corporation
Kenneth Lord	Florida Power Corporation
Mick Vander Ploeg	Florida Power Corporation
Troy Buis	Florida Power Corporation

On February 25, 1999, at 1:30 p.m., Florida Power Corporation (FPC) and Pinellas County representatives held a conference call to discuss the required legal agreements for the FPC Trail. These agreements included the Joint Partnership Agreement (JPA), Agreement to Lease, and the Lease.

The JPA was reviewed and the representatives reached consensus on the wording with the addition of a fiscal funding clause and a section that will be faxed by the County Attorney's Office to be included.

Additional discussion was deemed necessary on the 15% administrative costs for the project. FPC representatives agreed to evaluate this cost and provide County representatives with the methodology used in its determination. Pinellas County representatives stated the usual percentage used for the administrative cost of a project was significantly lower than 15%. It was agreed that the engineers should have a face-to-face meeting to discuss and work out the agreement details in the next two weeks.

The Agreement to Lease was also reviewed with consensus on all issues except insurance responsibilities. The representatives agreed to set a meeting the second week of March with Pinellas County and FPC Risk Managers and the insurance underwriter to work out the details on the coverage.

The Lease was discussed with general consensus on wording edits. The expenses incurred in the event FPC requires the Trail to be relocated was discussed. Representatives seemed receptive to sharing the relocation cost (50/50) in the event relocation is needed.

Florida Power Corridor/Bicycle Pedestrian Trail Conference Call

Dated February 25, 1999

Page 2

Other items discussed were the procedures for contractor selection and how the two parties will be involved. FPC representatives agreed to provide the Pinellas County Public Works representative with a copy of their selection procedure for review. The project will require secondary easements for areas of the corridor that are not FPC fee owned. FPC will provide this service for Pinellas County if needed with details to be inserted in the JPA.

FPC and Pinellas County representatives will meet the second week of March to further discuss engineering aspects of the project. The next meeting, which will be a face-to-face meeting, will be set up for the third week of March.

Meeting Summary
Florida Power Corridor - Bicycle/Pedestrian Trail
Thursday, April 29, 1999

Attendees:

George Townsend	Florida Power Corporation
Belisa Oliveira	Florida Power Corporation
Ken Lord	Florida Power Corporation
Troy Buis	Florida Power Corporation
Brian Smith	Pinellas County Planning
Gina Harvey	Pinellas County Planning
Jan Herbst	Pinellas County Public Works
Joe DeMoss	Pinellas County Public Works
David Sadowsky	Pinellas County Attorney's Office
Carol Abernathy	Pinellas County Attorney's Office

On April 29, 1999, at 1:30 p.m., Florida Power Corporation (FPC) and Pinellas County representatives held a meeting to discuss the Joint Participation Agreement (JPA), Agreement to Lease, and Lease. Updated drafts of these three documents were provided by FPC for Pinellas County's approval.

The attendees discussed the incentive program requested by FPC at length. This included discussions regarding the percentages proposed and whether the concept was acceptable or not. The County would need more definitive numbers to determine if an incentive program was acceptable and, if so, to define the FPC percentage.

After considerable discussion, it was agreed that actual cost estimates were needed prior to proceeding with the JPA as written. FPC agreed to write a quick, short JPA for execution which would include the development of a Preliminary Engineering Report (PER). Pinellas County Public Works representatives agreed to work with them on the JPA and provide them sample documentation on previously-executed JPAs and PERs. The PER would include a recommendation on project phasing and more definitive cost estimates of the project.

The attendees were briefed on the field trip held on April 26 and the resulting changes to the FPC Trail alignment. The field trip resulted in several FPC Trail shifts to avoid tower legs and improve necessary turns to eliminate 90 degree angles. FPC had agreed to several shifts to eliminate conflicts with distribution poles/lines, which will reduce Pinellas County's cost of relocating these facilities.

The attendees reviewed the Agreement to Lease and Lease contracts and were in agreement with new wording on both contracts, with the exception of Section 5 - Limitation of Liability. Pinellas County representatives will discuss these concerns internally and with FPC representatives with the intent to resolve any potential problems.

FPC will amend the draft contracts and provide them to the Pinellas County representative for review.

Meeting Summary
Florida Power Corporation Recreation Trail
Wednesday, August 25, 1999

Attendees:

Sallie Parks
Fred E. Marquis
Susan Churuti
Brian Smith
George Townsend
Nancy Loehr
Belisa Oliveira

County Commission Chairman
County Administrator
Pinellas County Attorney's Office
Pinellas County Planning
Florida Power Corporation
Florida Power Corporation
Florida Power Corporation

The meeting was held in the Board of County Commissioners Conference Room in the Clearwater Courthouse and was convened at 2:00 p.m. County Commissioner Parks convened the meeting and said its purpose was to provide for a discussion of the agreements associated with the proposed Trail on the Florida Power corridor on the east side of the County.

George Townsend then reviewed briefly the history of the discussions that have occurred between Pinellas County and the Florida Power Corporation. He indicated there was considerable work performed during the last two years. He said that the Trail had been located through the corridor by extensive field meetings and discussions between Florida Power and the County staff. He then said an agreement had been negotiated for a lease and an agreement to lease. He said the details of that had been worked out and that discussions were down to one paragraph, which was the issue. The was the paragraph referred to as "Limitations of Liability", which is Paragraph 5 on Page 3 of the Trail Lease. He indicated that the way the agreement is currently worded is the position of Florida Power and that this had been reconfirmed by the Corporation's management. He indicated he understood that Pinellas County did not find this provision to be acceptable.

Susan Churuti then indicated that George Townsend had summarized things correctly and that was the issue.

A discussion then proceeded concerning that paragraph. It was recognized that this matter needed to be resolved for the project to proceed since the project relied on an agreement that was acceptable by both parties.

Fred Marquis indicated that this was a matter that the Board of County Commissioners should decide, not himself, and that he was prepared to take that to the County Commission to see if they would accept the Florida Power position on the agreement and the referenced paragraph.

The Florida Power representatives then requested time to consider the matter before the meeting was concluded. Therefore, the meeting was briefly recessed to allow Florida Power that opportunity.

When the meeting was reconvened, Nancy Loehr indicated that Florida Power would request some time to review the information that had been provided at the meeting before the discussions would proceed further to the Board of County Commissioners. She indicated that Florida Power understood better the position of Pinellas County and wanted to be sure that this gained knowledge did not in some way modify Florida Power's viewpoint. She went on to say that the information provided by Susan Churuti to Belisa Oliveira could also be considered during this time.

It was, therefore, agreed that a second meeting would be scheduled in approximately a month to continue the discussion on this matter. Brian Smith agreed to conduct a phone poll to establish a follow-up meeting.

The meeting was then adjourned.

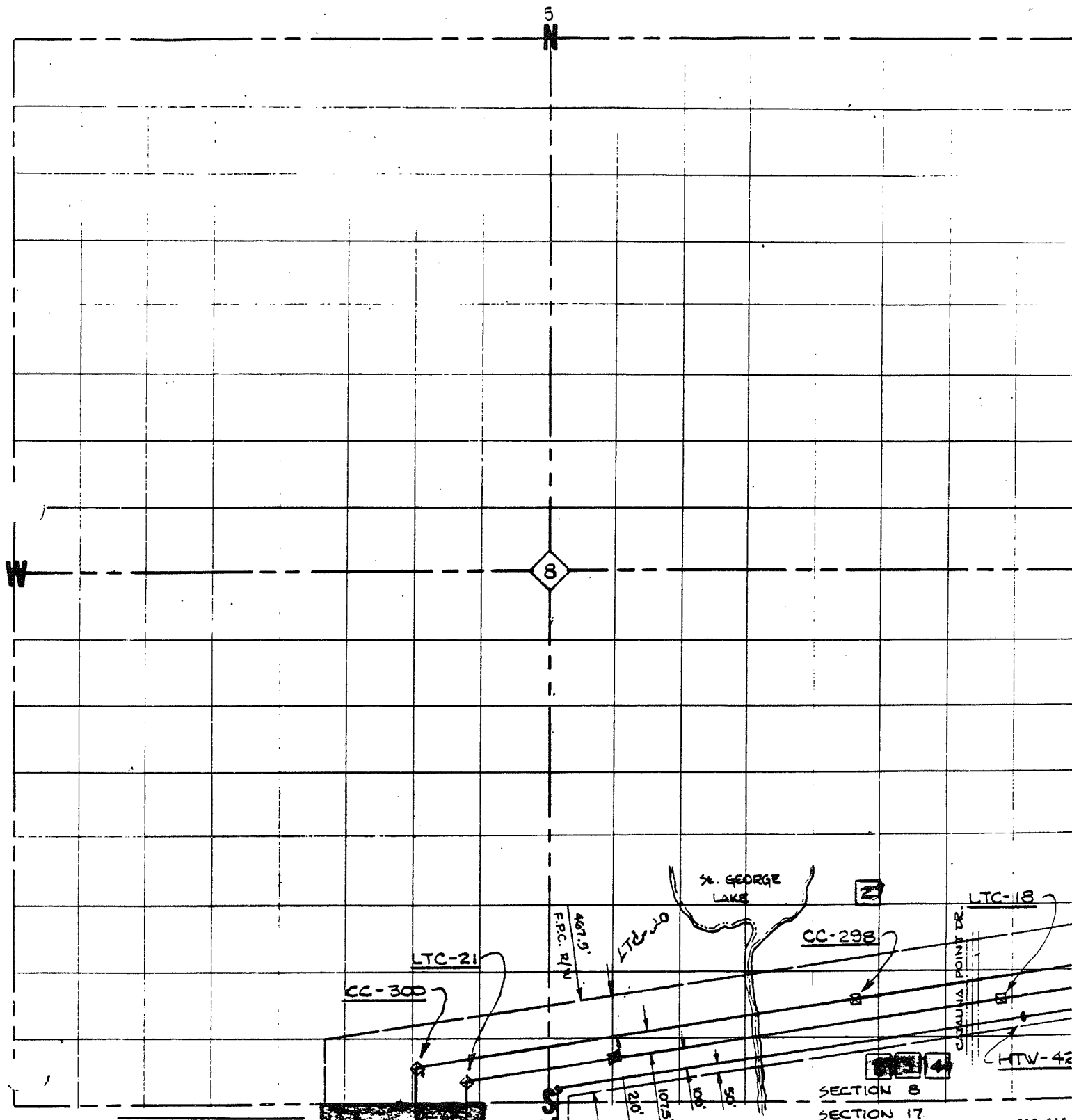


Power Lake Tarpon-Curlew 230 KV (LTC)

MAP 1

LINE: Higgins-Tarpon Spgs W 115KV SEC 8 TWP 28 S RGE 16 E, Pinellas COUN

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



Rev. 5/80

F.P.C. CURLEW SUB.

SCALE FOR QUARTER SECTION, 330 Ft. = 1 inch

Each side large blue squares = 10 chains, 40 rods, 660 feet; area of square 10 acres.
Each side small red squares = 2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

Copyright, 1967, James Hamilton Adair, Flint, Michigan

913 515

EASEMENT LIST

SEC. 8 TWP 28 S RGE. 16 E, Pinellas COUNTY

- 1 E FJ 4/3/64
Evans Properties, Inc.
237.5' thru SE $\frac{1}{4}$ of SW $\frac{1}{4}$
& S $\frac{1}{2}$ of SE $\frac{1}{4}$
4/7/64 OR 1902/38 52759 ✓ in Sec 9
- 2 E 11/12/64
230' thru SE $\frac{1}{4}$ of SW $\frac{1}{4}$
& S $\frac{1}{2}$ of SE $\frac{1}{4}$ N of R/W
11/18/64 OR 2042/680 52760 ✓ in Sec 9
- 3 SE 6/22/81 (distr)
U. S. Home Corp.
supplemental to FJ 1902/38
8/28/81 OR 5240/920 52761 ✓
- 4 SE 7/7/81 (distr)
Woodmere-Jacksonville, Inc.
supplemental to FJ 1902/38
7/24/81 OR 5225/625 52762 ✓ in Sec 9



MAP 2

SECTION PLA

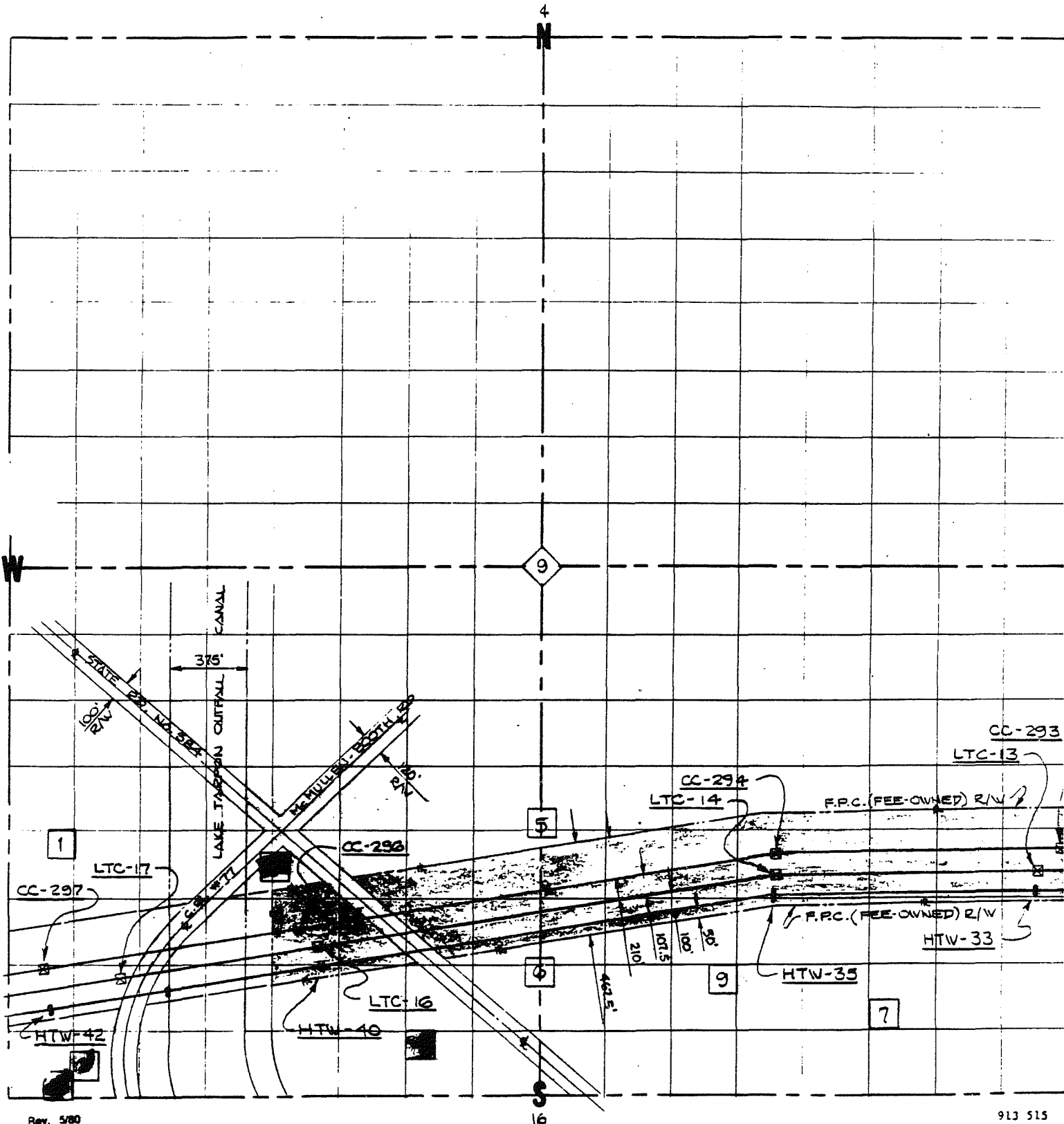
Florida
River

Crystal River-Curlew 230 KV (CC)

Lake Tarpon-Curlew 230 KV (LTC)

VE: Higgins-Tarpon Spqs W 115KVSEC 9 TWP 28 S RGE 16 E , Pinellas COUNT

SCALE FOR SECTION, (HTW)
660 Ft.=1 Inch. } Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



Rev. 5/80

SCALE FOR QUARTER SECTION, } Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres. M. A. 5/26
330 Ft.= 1 Inch } Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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EASEMENT LIST

SEC. 9 TWP. 28 S RGE. 16 E, Pinellas COUNTY

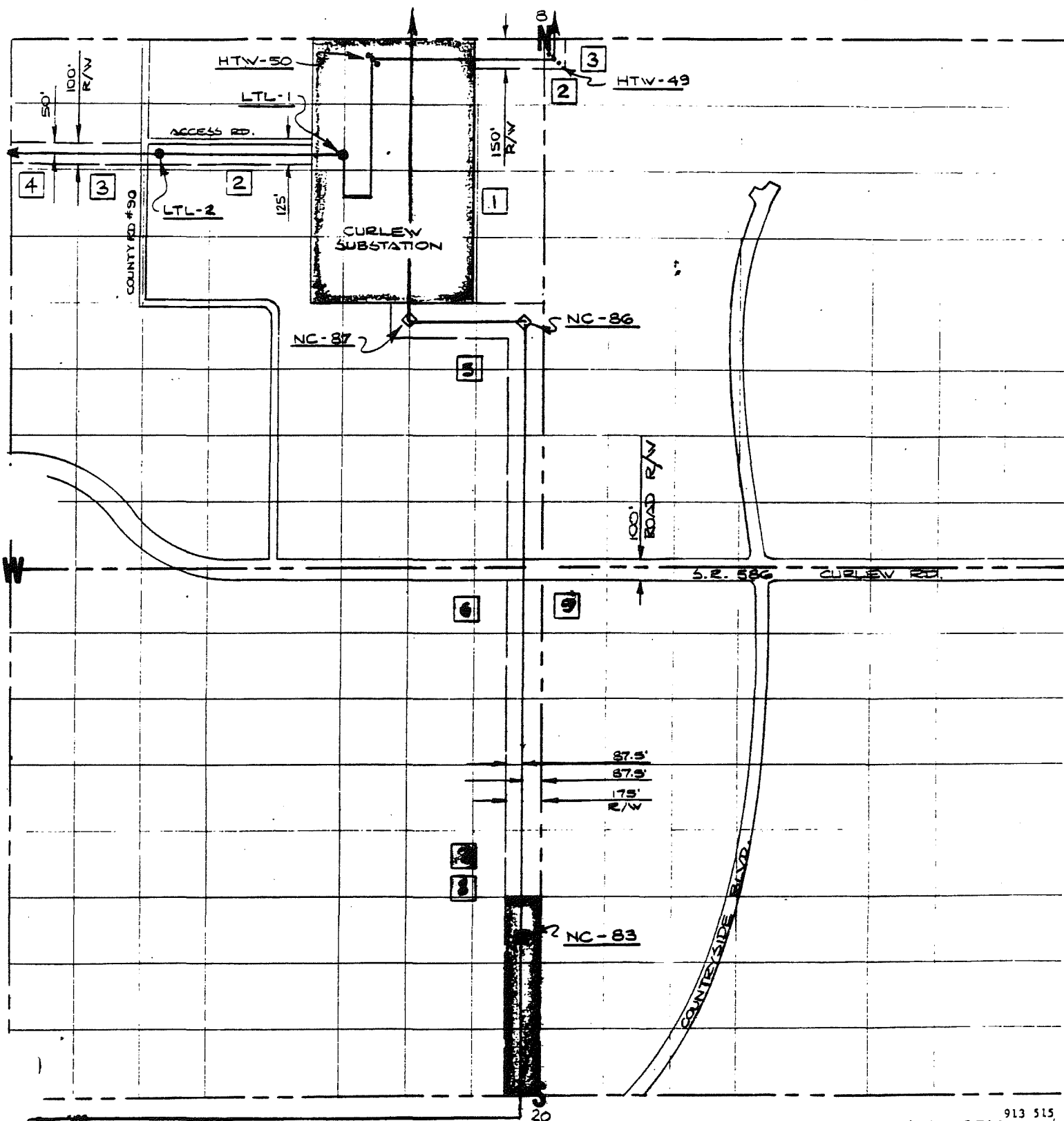
- [1] E 11/12/64
Evans Properties, Inc.
230' thru SW $\frac{1}{4}$ of SW $\frac{1}{4}$ less 375'
canal & CR-77
11/18/64 OR 2042/680 52760 ✓
- [2] E FJ 4/3/64
237.5' thru SW $\frac{1}{4}$ of SW $\frac{1}{4}$ less
375' canal & CR-77
4/7/64 OR 1902/38 52759 ✓
- [3] D 4/23/65
Boyd, R. Eugene
230' thru part of SE $\frac{1}{4}$ of SW $\frac{1}{4}$
S of SR-584
5/11/65 OR 2153/230 52766 ✓
- [4] D 8/16/62
237.5' thru part of SE $\frac{1}{4}$ of
SW $\frac{1}{4}$ S of SR-584
8/30/62 OR 1498/649 52767 ✓
- [5] D 6/14/65 ~~NATIONAL BANK~~
Union Trust ~~Co.~~, TR
230' thru SE $\frac{1}{4}$ of SW $\frac{1}{4}$ E of
SR-584 & thru S $\frac{1}{2}$ of SE $\frac{1}{4}$
6/16/65 OR 2174/474 52792 ✓
- [6] D 12/17/63 ~~UNION TRUST NATIONAL BANK~~
237.5' thru SE $\frac{1}{4}$ of SW $\frac{1}{4}$ E of
SR-584 & thru S $\frac{1}{2}$ of SE $\frac{1}{4}$ to
pt HTW intersects & 175' from
that pt to E bdy of section 52768
12/19/63 OR 1825/673
- [7] E 9/13/50
Boyd, Jesse A., Est of 521676
100' thru Lots 1, 2, 12, 13, 14, 17,
19, 20 & 21 ~~OF TAMPA &~~
ST. PETERSLAND CO., BLK "A"
9/16/50 ~~OR~~ 1286/309
- [8] SE 7/7/81
Woodmere Jacksonville, Inc.
supplement to OR 1902/38
7/24/81 OR 5225/625 523122 ✓
- [9] D 1/24/75
East Lake Woodlands, Ltd.
62.5' thru SE $\frac{1}{4}$
2/28/75 OR 4264/1037 52769



Higgins-Tarpon Springs West 115 KV (underbuilt on LTL 230 KV) (HTW)
Northeast-Curlew 230 KV (NC)

LINE: Lake Tarpon-Largo 230 KV (LTL) SEC 17 TWP 28S RGE 16E, Pinellas COUNTY

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



SCALE FOR QUARTER SECTION, $\left\{ \begin{array}{l} \text{Each side large blue squares} = 10 \text{ chains, } 40 \text{ rods, } 660 \text{ feet; area of square } 10 \text{ acres.} \\ 330 \text{ Ft.} = 1 \text{ inch} \quad \text{Each side small red squares} = 2.5 \text{ chains, } 10 \text{ rods, } 165 \text{ feet; area of square } .625 \text{ of 1 acre.} \end{array} \right.$

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

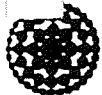
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913 515
M. ANDERSON 11/c

EASEMENT LIST

SEC. 17 TWP. 28S RGE. 16E, Pinellas COUNTY

- | | |
|--|---|
| <p>[1] D 1-26-62 (Curlew Substation)
Evans Properties, Inc.
West 808' of East 1155'
of NE$\frac{1}{4}$ of NW$\frac{1}{4}$
2-21-62 OR 1366/527 - 52781 ✓</p> | <p>[4] E 6-8-62 (HTW) 5278
Herman, Edward M.
100' thru W$\frac{1}{2}$ of NW$\frac{1}{4}$ of N
8-10-62 OR 1485/717</p> |
| <p>[2] E FJ 4-1-64 (HTW-CX)
Evans Properties, Inc.
125' thru W 17' of
NE$\frac{1}{4}$ of NW$\frac{1}{4}$ & E$\frac{1}{2}$ of NW$\frac{1}{4}$ of NW$\frac{1}{4}$
(Parcel I) & 25' access rd.
N 150' of E 347' of NE$\frac{1}{4}$ of NW$\frac{1}{4}$
and N 150' of W 100' of NW$\frac{1}{4}$ of NE$\frac{1}{4}$
(Parcel II, Tract A0
4-6-64 OR 1902/38 52759 ✓ in 9/28/16</p> | <p>SE 6-7-79 (distr.) 52785
Williams, Marvin E.
N$\frac{1}{2}$ of W$\frac{1}{2}$ of NW$\frac{1}{4}$ of NW$\frac{1}{4}$
7-27-79 OR 4889/310</p> |
| <p>SE 6-22-81 (distr.)
U.S. Home Corporation
N 150' of E 347' of
NE$\frac{1}{4}$ of NW$\frac{1}{4}$ and N 150'
of W 100' of NW$\frac{1}{4}$ of NE$\frac{1}{4}$
8-28-81 OR 5240/920 52761 ✓ in 8/28/16</p> | <p>[5] E FJ 4-1-63 (NC) 52786
Brenner, Hugh L.
E 175$\frac{1}{2}$ of SE$\frac{1}{4}$ of NW$\frac{1}{4}$ &
E 765.02' of N 175' of
SE$\frac{1}{4}$ of NW$\frac{1}{4}$
4-1-63 OR 1648/370</p> |
| <p>[3] SE 8-16-79 (distr.)
Woodmere Jacksonville, Inc.
E$\frac{1}{2}$ of NW$\frac{1}{4}$ of NW$\frac{1}{4}$ and
W 175' of NE$\frac{1}{4}$ of NW$\frac{1}{4}$
12-12-79 OR 4955/1031 52782 ✓</p> | <p>SE 10-9-79 (distr.)
Curlew Estates Development
Inc. 52787 ✓
E 175' of SE$\frac{1}{4}$ of NW$\frac{1}{4}$ &
N 175' of E 765.02' of
SE$\frac{1}{4}$ of NW$\frac{1}{4}$
11-28-79 OR 4948/1451</p> |
| <p>SE 7-7-81 (distr.)
N 150' of E 347' of
NE$\frac{1}{4}$ of NW$\frac{1}{4}$ and N 150' of
W 100' of NW$\frac{1}{4}$ of NE$\frac{1}{4}$
7-24-81 OR 5225/625 52762 ✓ in 9/28/16</p> | <p>[6] E FJ 2-25-63 (NC) 52788 ✓
Winner, Edward E.
E 175' of E 3/4 of NE$\frac{1}{4}$ of
2-25-63 OR 1623/56</p> |
| <p>SE 10-12-79 (distr.)
S 25' of E$\frac{1}{2}$ of NW$\frac{1}{4}$ of NW$\frac{1}{4}$
and S 25' of W 175' of
NE$\frac{1}{4}$ of NW$\frac{1}{4}$ lying East of
County Road 90
12-17-79 OR 4957/912 52783 ✓</p> | <p>[7] E 11-8-62 (NC) 52789 ✓
Catholic Burse Endowment
Fund, Inc.
E 175' thru E 3/4 of N$\frac{1}{2}$ of
SE$\frac{1}{4}$ of SW$\frac{1}{4}$ & S$\frac{1}{2}$ of SE$\frac{1}{4}$ of
12-14-62 OR 1571/187</p> |
| | <p>[8] D 4-21-78 (NC) 52790 ✓
U.S. Home Corporation
E 175' thru E 3/4 of N$\frac{1}{2}$ of
SE$\frac{1}{4}$ of SW$\frac{1}{4}$ and S$\frac{1}{2}$ of SE$\frac{1}{4}$ of
SW$\frac{1}{4}$
5-19-78 OR 4697/1916</p> |
| | <p>[9] GE 8-23-79 (distr.) 52791
St. Petersburg, Diocese of
NW$\frac{1}{4}$ of NW$\frac{1}{4}$ of SE$\frac{1}{4}$
9-12-79 OR 4912/143</p> |

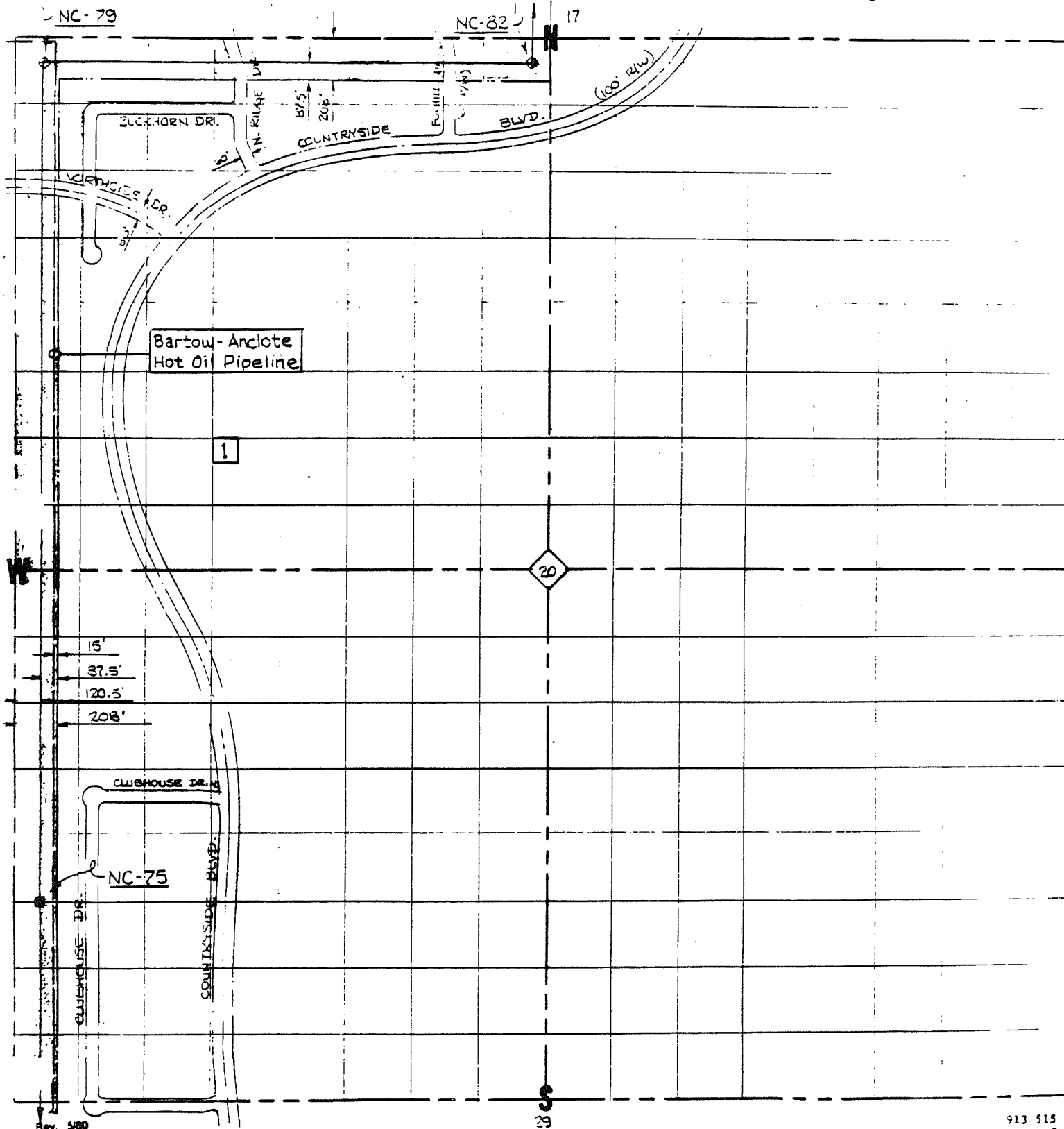


Florida
Power

Northeast - Curlew 230 kv (NC)

LINE: Bartow - Anclote - Pipeline (B.A.P.) SEC 20 TWP 28 S RGE 16 E Pinellas COUN

SCALE FOR SECTION, } Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. } Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



SCALE FOR QUARTER SECTION, } Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch } Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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913 515

A. ANDERSON 1-15-

EASEMENT LIST

SEC. 20 TWP. 28 S RGE. 16 E, Pinellas COUNTY

1 D 10/3/62 (UC) .
Exchange National Bank of
Tampa, Florida, The
W 208' of section & N 208'
of NW $\frac{1}{4}$.
10/12/62 OR 1528/222

52818 ✓



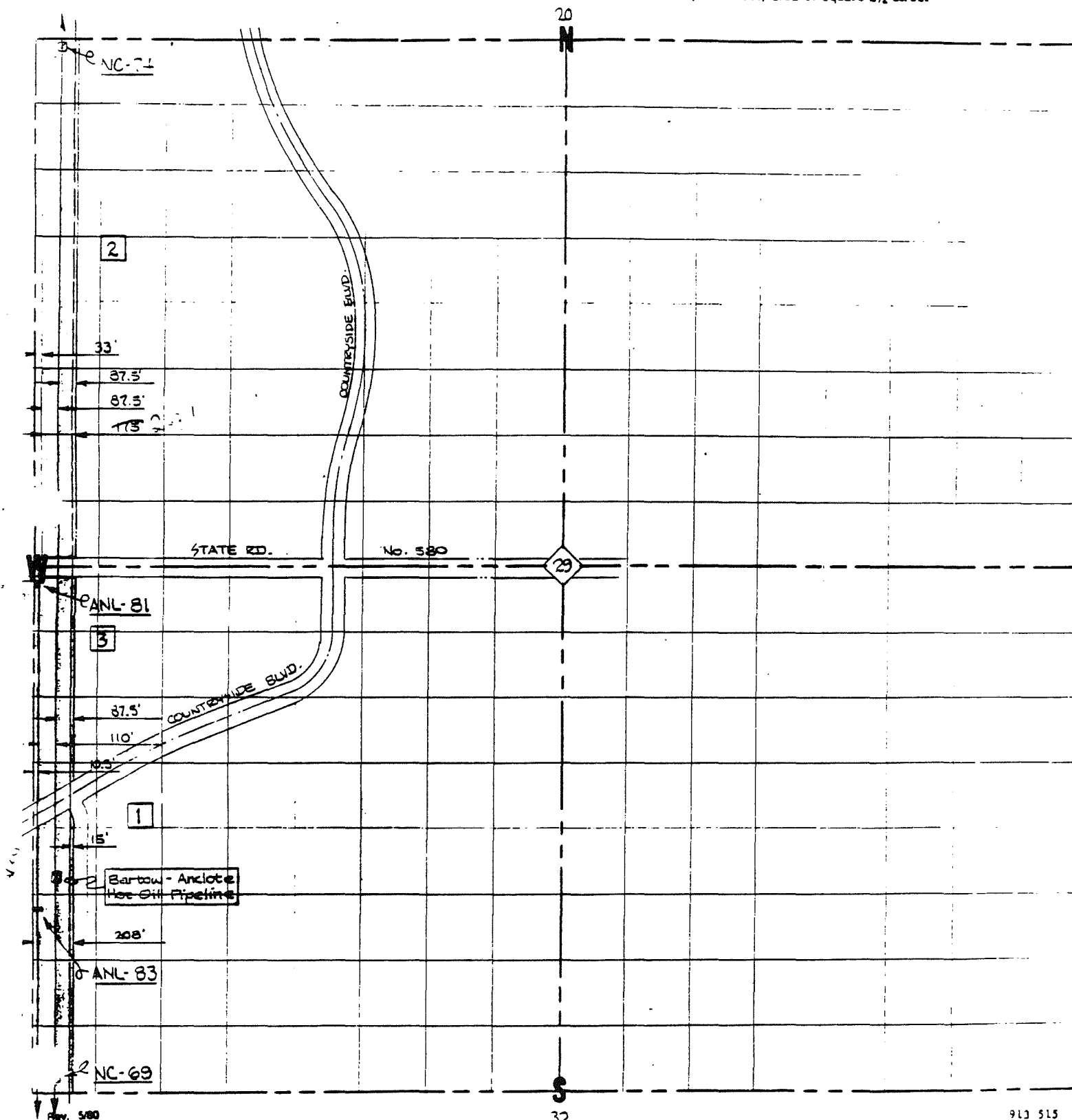
Florida
Power

Northeast-Curtis 230 KV (NC)

Anclote-Largo 230 KV (ANL), Anclote-E.C. Parwater 230 KV (ANEC } Dbl. circuit

LINE: Bartow-Anclote Pipeline (B.A.P.) SEC 29 TWP 28 S RGE 16 E Pinellas COUNTY

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2 1/2 acres.



SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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EASEMENT LIST

SEC. 29 TWP 28S RGE. 16E, Pinellas COUNTY

- [1] D 9-26-62 (NC) 52839 ✓
Koch, Carl E.
W. 208' of SW $\frac{1}{4}$
9-27-62 OR 1518/92
- [2] D 10-3-62 (NC) 52818 ✓ in sec 20/28/10
Exchange Nat'l. Bank of Tampa, The
W. 208' of NW $\frac{1}{4}$
10-12-62 OR 1528/222
(W. 33' of W 208' of NW $\frac{1}{4}$ SOLD TO-Regency Hgt's
for Mobile Homes, Inc.)
- [3] E 5-22-85 (ANL)
Florida, State of - DOT
R/W for ANL-81 52839 ✓
10-3-85 OR 6087/1532
OR 6097/845
(Filed Carl E. Koch, OR 1518/92)



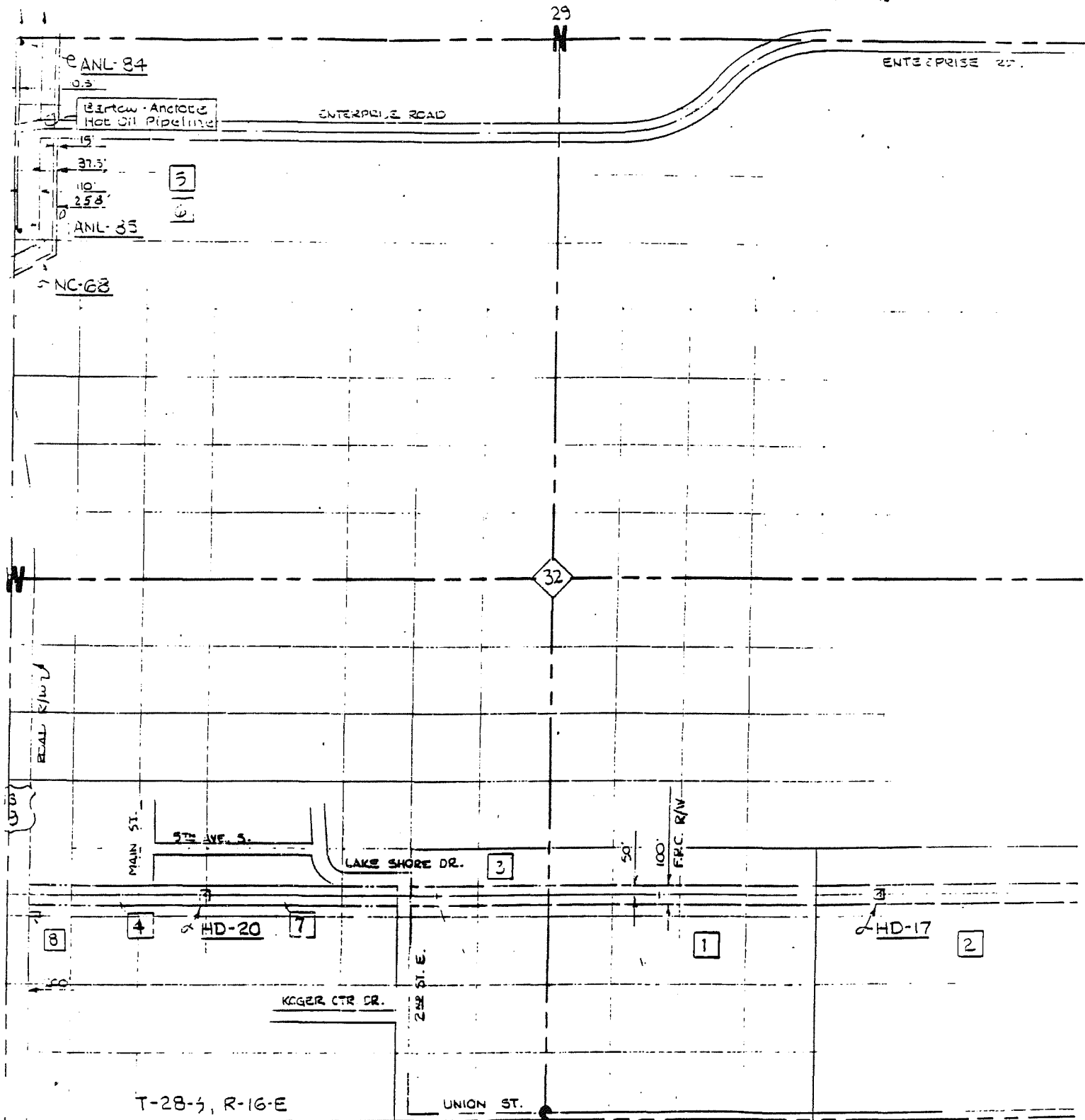
Florida
Power

SECTION PL

MAP 9

LINE: Higgins - Disston 115 KV (H) SEC 32 TWP 28 N RGE 16 E Pine 25 COUN

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch.. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2 1/2 acres.



Rev. 5/80

T-28-N, R-16-E

T-29-N, R-16-E

SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

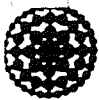
PRONTO LAND MEASURE

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EASEMENT LIST

SEC. 32 TWP 28 S RGE. 16 E, Pinellas COUNTY

-] E 12/1/54 (HD) [8] GE 4/18/84 52844
Bilgore, David & Co. 52844
N $\frac{1}{2}$ of SW $\frac{1}{4}$ of SE $\frac{1}{4}$
1/31/55 DB 1517/164 Premier Properties, Inc.
20 x 40' within 6th Ave S,
5/14/84 OR 5759/248
-] E 1/12/55 (HD) 52845
Taylor, Charles H. 52845
SE $\frac{1}{4}$ of SE $\frac{1}{4}$
1/31/55 DB 1.17/156
-] E 10/28/54 (HD) 52846
Pinellas Area Council, Inc. 52846
N 280' of E $\frac{1}{2}$ of SE $\frac{1}{4}$ of SW $\frac{1}{4}$
1/31/55 DB 1517/162
-] D 1/28/55 (HD)
Koch, Carl E.
Part of Lot 2 E of US 19; all of
Lots 3-12, Blk 16, Sec. A, UNIT 5,
CHAUTAUQUA, PB 18/61, in NW $\frac{1}{4}$ of
SW $\frac{1}{4}$ of SW $\frac{1}{4}$ 52848
1/28/55 DB 1516/581
-] D 9/26/62 (NC, ECTE) CARL E. KOCH
Portion of Lots 26, 25&24, Blk 5;
Lots 1, 2, 3, 4, 23, 24, 25&26, Blk 4;
Lots 1, 2, 3, 4, 23, 24, 25&26, Blk 3;
Lots 1, 2, 3, 4, 23, 24, 25&26, Blk 2; &
Lots 1, 2, 3, 4, 23, 24, 25&26, Blk 1 in
UNIT 2, SECTION A, CHAUTAUQUA, in
NW $\frac{1}{4}$ of NW $\frac{1}{4}$ 52834 + 6
9/27/62 OR 1518/92
-] E 1/26/73 (BAP) 52856
Tract in N $\frac{1}{2}$ being part of Lots 17,
18, 19&20, Blk 26; Lots 9-14, Blk 27;
Lots 13&14, Blk 28; Lots 13&14, Blk
29; & Lots 13&14, Blk 30; PLAT OF
CHAUTAUQUA ON THE LAKE, UNIT NO. 1,
SECTION B, PB 9/57; also Lots 1&26,
Blk 3; & Lots 1 & 26, Blk 4, PLAT
OF CHAUTAUQUA ON THE LAKE, UNIT NO. 2,
SECTION A, PB 9/146.
8/2/73 OR 4061/999
-] D 2/22/55 (HD)
Pinellas County 52848
100' thru E-3/4 of W $\frac{1}{2}$
3/11/55 DB 1523/607



Florida
Power

Map No. 10 SECTION PLAT

Northeast-Clearwater 230KV (NC)

Largo E.-Clearwater W. 69KV (LECW)

E.-Clearwater-Tarpon Springs 230KV (ECTW)

INE: Bartow-Anclote Pipeline (EAP)

Anclote-Largo 230KV (ANL)

Anclote-E.-Clearwater 230KV (ANEC)

Del. Circuit North of Substation

SEC 31

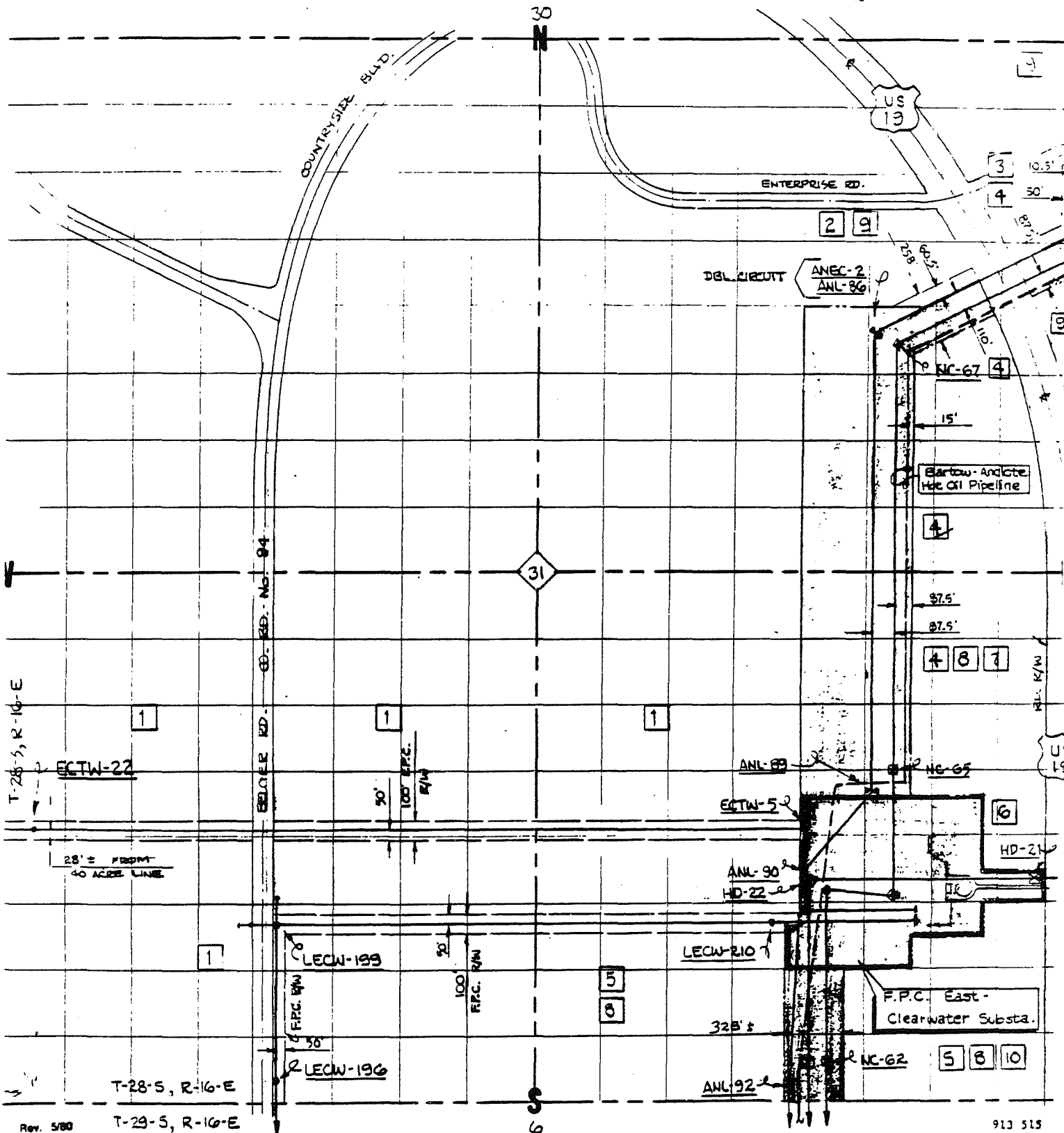
TWP 28 S

RGE 16 E

Pinellas

COUNTY

SCALE FOR SECTION, } Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. } Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2 1/2 acres.



SCALE FOR QUARTER SECTION, } Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch } Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

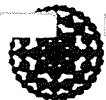
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913 515
A. ANDERSON 1/11/72

EASEMENT LIST

SEC. 31 TWP. 28 S RGE. 16 E, Pinellas COUNTY

- | | |
|---|--|
| <p>E 4/11/56 (ECTW, LECW) <u>52850</u> 9
 Bird, John U.
 W-3/4 of N$\frac{1}{2}$ of S$\frac{1}{2}$; S$\frac{1}{2}$ of SW$\frac{1}{4}$
 4/27/56 DB 1593/267</p> <p>E 7/29/76 (ANL)
 U.S. Homes of FL, Inc. <u>52851</u> ✓
 83' thru N$\frac{1}{2}$
 11/1/76 OR 4472/1849</p> <p>D 9/25/62 (NC)
 LaMont, Barbara Gibson <u>52852</u>
 Lots 12-15, Blk 26, UNIT NO. 1-
 Section B, CHAUTAUQUA, PB 9/57
 9/27/62 OR 1517/424</p> <p>D 9/26/62 (NC, ECTE)
 Koch, Carl E.
 Part of NE$\frac{1}{4}$ of SE$\frac{1}{4}$ & SE$\frac{1}{4}$ of NE$\frac{1}{4}$ W of
 survey line & portion Lots 1-5 W of
 US 19, Blk 26, CHAUTAUQUA UNIT 1,
 Sec. B in NE$\frac{1}{4}$ of NE$\frac{1}{4}$; Lots 8, 9, 10 &
 19, E of US 19, & 11-18, Blk 26, &
 Lot 13, Blk 27 in NE$\frac{1}{4}$ of NE$\frac{1}{4}$
 9/27/62 OR 1518/92 <u>52839</u> ✓
 5/29/62 (NC, LECE)
 Portions SE$\frac{1}{4}$ of SE$\frac{1}{4}$ & SW$\frac{1}{4}$ of SE$\frac{1}{4}$
 5/7/62 OR 1443/350 <u>52853</u> ✓</p> <p>D 1/28/55 (E Clearwater Substation)
 Fred N$\frac{1}{2}$ of SE$\frac{1}{4}$ of SE$\frac{1}{4}$
 1/28/55 DB 1516/581 <u>52854</u> ✓</p> <p>D 4/9/62 (Addn to E Clearwater Substa)
 S$\frac{1}{2}$ of NE$\frac{1}{4}$ of SE$\frac{1}{4}$
 4/19/62 OR 1407/661 <u>52855</u> ✓</p> <p>E 7/3/56 (ECTW, LECW)
 SW$\frac{1}{4}$ of SE$\frac{1}{4}$; part of SE$\frac{1}{4}$ of SE$\frac{1}{4}$ S of
 FPC property line & NE$\frac{1}{4}$ of SE$\frac{1}{4}$
 7/19/56 DB 1608/435 <u>52882</u> ✓</p> | <p>E 1/26/73 (ANL) <u>52856</u> ✓
 Koch, Carl E.
 Tract in N$\frac{1}{2}$ being part of
 Lots 10-13, Blk 25; Lots
 1-4 & 24, Blk 26, CHAUTAUQUA
 ON THE LAKE, UNIT NO. 1,
 SECTION B, PB 9/57
 & Tract in N$\frac{1}{2}$ & being part
 of Lots 17-20, Blk 26;
 Lots 9-14, Blk 27; Lots
 13&14, Blk 28; Lots 13&14,
 Blk 29; & Lots 13&14, Blk
 30, CHAUTAUQUA ON THE LAKE
 UNIT NO. 1, SECTION B;
 also part of Lots 1&26,
 Blk 1; Lots 1&26, Blk 2;
 Lots 1&26, Blk 3; & Lots
 1&26, Blk 4; PLAT OF CHAU-
 TAUQUA ON THE LAKE, UNIT
 NO. 2, SECTION A, PB 9/146
 8/2/73 OR 4061/999</p> <p>10 E 7/3/56 (ECTE) <u>52814</u> ✓
 NE$\frac{1}{4}$ of SE$\frac{1}{4}$; SE$\frac{1}{4}$ of NE$\frac{1}{4}$ &
 NE$\frac{1}{4}$ of NE$\frac{1}{4}$
 7/19/56 DB 1608/438</p> <p>11 E 6/13/39 (ECTE) <u>521678</u>
 Evans, W., & Co.
 SE$\frac{1}{4}$ of NE$\frac{1}{4}$ & E$\frac{1}{4}$ of SE$\frac{1}{4}$
 7/25/46 DB 1086/539</p> |
|---|--|



Florida
Power

LINE:

Northeast-Corlew 230KV (NC)
Largo E. - Clearwater W. 69KV (LECW)
Higgins-District 115KV (HD)
Anclote-Largo 230KV (ANL)
Bartow-Anclote Pipeline (BAP)

SEC 6

TWP 29S

RGE 10E

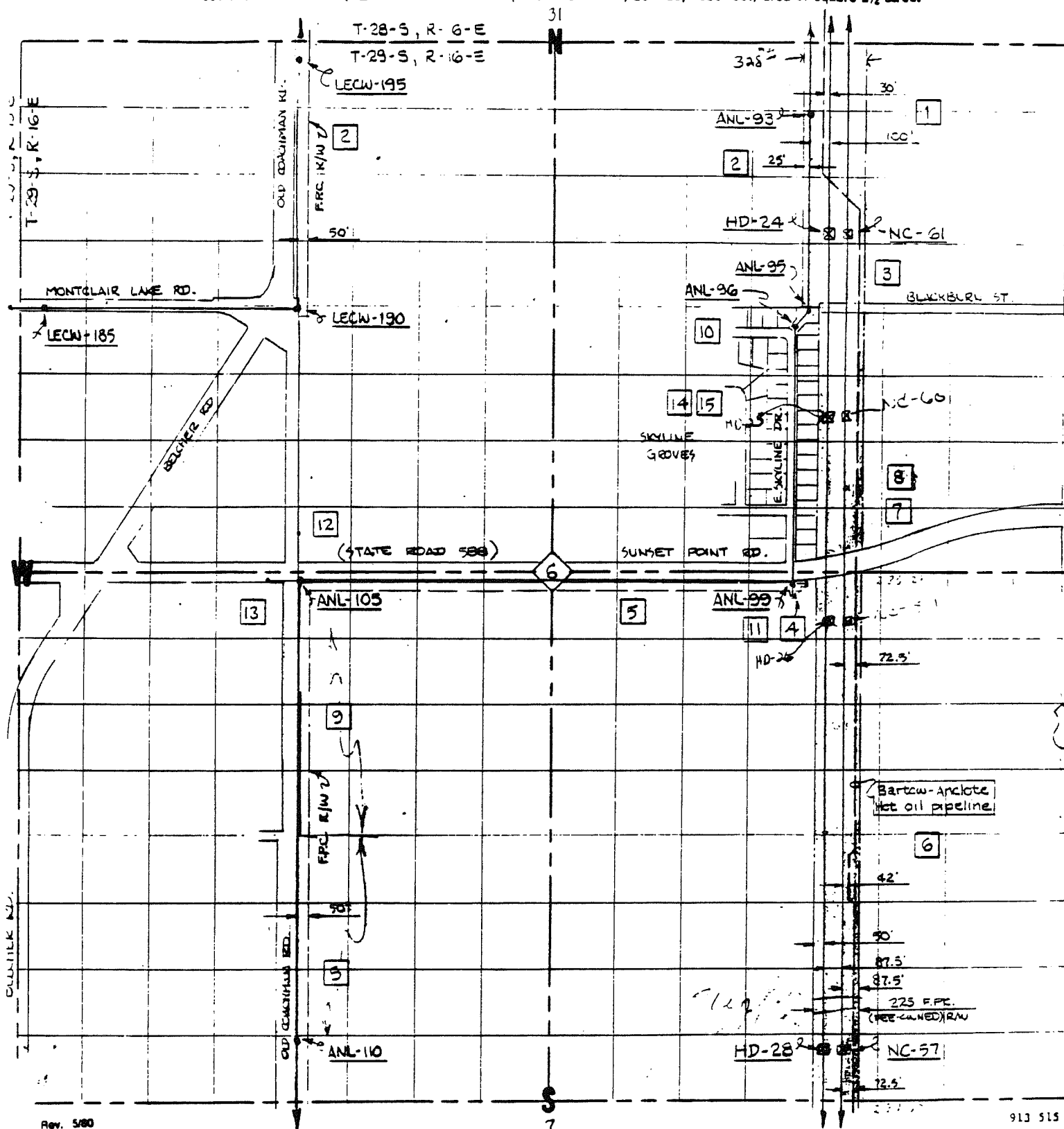
Pinellas

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MAP 11

SECTION PL

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2 1/2 acres.



SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

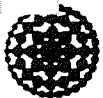
PRONTO LAND MEASURE

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EASEMENT LIST

SEC. 6 TWP 29 S RGE. 16 E, Pinellas COUNTY

- | | | | | | |
|---|----|---|----|-------|--|
| 1 | D | 4/30/62 (NC)
Thompson, Harry L.
W 228' of Lot 11, BLACKBURN'S
SUBDIV., less S 260' in NE½ NE½
5/17/62 OR 1428/48 | 10 | GP | 9/6/73 (ANL)
Boutell, John
Lot 25, SKYLINE GROVE SUBDIV.
PB 44/22, in SW½ of NE½
11/10/73 OR 4088/1925 |
| 2 | E | 9/26/56 (LECW)
Ruff, F. G., Est of
NE½ of NW½
10/23/56 DB 1626/556 | 11 | GP | 8/17/63 (ANL)
Reiner, Jan
Lot 48, SGS, in SW½ of NE½
9/20/63 OR 1764/287 |
| | TP | 6/20/57 (ANL)
W 25' of E 100' of NW½ of NE½
6/27/57 OR 1428 | 12 | E & G | 6/26/56 (LECW)
Sunset Point Estates, Inc.
Lot 1, Blk C, SUNSET POINT
ESTATES, PB 39/67, in W½ of
SE½ of NW½, & port. SW½ of
NW½ E of hwy
9/21/56 DB 1620/267 |
| | D | 6/20/57 (LECE)
E 75' of NW½ of NE½
6/27/57 OR 1428 | 13 | GP | 2/22/57 (ANL)
Webb, James B.
NE½ of NE½ of NW½ of SW½
5/17/57 OR 94/317 |
| 3 | D | 4/26/62 (NC)
Lutz, John H.
W 229' of S 260' of Lot 11,
BS in NE½ of NE½
6/11/62 OR 1445/395 | 14 | E | 5/6/57 (ANL)
Weidemeyer, L. Dudley
Lots 25-41, SGS, in SW½ of NE
6/6/57 OR 107/446 |
| 4 | GP | 9/21/73 (ANL)
Mickler Corp.
N 500' of E 400' of W½ of
SE½ less SR R/W
10/24/73 OR 4094/1621 | 15 | GP | 9/13/73 (ANL)
Thompson, Eric E.
Lot 26, SGS
10/10/73 OR 4088/1921 |
| 5 | E | 5/18/64 (ANL)
Coachman Properties, Inc.
Port. N 120' of NW½ of SE½ less
rd R/W & E 100' of W 135' of
SE½ of SW½
6/10/64 OR 1945/45 | 16 | E | 3/7/94 (ANL Relocation)
Great Western Bank
Port Tr A, SUNSET COACHMAN CENTER,
PB 84/81
4/7/94 OR 8624/1069 |
| 6 | D | 7/2/62 (NC)
W 232' of W½ of E½ of SE½
7/12/62 OR 1446/117 | 17 | E | 11/2/93
Pinellas County
taken back from sale of Item 6 above
1/18/94 OR 8537/2032 |
| 7 | D | 6/8/62 (HD)
Calvary Baptist Church, TR
W 231' of S 254' of Lot 12,
BS, in SE½ of NE½
6/11/62 OR 1446/117 | 18 | E | Addendum to Item 17 above
OR 8537/2036 |
| 8 | D | 12/17/62 (NC)
Blackburn, Wayne W.
W 230' of Lot 12, BS less S 254'
12/21/62 OR 1576/549 | | | |
| 9 | E | 6/24/57 (ANL)
Biggs, David W.
NE½ of SW½
7/10/57 OR 130/40 | | | |



Florida
Power
& Light

LINE:

Bartow-Anclote Pipeline (BAP)

Anclote-Largo 230 kv (ANL)

Higgins-Diston 115 kv (HD)

Northeast-Cutler 230 kv (NC)

SEC 7

TWP 29-S

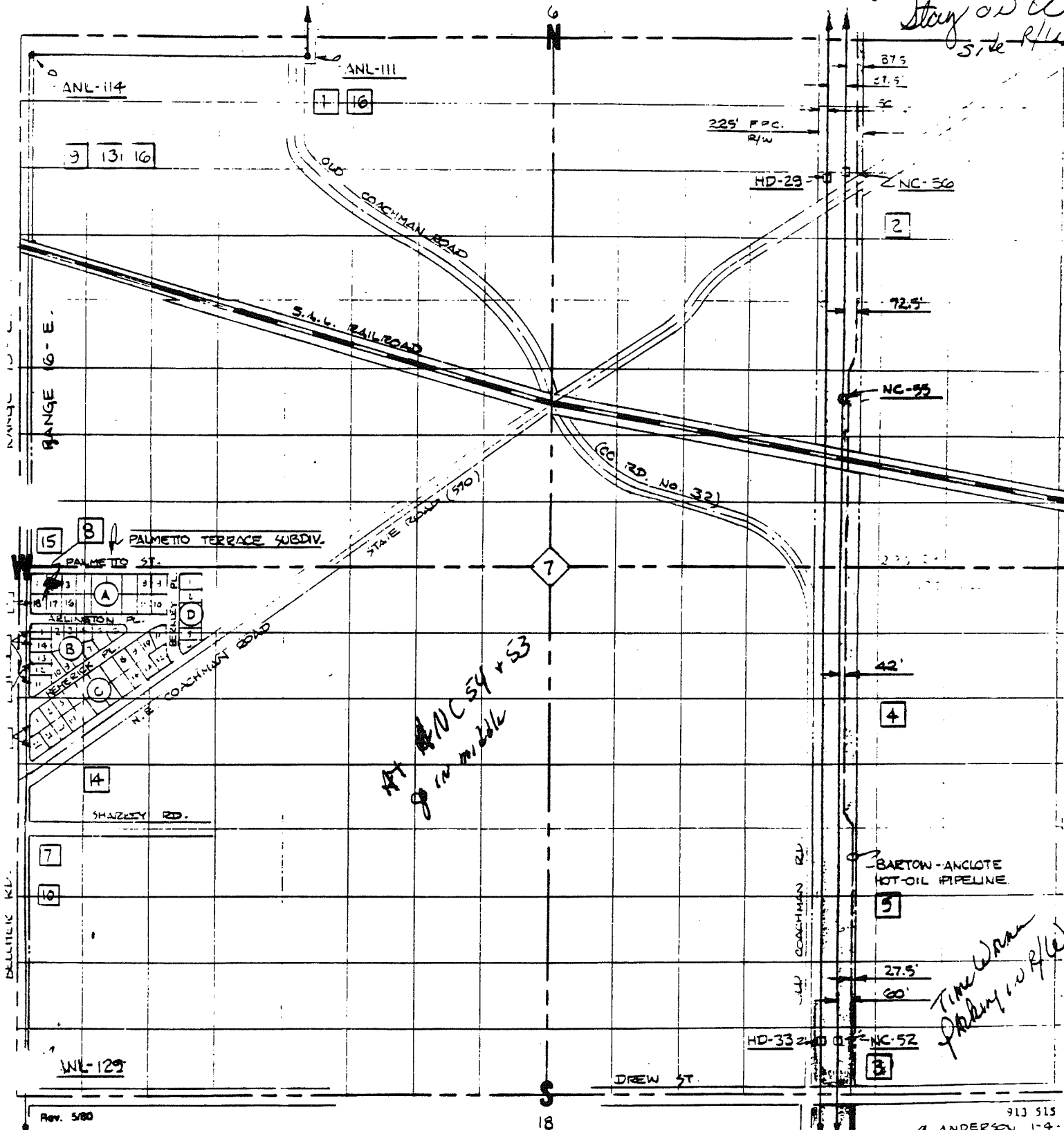
RGE 16-E

Pinellas

COUN

MAP 12 SECTION PL

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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EASEMENT LIST

SEC. 7 TWP 29 S RGE. 16 E, Pinellas COUNTY

- | | |
|--|--|
| <p>[1] GE 9/11/73 (ANL)
Coachman Properties, Inc.
60' E from pole approx 100' S of
N boundary of CR 32 <i>52920</i>
10/10/73 OR 4088/1926</p> <p>[2] D 7/2/62 (NC)
W$\frac{1}{2}$ of E$\frac{1}{2}$ of NE$\frac{1}{4}$ <i>52910</i>
7/12/62 OR 1466/156 <i>in Dec 6-29-16</i></p> <p>[3] D 6/2/62 (NC)
Brown, Edna M.
Strip thru S 400' of SE$\frac{1}{4}$ of SE$\frac{1}{4}$ <i>52921</i>
6/18/62 OR 1450/150</p> <p>[4] D 7/20/62 (NC)
Miller, John D.
NE$\frac{1}{4}$ of SE$\frac{1}{4}$ <i>52922</i>
7/23/62 OR 1472/385</p> <p>[5] D 7/24/62 (HD & NC)
Rothman, Maurice A.
Strip thru N 945' of SE$\frac{1}{4}$ of SE$\frac{1}{4}$ <i>52923</i>
8/1/62 OR 1479/95</p> <p>[6] - E 4/20/57 (ECL)
Heldobler, Alfred O.
Lot 18, Blk A, PALMETTO TERRACE
SUBDIV., PB 39/75, in W$\frac{1}{2}$ of
NW$\frac{1}{4}$ of SW$\frac{1}{4}$ <i>52924</i>
5/17/57 OR 94/322</p> <p>[7] E 1/13/57 (ECL)
Gribble, Jack M.
N 191.94' of W 277.23' of SW$\frac{1}{4}$ of
SW$\frac{1}{4}$ less rd <i>52925</i>
6/4/57 OR 104/656</p> <p>[8] - E 10/15/57 (ECL)
Evans, Ben M. <i>521682</i>
Lots 1 & 18, Blk A; Lots 1, 14 & 11,
Blk B; & Lots 1 & 22, Blk C, PTS,
in NW$\frac{1}{4}$ of SW$\frac{1}{4}$
not recorded</p> <p>[9] E 3/28/57 (ECL)
Hinn, Gordon G.
NW$\frac{1}{4}$ of NW$\frac{1}{4}$ less RR R/W & deed
to RR 5/15/20 <i>52926</i>
6/4/57 OR 105/10</p> <p>[10] E 11/24/67 (LECE) <i>521683</i>
Hellenic Center of Clearwater
S 140.06' of N 332' of W 277.23'
of SW$\frac{1}{4}$ of SW$\frac{1}{4}$ less W 42' for rd
4/3/68 OR 2806/589</p> | <p>[11] E 1/16/57 (ECL) <i>52927</i>
Janinda, John G.
Lots 1&14, Blk B, PTS, in
W$\frac{1}{2}$ of NW$\frac{1}{4}$ of SW$\frac{1}{4}$
6/4/57 OR 104/657</p> <p>[12] - E 3/16/57 (ECL)
Kost, William H.
Lots 12 & 13, Blk B, PTS, in
W$\frac{1}{2}$ of NW$\frac{1}{4}$ of SW$\frac{1}{4}$ <i>52928</i>
6/4/57 OR 104/659</p> <p>[13] E 10/20/72 (ANL)
Vogel, Nelson C. <i>52929</i>
same as Item 9
11/9/72 OR 3919/581</p> <p>[14] E 12/17/56 (ECL) <i>52930</i>
Stich, A. F.
NW$\frac{1}{4}$ of SW$\frac{1}{4}$ S of Coachman Rd
less 4 A. in NE corner
6/4/57 OR 104/660</p> <p>[15] TP 7/24/57 (ECL) <i>521681</i>
Rodgers, E. W.
W 16$\frac{1}{2}$' of S 660' of NW$\frac{1}{4}$
not recorded</p> <p>[16] E 5/18/64 (ANL)
Coachman Properties, Inc.
E 100' of W 135' of N 125' of
NE$\frac{1}{4}$ of NW$\frac{1}{4}$ & W 78' of SW$\frac{1}{4}$ of
NW$\frac{1}{4}$ less W 16.5' of S 660'
6/10/64 OR 1045/45 <i>52909</i></p> <p>Amend 10/3/72 (ANL)
W 78' of SW$\frac{1}{4}$ of NW$\frac{1}{4}$ less W 16'
of S 660' <i>521684</i>
10/24/72 OR 3905/422</p> |
|--|--|



Bartow - Anclote Hot Oil Pipeline (BAP)

SECTION PLAT

Florida

Anclote - Largo 230 KV (ANL)

Higgins - Dieston 115 KV (HD)

Northeast - Curlew 230 KV (NC)

NE:

SEC 18

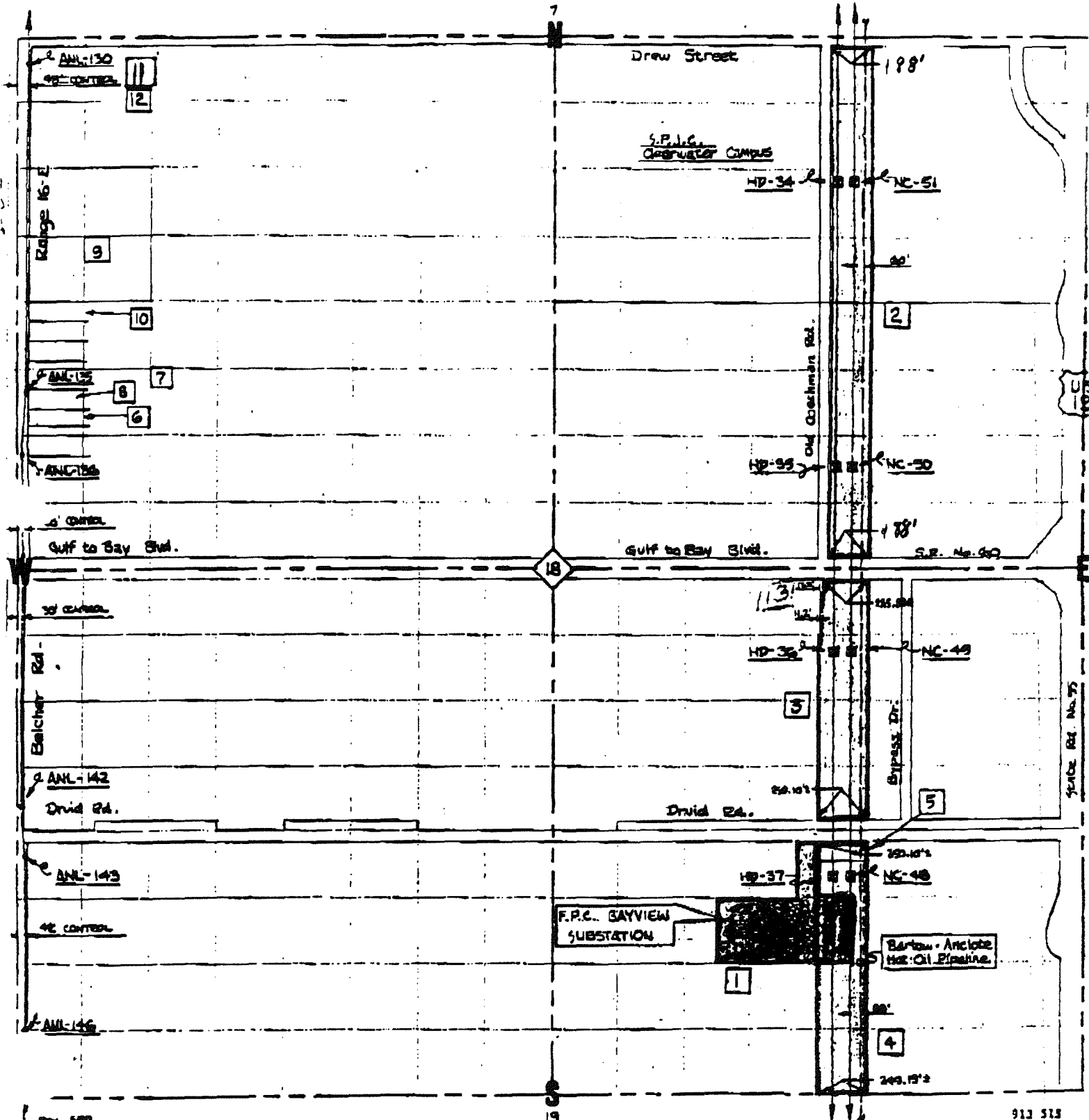
TWP 29S

RGE 16E

Pinellas

COUNTY

SCALE FOR SECTION, } Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 inch: } Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



SCALE FOR QUARTER SECTION. } Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.=1 inch: } Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-320 MAP SHEET

PRONTO LAND MEASURE

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912 515
4-17-68

EASEMENT LIST

SEC. 18 TWP. 29S RGE. 16E Pinellas COUNTY

- 1 D 6/5/61 (Bayview Substation)
Sorin, Murray *521054*
N $\frac{1}{2}$ of E $\frac{1}{2}$ of E-3/4 of SW $\frac{1}{4}$ of SE $\frac{1}{4}$
10/2/61 OR 1262/290
- 2 D 6/2/62 (NC)
Brown, Edna M. *52921*
Port. 225' wide thru E $\frac{1}{2}$ of NE $\frac{1}{4}$
6/18/62 OR 1450/150
- 3 D 4/13/62 (NC)
Pearce, Eugene L., Jr. *521056*
Strip thru W $\frac{1}{2}$ of NE $\frac{1}{4}$ of SE $\frac{1}{4}$
4/20/62 OR 1408/735
- 4 D 6/7/62 (NC)
Saver, Louise Knight *521057*
Strip 1184.64 x 249.19' thru
SE $\frac{1}{4}$ of SE $\frac{1}{4}$
6/13/62 OR 1447/397
- 5 D 4/11/62 (NC)
Lewis, Asa J. *521058*
Port. 250.10 x 150' in SE $\frac{1}{4}$ of SE $\frac{1}{4}$
4/12/62 OR 1402/580
- 6 E 8/17/67 (LECE)
Ayers, Shirley A. *521059*
W 100' of N 80' of S 780' of
SW $\frac{1}{4}$ of NW $\frac{1}{4}$
9/6/67 OR 2669/472
- 7 TP 4/23/57 (ECL)
City Fuel Oil & Coal Co. *521060*
W 78' of W $\frac{1}{2}$ of SW $\frac{1}{4}$ of NW $\frac{1}{4}$ less
N 100' & S 350'
5/17/57 OR 94/312
- 8 E 8/17/67 (LECE)
Bryan, W.E. *521061*
W 100' of N 100' of S 880' of
SW $\frac{1}{4}$ of NW $\frac{1}{4}$ less CR 27
10/10/67 OR 2690/610
- 9 E 2/23/57 (ECL)
Shingler, C.E. *521062*
SW $\frac{1}{4}$ of NW $\frac{1}{4}$ of NW $\frac{1}{4}$
6/4/57 OR 104/643
- 10 E 8/17/67 (LECE)
Lambdin, W.W. *521063*
W 100' of N 100' of SW $\frac{1}{4}$ of NW $\frac{1}{4}$
9/6/67 OR 2669/471
- 11 E 12/22/58 *521064*
Peale, Ruth S. & Norman Vincent
(Easement "referred to" in Dead)
Easement along west side of
S $\frac{1}{2}$ of N $\frac{1}{2}$ of NW $\frac{1}{4}$ of NW $\frac{1}{4}$
1/8/59 OR 503/259
- 12 E(D) 9/7/88
Fernandez, Peter G. & Eva C. *521065*
(Easement "reserved" in QCD)
W 25' of S 220' of N 270' of
E 200' of W 250' of NW $\frac{1}{4}$
9/14/88 OR 6833/1806
- 13 D 10/21/94 *521785*
Clearwater, City of
Port NW $\frac{1}{4}$ of NW $\frac{1}{4}$ of NE $\frac{1}{4}$ of SE $\frac{1}{4}$
11/17/94 OR 8842/2399



Florida
Power

Lake Tarpon-Largo 230 KV (LTL)

Anclote-Largo 230 KV

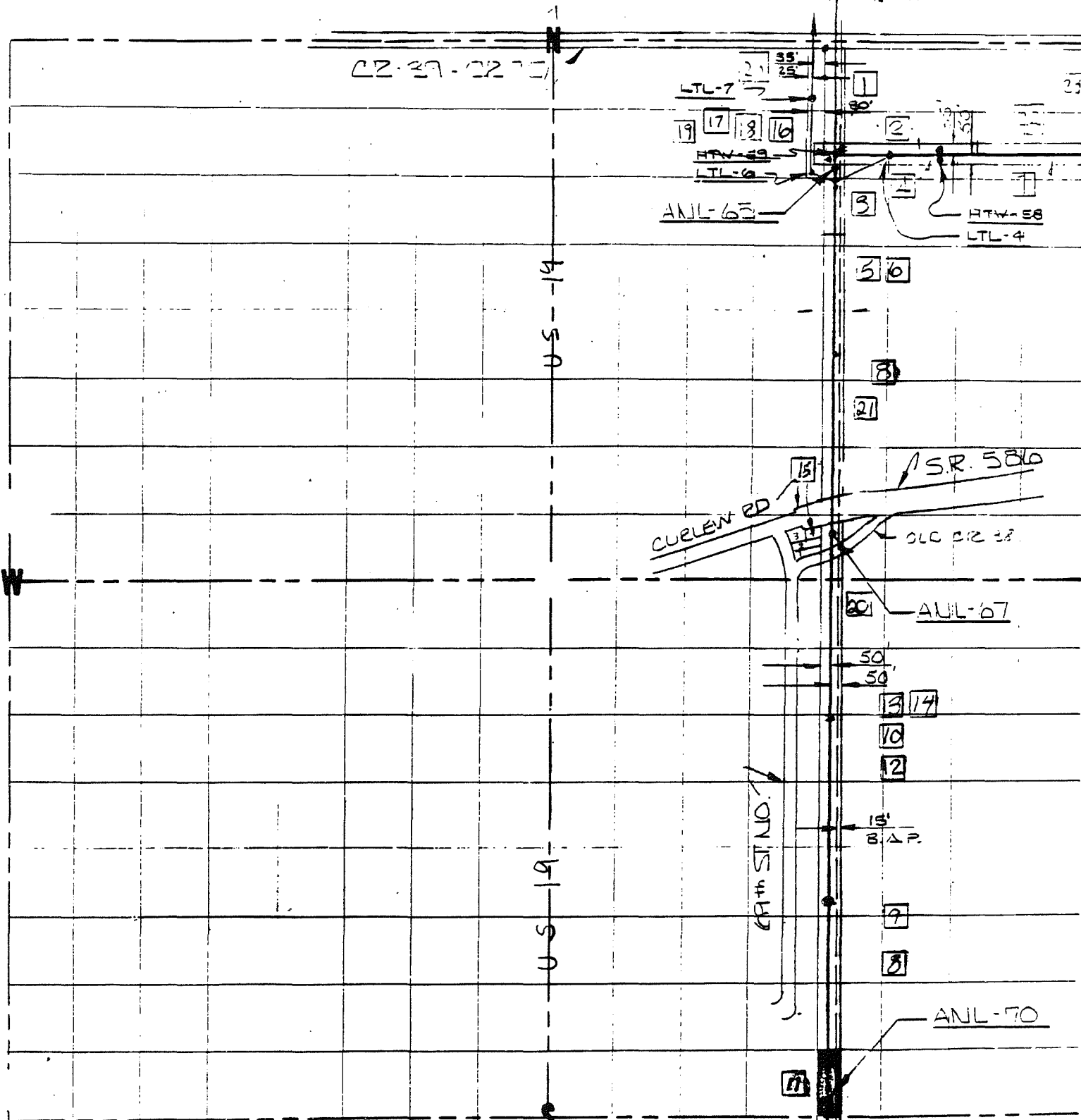
East Clearwater-Tarpon Sps E 115KV

Higgins-Tarpon Sps. W115KV

LINE: Bartow-Anclote Pipeline SEC 18 TWP 28S RGE 16E Pinellas COUN

MAP 13 SECTION PL

SCALE FOR SECTION, } Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. } Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



Rev. 5/80

913 515

SCALE FOR QUARTER SECTION, } Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch } Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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EASEMENT LIST

SEC. 18 TWP 28 S RGE. 16 E, Pinellas COUNTY

- | | |
|--|--|
| <p>E 11/6/62 (HTW & ECTE) 52793✓
Moore, Effie May
100 x 495' in NW$\frac{1}{4}$ of NE$\frac{1}{4}$ & NE$\frac{1}{4}$ of NE$\frac{1}{4}$
11/29/62 OR 1560/42</p> <p>D 5/25/62 (HTW) 52794
15 x 800' in N$\frac{1}{4}$ of NE$\frac{1}{4}$
5/31/62 OR 1437/493</p> <p>E 11/6/62 (HTW & ECTE) 52795✓
Hoadley, John B.
100 x 345' in N$\frac{1}{4}$ of NE$\frac{1}{4}$
11/29/62 OR 1560/40</p> <p>E 5/16/62 (HTW & ECTE) 52796
N 50' of E 798' of area 1,268' x
395.5' in N$\frac{1}{4}$ of NE$\frac{1}{4}$
7/2/62 OR 1459/76</p> <p>E 12/4/62 (HTW & ECTE) 52734✓
Bilgore, David, & Co., Inc.
100' thru NW$\frac{1}{4}$ of NE$\frac{1}{4}$
1/24/63 OR 1600/600</p> <p>SE 2/26/73 (BAP) 52797✓
100' thru NW$\frac{1}{4}$ of NE$\frac{1}{4}$
4/4/73 OR 4012/801</p> <p>E 12/4/62 (HTW) 52798
100 x 660' in NE$\frac{1}{4}$
1/24/63 OR 1600/585</p> <p>E 5/23/39 (ECTE) 52799✓
Boyd, Jessie A.
S$\frac{1}{2}$ of NE$\frac{1}{4}$ & S$\frac{1}{2}$ of SE$\frac{1}{4}$
7/25/46 DB 1086/525</p> <p>E 8/25/52 (ECTE) 52800✓
Muscarella, Frank
100' thru SE$\frac{1}{4}$
8/28/52 DB 1390/268</p> <p>10 E 5/27/39 (ECTE) 52801✓
Brownlow, M. E.
N$\frac{1}{2}$ of SE$\frac{1}{4}$
7/25/46 DB 1086/527</p> <p>11 D 2/9/73 (ANL) 52802✓
Corzetto, Martin
E 100' of W 1,460' of S 330'
of SE$\frac{1}{4}$
3/1/73 OR 3999/275</p> | <p>12 E 1/4/40 (ECTE)
Harvey, Ella 52803✓
N$\frac{1}{2}$ of SE$\frac{1}{4}$
7/25/46 DB 1086/544</p> <p>13 GE 12/21/79 (ANL)
Roddey, Friedel B. 52804✓
NE$\frac{1}{4}$ of SE$\frac{1}{4}$
1/18/80 OR 4971/406</p> <p>14 SE 1/24/73 (BAP) 52805✓
100' thru NE$\frac{1}{4}$ of SE$\frac{1}{4}$
2/9/73 OR 3991/1520</p> <p>15 GE 12/14/79 (ANL) 52806✓
Sabell, John J.
Lot 4, Blk 1, & E 11' of
CURLEW CITY FIRST ADDN.
SUBDIV., PB 69/76
1/18/80 OR 4971/409</p> <p>16 SE 12/11/72 (BAP) 52807✓
Taylor, William H.
W 166' of E 1,460' of
N 495' of NE$\frac{1}{4}$
1/17/73 OR 3976/546</p> <p>17 E 8/13/80 (LTL) 52808✓
Portion in NE$\frac{1}{4}$
9/8/80 OR 5072/830</p> <p>18 SE 5/1/73 (ANL) 52809✓
Leave, Roger Harold
395.5 x 1,268' in NE$\frac{1}{4}$
5/21/73 OR 4031/1280</p> <p>19 SE 1/31/73 (BAP) 52810✓
same as Item 18
2/20/73 OR 3995/908</p> <p>20 SE 2/2/73 (BAP) 52811✓
S. C. Land Co., Inc.
100' thru CURLEW CITY FIRST
REPLAT, PB 63/11
2/28/73 OR 3998/764</p> <p>21 SE 2/2/73 (BAP) 52812✓
100' thru SE$\frac{1}{4}$ of NE$\frac{1}{4}$ N of
SR-586
2/28/73 OR 3998/762</p> |
|--|--|



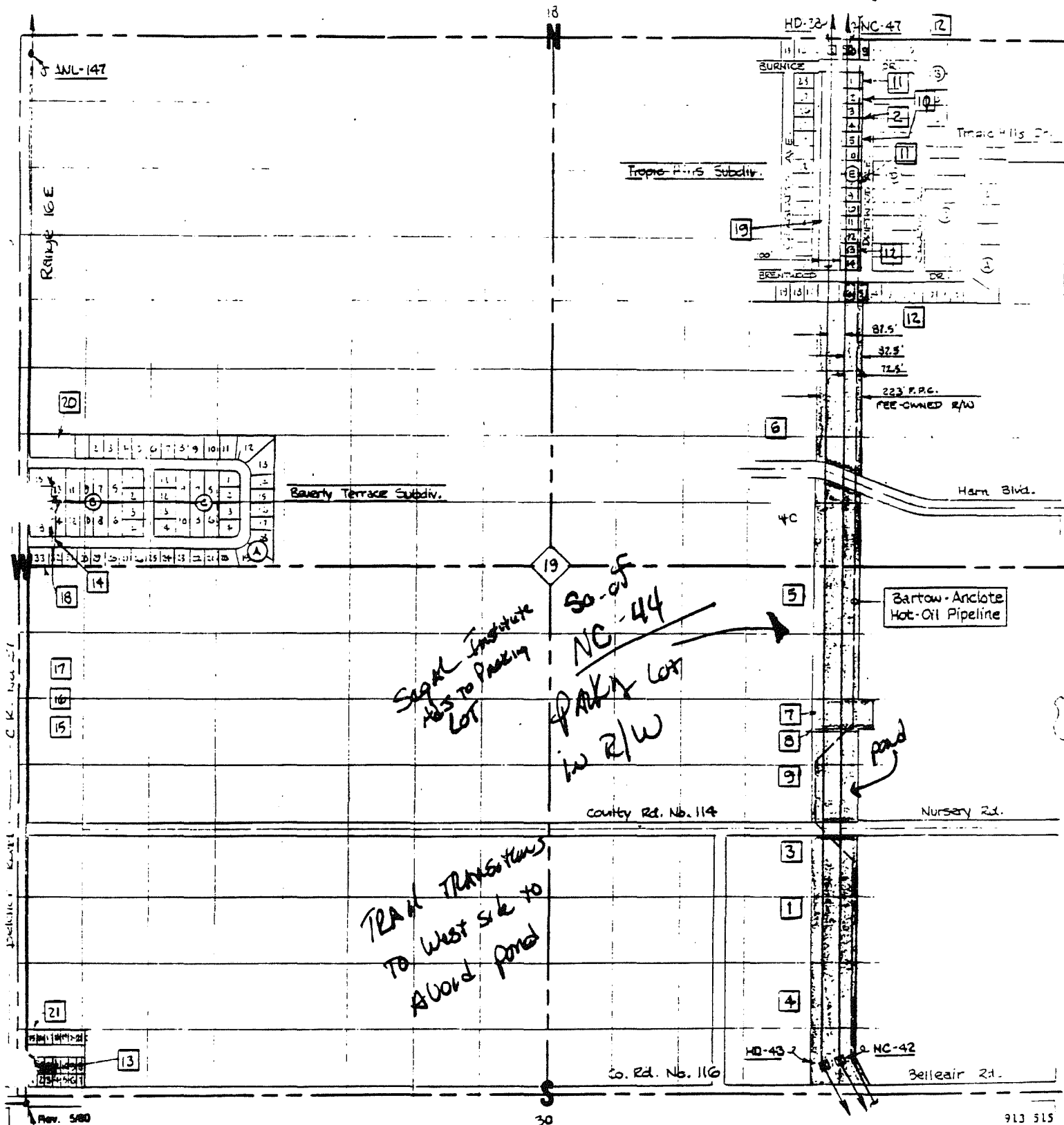
Bartow - Anclote Pipeline (BAP)
 Higgins - Disston 115 KV (HD)
 North 7th - Curlew 230 KV (NC)

MAP 14

SECTION PL

LINE: Anclote - Largo 230 KV (ANL) SEC 19 TWP 29 S RGE 16 E Pinellas COUN

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
 660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2 1/2 acres.



SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
 330 Ft.= 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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EASEMENT LIST

SEC. 19 TWP 29S RGE. 16E, Pinellas COUNTY

- | | |
|--|---|
| <p>1 D 5/14/62 (NC)
Schuster, Loreene E. <i>621026✓</i>
W 223' of NW$\frac{1}{4}$ of SE$\frac{1}{4}$ of SE$\frac{1}{4}$
5/17/62 OR 1223/69</p> | <p>10 D 8/1/61 (NC)
Langere, Ray <i>521035</i>
Lot 2, less N 70'; & Lot 3,
less S 10', Blk E, THUnit 1
8/3/61 OR 1223/628</p> |
| <p>2 D 9/26/61 (NC)
Greene, Elizabeth G. <i>521027✓</i>
Lot 4, less S 10'; & S 10' of
Lot 3, Blk E, Tropic Hills
Unit 1, PB 57/41
(12/28/61 OR 1260/181)</p> | <p>22 D 7/24/61 (NC) <i>521036</i>
Lot 5 & S 10' of Lot 4, Blk E,
THUnit 1
12/7/61 OR 1308/489</p> |
| <p>3 D 5/18/62 (NC) <i>521028✓</i>
Florida Gas Transmission Co.
Port. NW$\frac{1}{4}$ of SE$\frac{1}{4}$ of SE$\frac{1}{4}$
7/3/62 OR 1459/717</p> | <p>23 D 7/24/61 (NC) <i>521037</i>
Lot 6, Blk E, THUnit 1
7/27/61 OR 1219/33</p> |
| <p>4 D 9/7/61 (NC)
Woodland Estates, Inc. <i>521029✓</i>
W 223' of SW$\frac{1}{4}$ of SE$\frac{1}{4}$ of SE$\frac{1}{4}$
9/13/61 OR 1223/693</p> | <p>24 D 8/1/61 (NC) <i>521038</i>
Lot 7, Blk E, THUnit 1
8/31/61 OR 1223/631</p> |
| <p>5 D 3/27/62 (NC)
Weber, Emil <i>521030✓</i>
W 223' of NW$\frac{1}{4}$ of NE$\frac{1}{4}$ of SE$\frac{1}{4}$
4/12/62 OR 1223/578</p> | <p>25 D 7/18/61 (NC)
Schuh, William A. <i>521039</i>
Lot 1, Blk E, THUnit 1
9/28/61 OR 1260/174</p> |
| <p>6 D 3/29/62 (NC)
McMullen, Henry <i>521031✓</i>
E 223' of SE$\frac{1}{4}$ of NE$\frac{1}{4}$
5/1/62 OR 1223/189</p> | <p>26 D 8/1/61 (NC) <i>521040</i>
Lot 8, Blk E, THUnit 1
8/3/61 OR 1223/635</p> |
| <p>7 D 4/3/62 (NC)
Mueller, Helmut <i>521032✓</i>
W 300' of N 149.58' of N$\frac{1}{2}$ of
S$\frac{1}{2}$ of NE$\frac{1}{4}$ of SE$\frac{1}{4}$
5/15/62 OR 1223/693</p> | <p>27 D 8/1/61 (NC) <i>521041</i>
Lot 11, Blk E, THUnit 1
8/3/61 OR 1223/633</p> |
| <p>8 D 4/5/62 (NC)
Reimann, Louise M. <i>521033✓</i>
N 5.91' of S 185.91' of N$\frac{1}{2}$ of
S$\frac{1}{2}$ of NE$\frac{1}{4}$ of SE$\frac{1}{4}$
4/11/62 OR 1223/693</p> | <p>28 D 6/29/61 (NC) <i>521042</i>
Vemay Corp. <i>Sloane</i>
N 70' of Lot 2, Blk E, THUnit
12/7/61 OR 1308/491</p> |
| <p>9 D 3/9/62 (NC) <i>521034✓</i>
Lee, Ray E.
S 485.48' of E 223.20' of NE$\frac{1}{4}$ of SE$\frac{1}{4}$
3/15/62 OR 1223/117</p> | <p>29 D 9/27/61 (NC) <i>521043</i>
Lots 9 & 10, Blk E, THUnit 1
9/28/61 OR 1260/183</p> |
| | <p>30 D 6/29/61 (NC) <i>521044</i>
Lots 15 & 16, Blk A; Lot 10 &
W 45' of Lot 9, Blk B, & Lots
12, 13 & 14, Blk E, THUnit 1
7/3/61 OR 1223/693</p> |
| | <p>31 TP 7/10/57 (ECL)
Van Diver, Lucile <i>521045</i>
Lots 1 & 14, Dunromin Subdiv.
PB 30/93, in S$\frac{1}{2}$ of SW$\frac{1}{4}$ of
SW$\frac{1}{4}$ of SW$\frac{1}{4}$
8/1/57 OR 1223/693</p> |

EASEMENT LIST

SEC. 19 TWP. 29S RGE. 16E, Pinellas COUNTY

- 14 E 2/20/54 (ECL)
Ward, Earl S., Inc. 521046
Lot 18, Blk B, Beverly Terrace
Subdiv., in S $\frac{1}{2}$ of SW $\frac{1}{4}$ of NW $\frac{1}{4}$
less N 115' of W 280'
5/17/57 ~~DB 94/323~~
- 15 TP 4/12/57 (ECL)
Sabo, Ernest G. 521047
N 100' of W 243.02' of Lot 8,
Pinellas Groves, PB 1/55, in
W $\frac{1}{2}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
6/4/57 ~~DB 94/323~~
- 16 TP 5/10/57 (ECL)
Lothar, Rudolf 521048
W 243' of N 440' of Lot 8, PG,
in NW $\frac{1}{4}$ of SW $\frac{1}{4}$, less N 100'
6/4/57 ~~DB 94/323~~
- 17 TP 4/23/57 (ECL)
Lothar, Paul 521049
Lot 8, PG, less W 243' of N 440'
in NW $\frac{1}{4}$ of SW $\frac{1}{4}$
6/4/57 ~~DB 94/323~~
- 18 E 2/25/57 (ECL)
Keeler, George C., TR 521050
Lot 33, Blk A; Lots 15-18, Blk B,
Beverly Terrace Subdiv., PB 41/69,
in S $\frac{1}{2}$ of SW $\frac{1}{4}$ of NW $\frac{1}{4}$, less N 115'
of W 280'
6/4/57 OR 104/658
- 19 E 8/21/54 (HD) 521051
Dallgaard, S. Alton
NE $\frac{1}{4}$ of NE $\frac{1}{4}$
1/11/55 DB ~~DB 94/323~~
- 20 TP 4/10/57 (ECL) 521052
Meyer, A.R.
N 115' of W 280' of S $\frac{1}{2}$ of
SW $\frac{1}{4}$ of NW $\frac{1}{4}$
6/4/57 ~~DB 94/323~~
- 21 TP 6/24/57 (ECL)
Burns, William B. 521053
Lot 15, Dunromin Subdiv., in S $\frac{1}{2}$ of
SW $\frac{1}{4}$ of SW $\frac{1}{4}$ of SW $\frac{1}{4}$
8/1/57 ~~DB 94/323~~



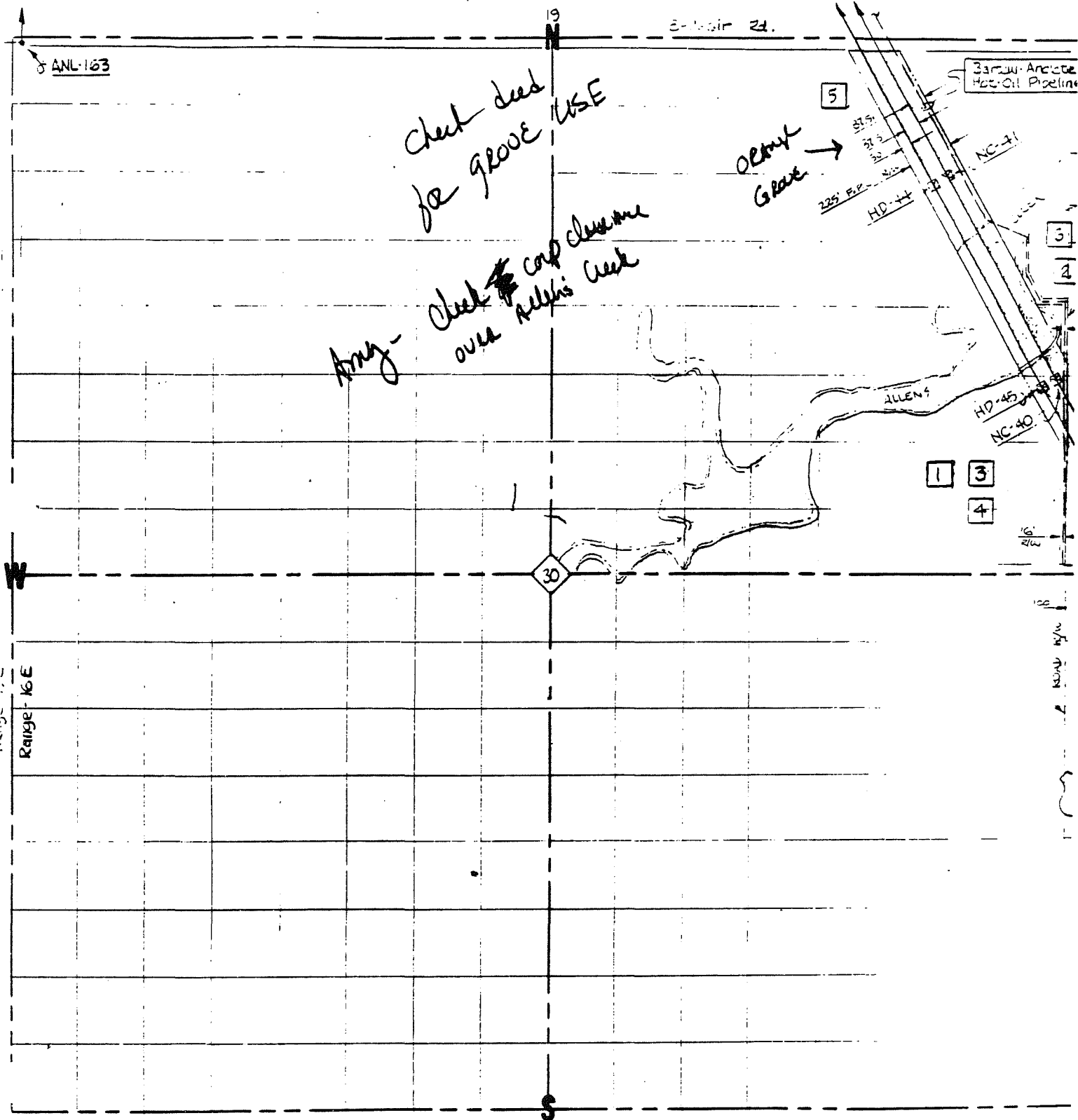
LINE: Higgins - Disston 115 kv (HD)

Pinellas

COUNT

MAP 15 SECTION PLA

Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



31

Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE

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4-27-6

EASEMENT LIST

SEC. 30 TWP. 29S RGE. 16E, Pinellas COUNTY

- 1 E 4/1/40 (HD)
Perkins, V.O. 52998 ✓
SE $\frac{1}{4}$ of NE $\frac{1}{4}$
7/25/46 DB 1086/585
- 2 E 1/8/73 (BAP)
Carpet Fashions of St. Petersburg, Inc.
Port. N $\frac{1}{2}$ of SE $\frac{1}{4}$ of NE $\frac{1}{4}$ W of US 19
& E of HD line 52999 ✓
1/25/73 OR 3982/699
- 3 D 4/4/62 (NC)
Castaneda, Vincent, Jr.
N $\frac{1}{2}$ of SE $\frac{1}{4}$ of NE $\frac{1}{4}$ 521022 ✓
4/6/62 OR 1398/559
- 4 FJ E 2/19/74 (BAP)
Rich, Spencer H. 521023 ✓
Filed: Casey, Wilbur J.
S $\frac{1}{2}$ of SE $\frac{1}{4}$ of NE $\frac{1}{4}$
2/19/74 OR ~~4020/925~~ OT 4020/925
- 5 D 4/4/62 (NC).
Repetto, William A. 521024 ✓
Strip thru NE $\frac{1}{4}$ of NE $\frac{1}{4}$
4/6/62 OR ~~1398/547~~
- 6 E 2/23/73 (BAP) 521025 ✓
25' thru SE $\frac{1}{4}$ of NE $\frac{1}{4}$ of NE $\frac{1}{4}$
4/4/73 OR ~~4022/798~~



Florida
Power

Bartow - Anclote Pipeline (BAP)

Northeast - Curlew 230 KV (NC)

LINE: Higgins - Disston 115 KV (HD)

SEC 29

TWP 29S

RGE 16 E

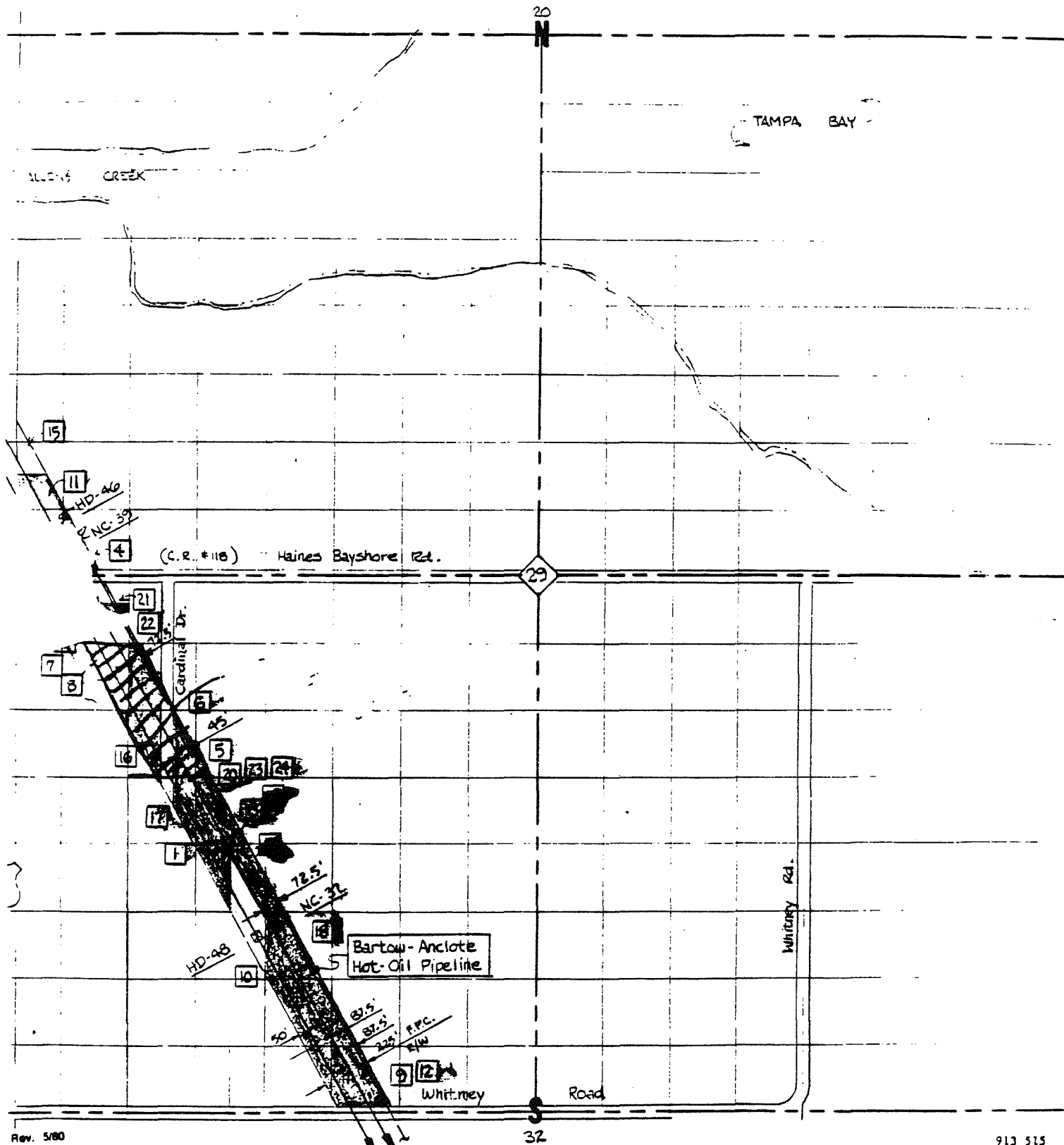
Pinellas

COUNTY

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.

MAP 16

SECTION PLAT



Rev. 5/80

SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft. = 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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EASEMENT LIST

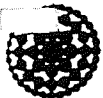
SEC. 29 TWP 29S RGE. 16E, Pinellas COUNTY

- | | | | | | |
|---|------|---|----|------|---|
| 1 | D | 2/7/62 (NC) 521001 ✓
Wells, Henry E.
N $\frac{1}{2}$ of W $\frac{1}{2}$ of E $\frac{1}{2}$ of E $\frac{1}{2}$ of SW $\frac{1}{4}$ of SW $\frac{1}{4}$
2/8/62 OR 1357/732 | 10 | E | 10/14/54 (HD) 521010 ✓
Kendra, Robert J.
E-1/8 of SW $\frac{1}{4}$ of SW $\frac{1}{4}$
1/11/55 DB 1514/49 |
| 2 | D | 1/19/62 (NC) 521002 ✓
Lawrence, Nick
Port. in E $\frac{1}{2}$ of E $\frac{1}{2}$ of E $\frac{1}{2}$ of SW $\frac{1}{4}$ of SW $\frac{1}{4}$
1/22/62 OR 1344/110 | 11 | D | 1/26/55 (HD) 521011 ✓
Kohn, M.C.
Port. in SW $\frac{1}{4}$ of NW $\frac{1}{4}$
1/28/55 DB 1516/579 |
| 3 | E | 12/5/72 (BAP) 521003 ✓
Sunstate Builders, Inc.
20' Strip thru SW $\frac{1}{4}$ of SW $\frac{1}{4}$ of SW $\frac{1}{4}$ of NW $\frac{1}{4}$
1/17/73 OR 3976/544 | 12 | D | 3/12/62 (UC) 521012 ✓
Kamps, Nickolas
E $\frac{1}{2}$ of W $\frac{1}{2}$ of SE $\frac{1}{4}$ of SW $\frac{1}{4}$
3/15/62 OR 1382/116 |
| 4 | E | 2/14/73 (BAP) 521004 ✓
Port. in S $\frac{1}{2}$ of SW $\frac{1}{4}$ of SW $\frac{1}{4}$ of NW $\frac{1}{4}$
2/28/73 OR 3998/750 | 13 | D | 2/8/62 (UC) 521013 ✓
Silva, Aaron S., Jr.
E $\frac{1}{2}$ of E $\frac{1}{2}$ of E $\frac{1}{2}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
2/19/62 OR 1364/106 |
| 5 | D | 12/21/61 (UC)
 Logee, J. Mason
 Port. in S-3/5 of E$\frac{1}{2}$ of W$\frac{1}{2}$ of SE$\frac{1}{4}$ of NW$\frac{1}{4}$ of SW$\frac{1}{4}$
 12/26/64 OR 1321/297 SOLD | 14 | D | 2/8/62 (UC) 521014
Silva, Vernon W.
E $\frac{1}{2}$ of E $\frac{1}{2}$ of E $\frac{1}{2}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
2/19/62 OR 1364/95 |
| 6 | D | 2/10/62 (UC) 521006 ✓
Finch, Alex D.
Port. in E $\frac{1}{2}$ of N $\frac{1}{2}$ of W $\frac{1}{2}$ of E $\frac{1}{2}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
3/2/62 OR 1372/642 | 15 | FJ E | 3/26/63 (NC) 521008 ✓
Sutton, Rose
Filed: Dosch, Nola Tersey
SW $\frac{1}{4}$ of SW $\frac{1}{4}$ of NW $\frac{1}{4}$
3/26/63 OR 1644/220 |
| 7 | FJ E | 4/6/64 (HD) 521007 ✓
Dosch, Nola Tersey
Port. 100' thru N $\frac{1}{2}$ of N $\frac{1}{2}$ of N $\frac{1}{2}$ of SW $\frac{1}{4}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$; & S $\frac{1}{2}$ of N $\frac{1}{2}$ of N $\frac{1}{2}$ of SW $\frac{1}{4}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$; & S $\frac{1}{2}$ of NW $\frac{1}{4}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$; less W 100'
4/6/64 OR 1919/174 | 16 | D | 2/16/62 (NC)
 Gallot, Charles A.
 Port. W$\frac{1}{2}$ of N$\frac{1}{2}$ of W$\frac{1}{2}$ of E$\frac{1}{2}$ of NW$\frac{1}{4}$ of SW$\frac{1}{4}$
 2/20/62 OR 1365/56 SOLD |
| 8 | FJ E | 3/26/63 (NC) 521008A
Port. 125' thru S $\frac{1}{2}$ of NW $\frac{1}{4}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
3/26/63 OR 1644/220 | 17 | D | 11/22/61 (UC) 521016 ✓
Sievert, Hazen J.
N-2/5 of E $\frac{1}{2}$ of W $\frac{1}{2}$ of SE $\frac{1}{4}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
6/29/62 OR 1358/301 |
| 9 | D | 3/14/62 (UC) 521009 ✓
Alderman, James W.
Port. E $\frac{1}{2}$ of W $\frac{1}{2}$ of SE $\frac{1}{4}$ of SW $\frac{1}{4}$
3/26/62 OR 1382/736 | 18 | D | 3/14/62 (UC) 521017
Alderman, Elwood B.
Port. W $\frac{1}{2}$ of W $\frac{1}{2}$ of SE $\frac{1}{4}$ of SW $\frac{1}{4}$
3/16/62 OR 1382/734 |
| | | | 19 | D | 1/29/62 (UC) 521018 ✓
Clark, Alfred B.
Port. N $\frac{1}{2}$ of S $\frac{1}{2}$ of N $\frac{1}{2}$ of NW $\frac{1}{4}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
1/31/62 OR 1351/411 |

EASEMENT LIST

SEC. 29 TWP. 29S RGE. 16E, Pinellas COUNTY

- [20] E 1/23/73 (BAP) *521019* ✓
Crawford, Glenn
Port. S 336' of W $\frac{1}{2}$ of E $\frac{1}{2}$ of E $\frac{1}{2}$
of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
2/9/73 ~~OR 3991/1512~~
- [21] FJ E 3/26/63 (NC) *521007* *Same as #7* ✓
Christ, Virginia B.
Filed: Dosch, Nola Tersey
Port. N $\frac{1}{2}$ of S $\frac{1}{2}$ of NW $\frac{1}{4}$ of NW $\frac{1}{4}$ of
SW $\frac{1}{4}$; & N $\frac{1}{2}$ of NW $\frac{1}{4}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
4/6/64 OR 1902/24
- [22] FJ E 3/26/63 (HD) *521008* *Same as #8* ✓
Port. N $\frac{1}{2}$ of S $\frac{1}{2}$ of NW $\frac{1}{4}$ of NW $\frac{1}{4}$ of
SW $\frac{1}{4}$; & N $\frac{1}{2}$ of NW $\frac{1}{4}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
3/26/63 OR ~~1902/220~~
- [23] D 12/27/61 (UC) ~~*521020*~~ *SOLD*
Register, Abner B.
100' thru S 336' of W $\frac{1}{2}$ of E $\frac{1}{2}$ of
E $\frac{1}{2}$ of NW $\frac{1}{4}$ of SW $\frac{1}{4}$
12/29/61 OR ~~1902/235~~
- [24] E 12/27/61 (NC) ~~*521020*~~ *521021* ✓
225' thru W $\frac{1}{2}$ of E $\frac{1}{2}$ of E $\frac{1}{2}$ of
NW $\frac{1}{4}$ of SW $\frac{1}{4}$ less S 336'
1/8/62 OR ~~1333/619~~
- 25 FJ E 4/23/73 (BAP) *521023A* ✓
Christ, Virginia B.
Filed: Casey, Wilbur J.
Parcel III - Trs A & B
4/23/73 OR 4020/925
- 26 E 12/20/96 ✓
D/L Motor Co.
Taken back at sale
9/2/97 OR 9823/2262



Florida
Power

Northeast - Corlew 230 kv (NC)

Higgins - Disston 115 kv (HD)

LINE: Bartow-Anclote Pipeline (BAP)

SEC 32

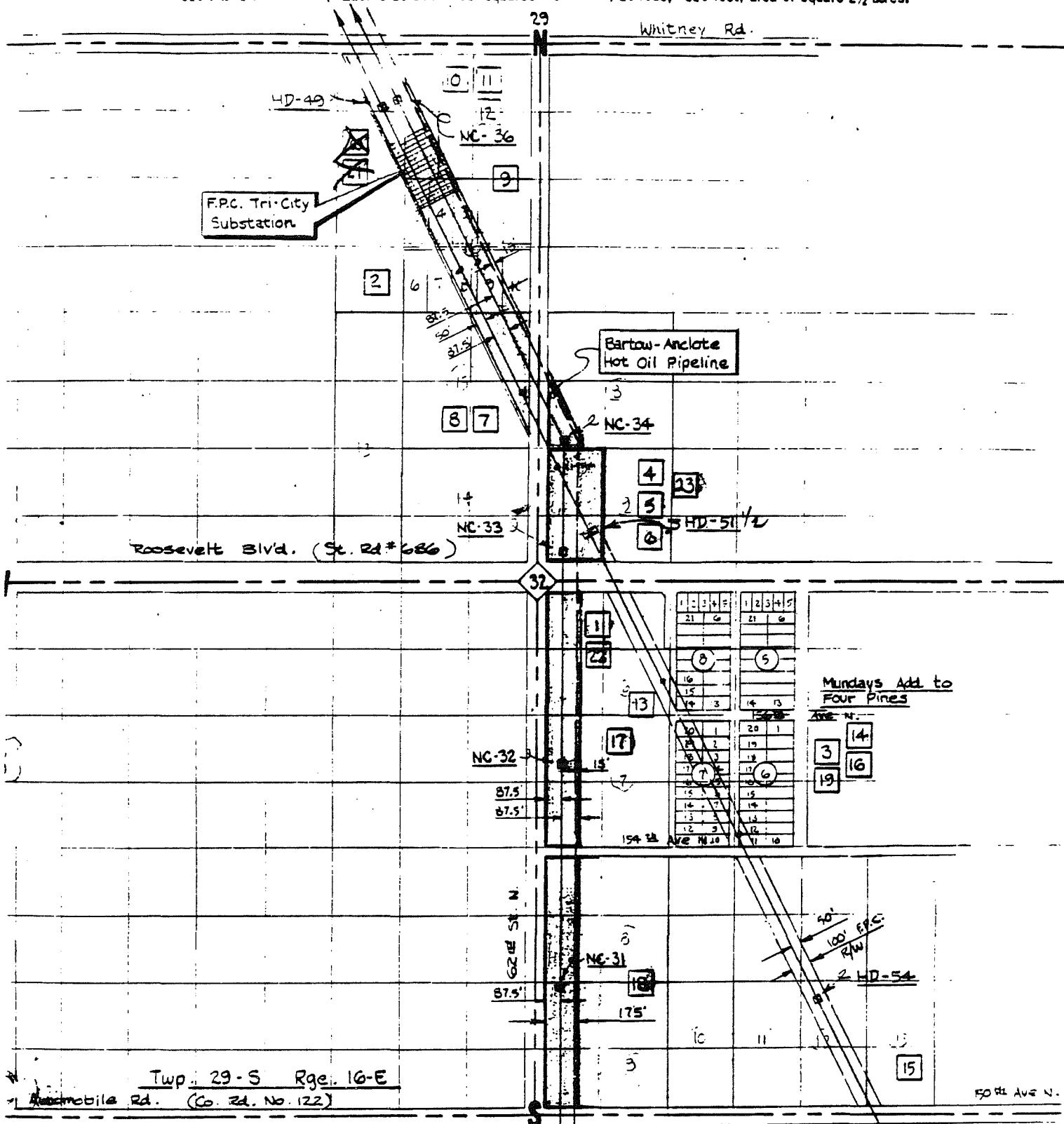
TWP 29 S

RGE 16 E

Pinellas

COUNTY

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2 1/2 acres.



Twp. 29-S Rge. 16-E

Automobile Rd. (Co. Rd. No. 122)

Rev. 5/80

Twp. 30-S Rge. 16-E

SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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A. ANDERSON 4-3-1

EASEMENT LIST

SEC. 32 TWP. 29S RGE. 16E, Pinellas COUNTY

- | | |
|---|---|
| <p>[1] D 3/29/62 (NC)
Bowman, Thomas E. <i>52971</i> ✓
Lot 6, Pinellas Groves, Inc.,
PB 1/55, in SE¼
3/30/62 OR 1393/367</p> | <p>[10] D 2/14/62 (Tri-City Substation)
Filed Reynolds, Chas S. <i>52980</i>
SW cor of Lot 1, PG, in NW¼
2/16/62 OR 1363/125</p> |
| <p>[2] D 6/15/61 (NC)
McNeely, M.S. <i>52972</i> ✓
Lots 6-10 of subdiv of Lot 16,
Pinellas Groves Co. Plat,
PB 8/14, in NW¼
6/19/61 1393/367</p> | <p>[11] D 2/13/62 (Tri-City Substation)
Filed: Severance, Irene (NC)
W¼ of Lot 1, PG, in NW¼ <i>52981</i>
2/16/62 OR 1363/194</p> |
| <p>[3] E 7/6/54 (HD)
Grimme, Chester M. <i>52973</i>
Lots 1 & 20, Blk 7, Munday's Addn.
to Four Pines, PB 16/115, E¼ of
NW¼ of SE¼
1/11/55 1514/58</p> | <p>[12] D 3/26/73 (Tri-City Substation)
Filed: Abilities, Inc. of FL
Triangle in NE cor. of Lot 2,
PG, in NW¼ (BAP) <i>52982a</i>
3/26/73 OR 4008/358</p> |
| <p>[4] E 5/21/54 (HD) <i>52974</i> ✓
Geiselman, Wilbert H.
E¼ of Lot 9, PG, less W 100' of
S 349' & rd r/w in SW¼ of NE¼
1/11/55 DE 1514/56 (triangle)</p> | <p>[13] D 2/8/62 (NC)
Snakenberg, Edward M. <i>52984</i>
SW cor. of Lot 6, PG, in SE¼
4/17/62 OR 1405/582</p> |
| <p>[5] D 2/6/62 (NC)
Francis, John P. <i>52975</i>
208' strip thru Lots 8&9, PG, in NE¼
2/8/62 OR 1367/30</p> | <p>[14] E 6/25/54 (HD)
Ofenloch, Jacob J. <i>52985</i>
Lots 16&17 in Blk 8, MatoFP,
in E¼ of NW¼ of SE¼
1/11/55 1514/129</p> |
| <p>[6] D 3/27/63 (HD)
entire W¼ of Lot 9, PG, in NE¼,
less Item 5 above <i>52976</i>
3/28/63 OR 1645/237</p> | <p>[15] E 7/27/54 (HD)
Sweat, L.S., Jr. <i>52986</i>
Lot 13 in SE¼ of SE¼
1/11/55 1514/132</p> |
| <p>[7] D 3/1/55 (HD)
Jones, William E. <i>52977</i>
E¼ of N¼ of Lot 15 in NW¼
3/18/55 1514/130</p> | <p>[16] E 10/18/54 (HD)
Wemlinger, Dorothea <i>52987</i>
Lot 18, Blk 8, MatoFP, in E¼
of NW¼ of SE¼
1/11/55 1514/130</p> |
| <p>[8] D 2/6/62 (NC)
Hall, J. Aldrich <i>52978</i>
S¼ of Lot 15, PG, in NW¼
2/16/62 OR 1363/187</p> | <p>[17] D 4/2/62 (NC) <i>52988</i>
Wilson, James Arthur
Lot 7, PG, in SE¼
4/10/62 OR 1400/681</p> |
| <p>[9] D 4/12/62 (Tri-City Substation) <i>52979</i>
Filed: Smith, Myrtle L. (NC)
Lots 1-5 of subdiv. of Lot 16
PGCP, in NW¼
6/27/62 OR 1456/325</p> | <p>[18] D 1/12/62 (NC)
Realty Development Corporation
of America <i>52989</i>
W¼ of Lots 8 & 9, PG, in
SW¼ of SE¼
1/23/62 OR 1345/271</p> |

EASEMENT LIST

SEC. 32 TWP. 29S RGE. 16E, Pinellas COUNTY

- 19 E 11/2/54 (HD) *52990*
Florida Conference of Evangelical
United Brethern Church
Lots 10-16, Blk 6; Lots 2-7 & 18
& 19, Blk 7; Lots 13-15, Blk 8;
E½ of NW¼ of SE¼, MATOFP
1/11/55 ~~DB 1514/131~~
- 20 E 10/13/56 (HD) *52991*
Wright, Ed C. **RELEASED**
100' thru E½ of W½ of NE¼ of NW¼
10/23/56 DB 1626/547
- 21 E 2/3/62 (NC) **RELEASED** *52992*
Abilities, Inc. of Florida
125' thru Lot 2, PG. in NW¼
2/27/62 ~~DB 1370/~~
- 22 E 5/26/54 (HD) *52993*
Bowman, Thomas E.
Lot 6 in NW¼ of SE¼
1/11/55 ~~DB 1086/563~~
- 29 TP 1/4/62 (NC) *52994*
Snakenberg, E.M.
S 350' of Lot 6, PG
1/17/62 ~~DB 1370/~~
- 23 E 7/24/39 (HD)
Mitchell, Lillian B. *52995*
Lots 8&9 in NE¼
7/25/46 Deed 1086/563
- 24 E 2/13/79 (Access Easement at *52983*
Tri-City Substation)
Abilitites, Inc. **RELEASED**
3/8/79 OR 4823/1375
- 25 E 5/22/54 (HD) *52996*
Smith, Myrtle L.
Filed: Smith, Myrtle L. OR 1456/325
superseded by Fee Purchase
1-11-55 DB 1514/53
- 26 E 4/18/73 (BAP) **RELEASED** *52997*
Abilities Inc.
N½ of Lot 2, PG
5/21/73 OR 4031/1268
- 27 E 5/21/54 (HD) *521715*
Geiselman, C. W.
W 100' of S 349' of E½ of Lot 9
1/11/55 DB 1514/55
- 28 SE 1/2/63 (HD)
Kok-Alblas, Clyde Willard *521715*
W 100' of S 349' of E½ of Lot 9
less rd in SW¼ of NE¼
2/18/63 OR 1617/351
- 29 D 3/15/55
Macassey, Bess *521715*
E½ of W½ of NE¼ of NW¼ aka
Lot 2 in NW¼
DB 1527/371
- SOLD**



Florida
Power

Bartow-Anclote Pipeline (BAP)

Higgins-Disston 115 KV (HD)

Ulmerton-Largo 230 KV (UL)

Northeast-Curlew 230 KV (NC)

SECTION PL

MAP 18

SEC 5

TWP

30S

RGE

16E

Pinellas

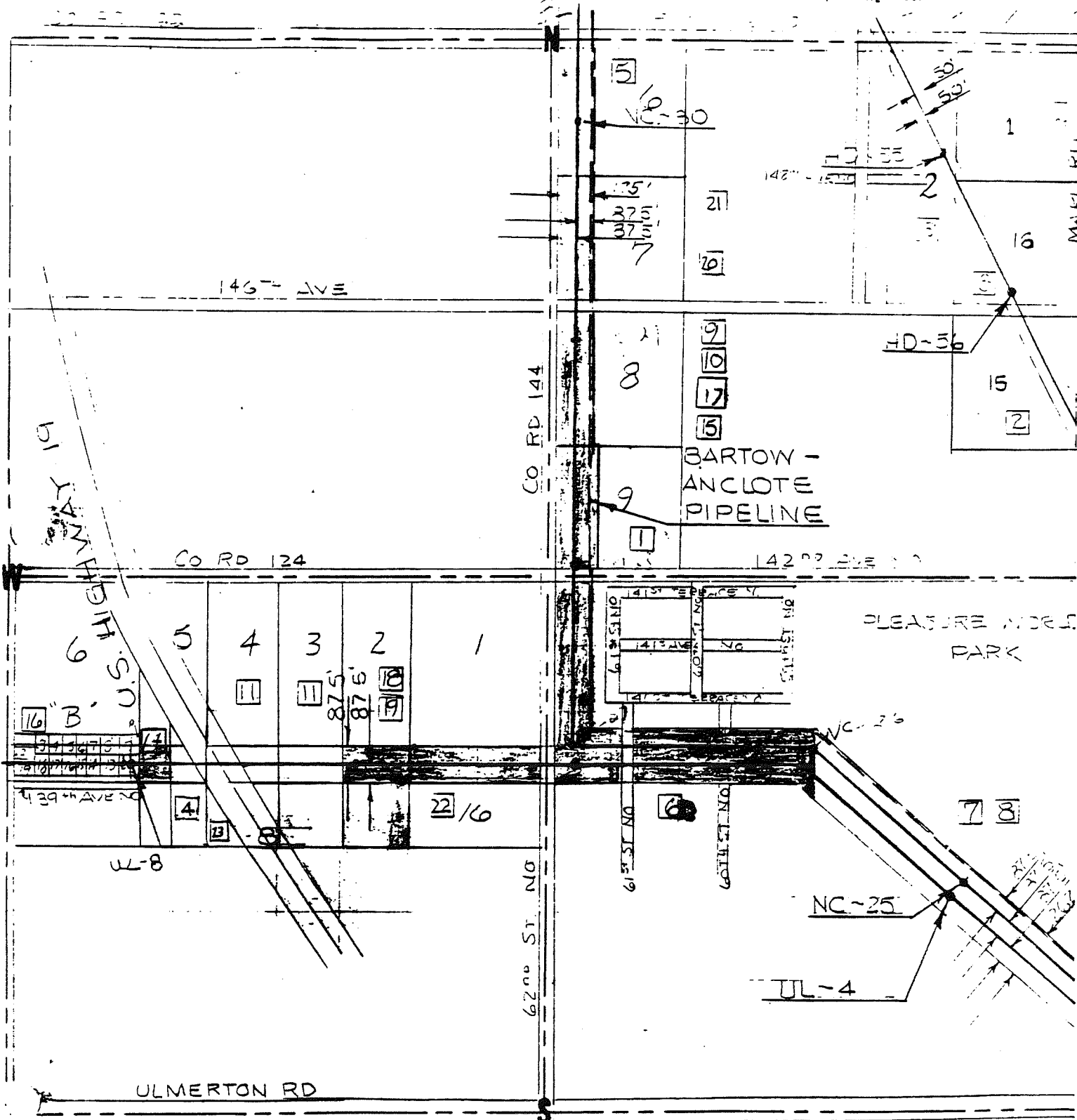
COUNT

SCALE FOR SECTION,

660 Ft.=1 Inch.

Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.

Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



Rev. 5/77

SCALE FOR QUARTER SECTION,

330 Ft.=1 Inch

Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.

Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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12
115
912 515

EASEMENT LIST

SEC. 5 TWP. 30 S RGE. 16 E, Pinellas COUNTY

- | | |
|---|---|
| <p>[1] D 11/14/61 (UC) <i>521072a</i>
McMullen, J. Tweed, TR
W 225' of Lot 9, PINELLAS GROVES,
PB 1/55, in NE$\frac{1}{4}$
11/22/61 OR 1297/739</p> <p>[2] E 8/10/54 (HD) <i>521069</i>
McMullen Dairy, Inc.
Lot 15, in SE$\frac{1}{4}$ of NE$\frac{1}{4}$
1/11/55 DB 1514/136</p> <p>[3] E 8/19/54 (HD) <i>521073</i>
Katchmar, Minnie L.
Lot 16, in NE$\frac{1}{4}$ of NE$\frac{1}{4}$
1/11/55 DB 1514/134</p> <p>[4] FJ E 2/4/63 (NC)
Johnson, Etta <i>52070</i>
175' thru E$\frac{1}{2}$ of Lot 5, PG, in SW$\frac{1}{4}$
2/4/63 OR 1608/39</p> <p>[5] D 1/3/62 (NC)
Grabowski, C. E. <i>521074</i>
W 208' of Lot 6, PG, in NE$\frac{1}{4}$
1/4/62 OR 1331/236</p> <p>[6] D 12/20/61 (NC) <i>521075</i>
Faulkenburg, Clarence W., TR
NW$\frac{1}{4}$ of SE$\frac{1}{4}$
12/26/61 OR 1321/310</p> <p>[7] FJ E 3/9/62 (NC)
Casey, Wilbur J. <i>52109267a</i>
E$\frac{1}{2}$ of SE$\frac{1}{4}$
3/21/62 OR 1387/315</p> <p>[8] FJ E 2/19/74 (BAP)
E$\frac{1}{2}$ of SE$\frac{1}{4}$ - <i>Parcel II 521023a</i>
2/19/74 OR 4139/1548
2/20/74</p> <p>[9] D 1/5/62 (NC)
Wenzel, Albert M. <i>521076</i>
W 208' of S$\frac{1}{2}$ of NE$\frac{1}{4}$ of Lot 8,
PG, in NE$\frac{1}{4}$
1/24/62 OR 1346/114</p> <p>[10] D 12/13/61 (NC)
Roberts, Myrtle D. <i>521077</i>
NW$\frac{1}{4}$ of SW$\frac{1}{4}$ of NE$\frac{1}{4}$ or NE$\frac{1}{4}$ of NE$\frac{1}{4}$
of Lot 8, PG
12/15/61 OR 1314/663</p> | <p>[11] FJ E 6/25/62 (NC) <i>521085a</i>
Stein, Leonard L.
Filed: Marriott, J. Alfred, Jr.
175' thru Lots 3 & 4, PG,
in SW$\frac{1}{4}$
6/25/62 OR 1455/390</p> <p>[12] E 5/25/54 (HD) <i>521078</i>
Korda, Peter
W$\frac{1}{2}$ of Lot 1, in NE$\frac{1}{4}$ of NE$\frac{1}{4}$
1/11/55 DB 1514/133</p> <p>[13] E 6/17/54 (HD) <i>521079</i>
St. Petersburg, City of
Lot 2 in E$\frac{1}{2}$ of W$\frac{1}{2}$ of NE$\frac{1}{4}$ of NE$\frac{1}{4}$
1/11/55 DB 1514/19</p> <p>[14] D 12/14/61 (UL)
Von Der Hyde, Henry <i>521080</i>
175' thru W$\frac{1}{2}$ of Lot 5,
PG in SW$\frac{1}{4}$
12/28/61 DB 1324/78</p> <p>[15] D 3/22/62 (UL)
Kuehl, Walter J. <i>521081</i>
W 208' of S$\frac{1}{2}$ of S$\frac{1}{2}$ of Lot 8,
PG, in NE$\frac{1}{4}$
4/19/62 OR 1407/658</p> <p>[16] FJ E 6/25/62 (UL)
Howe, Mary E. <i>521085a</i>
175' thru Lots 1-19, Blk B,
BRENTWOOD SUBDIV., PB 30/62,
in NW$\frac{1}{4}$ of SW$\frac{1}{4}$
6/25/62 DB 1455/390</p> <p>[17] D 11/2/61 (UL)
Collier, Beary <i>521082</i>
W 208' of NE$\frac{1}{2}$ of S$\frac{1}{2}$ of Lot 8,
PG, in NE$\frac{1}{4}$
11/7/61 OR 1287/712</p> <p>[18] D 12/6/61 (UL)
Andrews, Lois W. <i>521083</i>
175' thru W-2/3 of Lot 2,
PG, in SW$\frac{1}{4}$
12/11/61 DB 1310/77</p> <p>[19] D 12/11/61 (UC) <i>521084</i>
Badrian, Arthur E.
175' thru E-1/3 of Lot 2,
PG, in SW$\frac{1}{4}$
3/13/62 DB 1379/668</p> |
|---|---|

(con'd)

EASEMENT LIST

SEC. 5 TWP 30 S RGE. 16 E, Pinellas COUNTY

- [20] D 12/4/61 (UC) *521086*
Bray, Arthur R.
W 208' of N $\frac{1}{2}$ of Lot 7, PG,
in NW $\frac{1}{4}$ of NE $\frac{1}{4}$
12/13/61 OR 1312/298
- [21] D 12/28/61 (UC) *521087a*
Bleezarde, Warren E., Jr.
W 208' of S $\frac{1}{2}$ of Lot 7, PG,
in NW $\frac{1}{4}$ of NE $\frac{1}{4}$
12/29/61 OR 1328/159
- [22] D 12/5/61 (UC) *521088a*
Buell, D. Judd, Jr.
175' thru Lot 16, PG, in
NE $\frac{1}{4}$ of SW $\frac{1}{4}$
12/7/61 OR 1308/607
- [23] E 5/10/62 (UC) *521089*
Buchowiecki, Aldea
Port. 175' thru part of Lot 4,
PG, in SW $\frac{1}{4}$, S & W of US 19
5/21/62 ~~OR 1431/134~~



Florida
Power

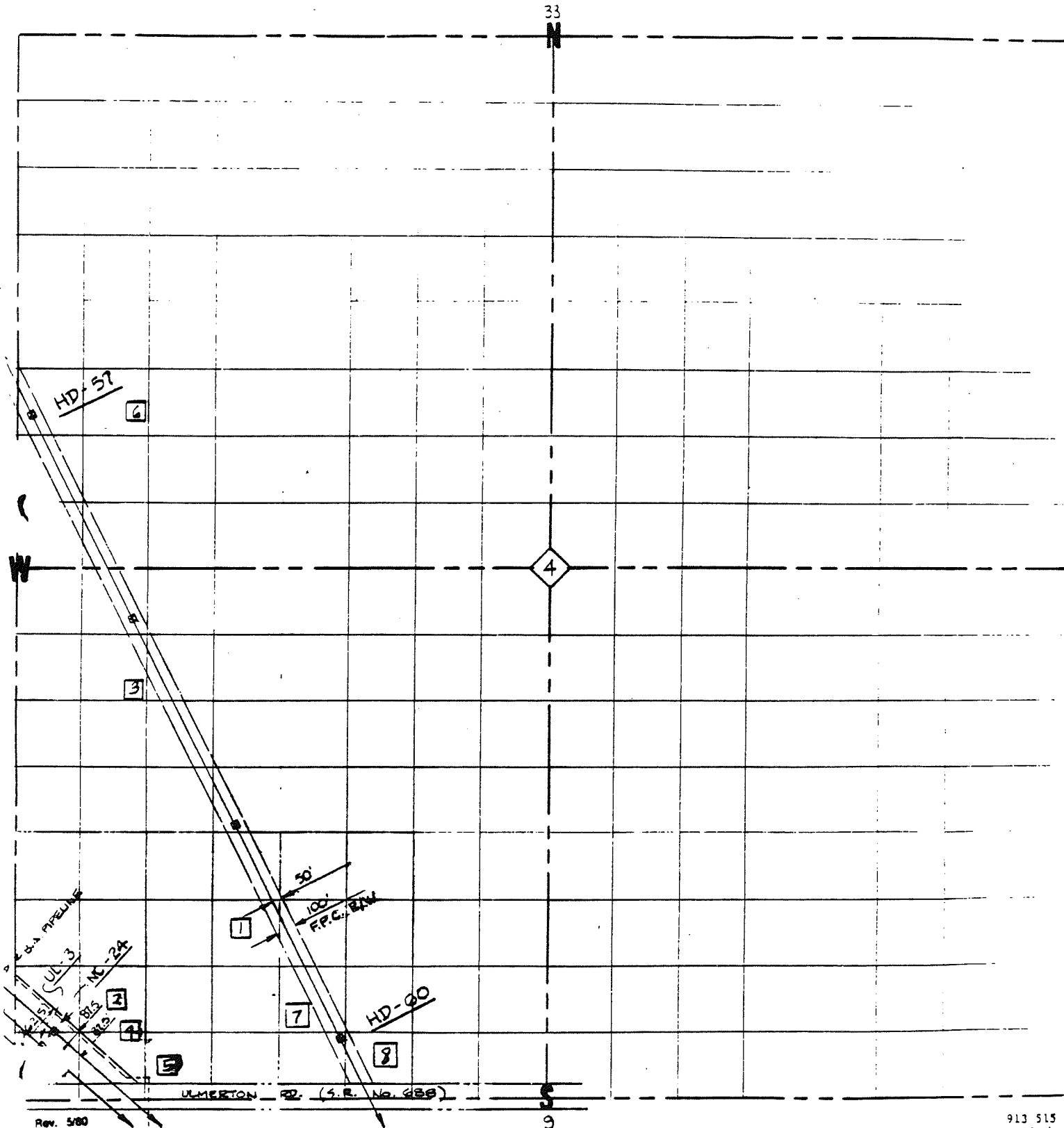
Northeast Curlew 230 KV (NC)
Higgins-Disston 115 KV (HD)
Bartow-Anclote Pipeline (BAP)

SECTION PL

MAP 19

LINE: Ulmerton Largo 230 KV (UL) SEC 4 TWP 30 S RGE 16 E , Pinellas COUN

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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M. ANDERSON 3-3-11

EASEMENT LIST

SEC. 4 TWP 30 S RGE. 16 E, Pinellas COUNTY

- 1] E 6/24/54 (HD) *521055*
Phillips, Joseph N.
Lot 11, PINELLAS GROVES, PB 1/55,
in SW $\frac{1}{4}$ of SW $\frac{1}{4}$, E of Canal
1/11/55 DB 1514/10
- 2] FJ E 4/23/73 (BAP)
Casey, Wilbur J. *521023* *mn 29+29-16*
Lots 8&9, PG, in SW $\frac{1}{4}$ of SW $\frac{1}{4}$
2/19/74 OR 4139/1548 OT OR 4020/925
- 3] E 10/13/54 (HD) *521066*
Lot 11, PG, in SW $\frac{1}{4}$ of SW $\frac{1}{4}$ & NW $\frac{1}{4}$ of SW $\frac{1}{4}$
1/11/55 DB 1514/135
- 4] FJ E 3/9/62 (UL) *521067* *mn 5-30-16*
262 $\frac{1}{2}$ ' thru Lots 8 & 9, PG,
in SW $\frac{1}{4}$ of SW $\frac{1}{4}$
3/21/62 OR 1387/315
- 5] E 2/21/73 (BAP)
Ace Outdoor Advertising, Inc. *521068*
S 30' of W 105' of Lot 10, PG, E of canal
3/16/73 ~~OR~~ 4004/1645
- 6] E 8/10/54 (HD)
McMullen Dairy, Inc. *521069*
SW $\frac{1}{4}$ of NW $\frac{1}{4}$
1/11/55 DB 1514/136
- 7] E 7/2/54 (HD)
MacDonald, Nell B. *521070*
Lot 12, PG, in SE $\frac{1}{4}$ of SW $\frac{1}{4}$
1/11/55 DB 1514/29
- 8] E 7/29/54 (HD) *521071*
Florida, State of - Pinellas County
Lot 13, PG, in SE $\frac{1}{4}$ of SW $\frac{1}{4}$
1/11/55 ~~DB 1514/30~~

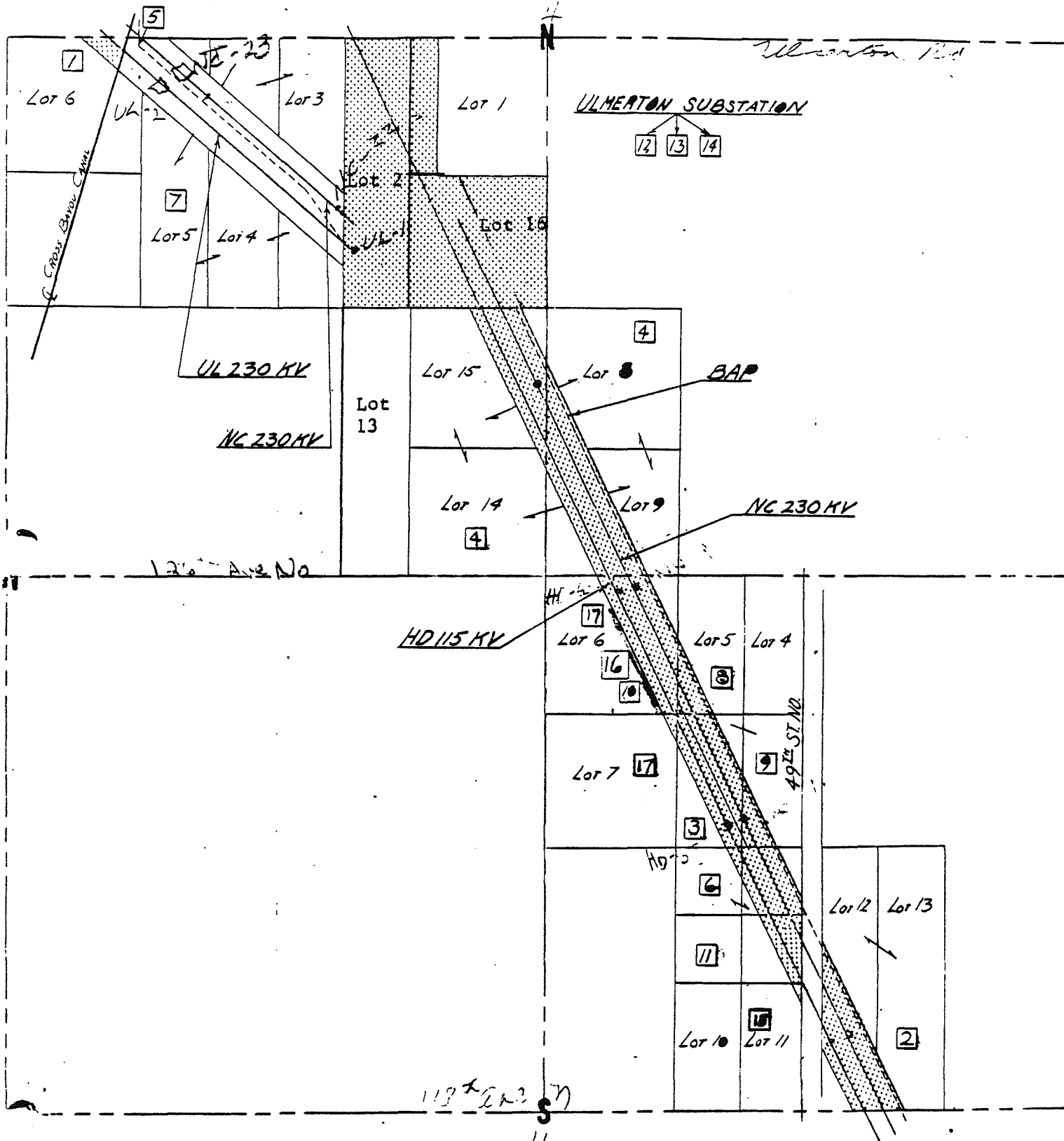


Bartow-Analote pipeline (BAP)
 Ulmerton-Largo 230 KV (UL)
 Higgins-Disston 115 KV (HD)
 LINE: Northeast-Curlew 230 KV (NC)

MAP 20

SEC 9 TWP 30S RGE 16E Pinellas COUNTY

SCALE FOR SECTION, } Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
 660 Ft.=1 Inch. } Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2 1/2 acres.



Rev. 5/77

SCALE FOR QUARTER SECTION, } Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
 330 Ft.= 1 Inch } Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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3/6/78
 DCS

EASEMENT LIST

SEC. 9 TWP. 30 S RGE. 16 E, Pinellas COUNTY

- | | |
|--|--|
| <p>[1] D 12/7/61 (UL) <i>521095</i>
Daniel, Cathleen W.
Port. Lot 6, PINELLAS GROVES,
PB 1/55, in NW$\frac{1}{4}$
12/20/61 OR 1318/251</p> <p>[2] D 1/22/62 (UL) <i>521096a</i>
Disston Boulevard Land Co.
225' thru Lots 12 & 13, PG,
in SE$\frac{1}{4}$
1/29/62 OR 1349/240</p> <p>[3] D 3/15/55 (HD) <i>521097</i>
Dorschler, Leonard L.
100' thru S$\frac{1}{2}$ of Lots 4
& 5, PG, in SE$\frac{1}{4}$
3/31/55 DB 1527/369</p> <p>[4] ✓ D 1/4/73 (BAP) <i>521098</i>
Gorby, Robert
225' thru Lots 8 & 9, PG, in NE$\frac{1}{4}$
& Lots 14 & 15, PG, in NW$\frac{1}{4}$
1/5/73 OR 3967/804</p> <p>[5] SE 1/10/73 (BAP)
Marriott, Robert M. <i>521099</i>
Port. of NW$\frac{1}{4}$
1/18/73 OR 3977/494</p> <p>[6] D 2/15/73 (BAP) <i>521100</i>
Miserere Guild, Inc.
225' thru N$\frac{1}{4}$ of Lots 10 &
11, PG, in SE$\frac{1}{4}$
2/23/73 OR 3997/31</p> <p>[7] FJ E 6/25/62 (UL) <i>521085</i>
Marriott, J. Alfred, Jr.
262.5' thru Lots 3-5 & port.
Lot 6, PG in NW$\frac{1}{4}$
6/2./62 OR 1455/390</p> <p>[8] D 10/31/61 (UL) <i>521101</i>
Kieth, Edith B. Smith
Port. N$\frac{1}{2}$ of Lot 5, PG, in SE$\frac{1}{4}$
11/10/61 OR 1290/515</p> <p>[9] D 10/31/61 (UL)
125' thru S$\frac{1}{2}$ of Lots 4 & 5, PG,
in SE$\frac{1}{4}$ <i>521102</i>
11/10/61 OR 1290/515</p> | <p>[10] D 4/19/73 (BAP) <i>521103a</i>
St. Petersburg, City of
E$\frac{1}{2}$ of Lot 6, PG, in SE$\frac{1}{4}$
5/1/73 OR 4024/167</p> <p>[11] D 12/27/61 (UL) <i>521104</i>
Wheeler, Lillian
Triangle in S$\frac{1}{2}$ of N$\frac{1}{2}$ of Lot 1.
PG, in SE$\frac{1}{4}$
1/5/62 OR 1332/391</p> <p>[12] D 4/28/61 (Ulmerton Substation)
Lightfoot, Melton E. <i>521105</i>
Lot 16, PG, in NW$\frac{1}{4}$
5/1/61 OR 1159/645</p> <p>[13] D 12/28/56 (Ulmerton Substation)
Phillips, Ola Lee <i>521106</i>
W 120' of W$\frac{1}{2}$ of Lot 1, PG, NW$\frac{1}{4}$
12/28/56 DB 1640/655</p> <p>[14] D 11/12/56 (Ulmerton Substation)
Renfro, Clyde E. <i>521107</i>
$\frac{1}{2}$ interest in Lot 2, PG, in NW$\frac{1}{4}$
12/20/56 DB 1632/369</p> <p>[15] D 11/14/56 (Ulmerton Substation)
Roe, Lee E. <i>521108</i>
same as Item 14
11/20/56 DB 1632/367</p> <p>[15] E 6/17/54 (HD) <i>521079</i>
St. Petersburg, City of
100' thru S$\frac{1}{2}$ of Lot 11, PG, in
SE$\frac{1}{4}$ & E$\frac{1}{2}$ of Lot 6, & port. Lot
1/11/55 DB 1514/19</p> <p>[16] E 11/2/61 (NC & HD) <i>521109</i>
125' thru E$\frac{1}{2}$ of Lot 6, PG, in
NW$\frac{1}{4}$ of SE$\frac{1}{4}$
12/14/61 OR 1313/518</p> <p>[17] E 5/27/54 (HD)
Holton, John B., Jr. <i>521110</i>
100' thru W$\frac{1}{2}$ of Lot 6 & Lot 7
PG in SE$\frac{1}{4}$
1/11/55 DB 1514/34</p> |
|--|--|

(cont'd)

EASEMENT LIST

SEC. 9 TWP. 30 S RGE. 16 E, Pinellas COUNTY

18 E 11/19/90 (HD 115KV Loop to Gateway Substation-N)
Alro Metals Service Center Corp.
N 37' of Lots 4 & 5 in SE $\frac{1}{4}$ 521111 e
12/10/90 OR 7445/285 1

20 E 10/3/90 (HD Loop) 521112
Gorby, Robert
N 10' of: Comm NW cor Lot 6, PINELLAS
GROVES, PB 1/55 in SE $\frac{1}{4}$
10/26/90 OR 7411/2380 1

21 E 7/11/90 (HD Loop) 521113
PYA/Monarch, Inc.
N 37' of N 300' of W 300' of Lot 5,
in SE $\frac{1}{4}$, PINELLAS GROVES,
7/31/90 OR 7340/2146 1

22 E 1/93 (taken back from sale) 521114
Pinellas County
(see doc for descriptions)
4/21/93 OR 8245/463
Filed: Disston Blvd Land Co.

23 SE 1/10/73 (BAP) 521752
Marriott, Ada C.
Beg pt E bndy Lot 3, PINELLAS
GROVES, PB 1/55 in NW $\frac{1}{4}$
1/18/73 OR 3977/492

24 E 7/28/54 (HD) 521755
Roe, Lee E.
Lot 2 in NE $\frac{1}{4}$ of NW $\frac{1}{4}$
1/11/55 DB 1514/31



Florida
Power & Light

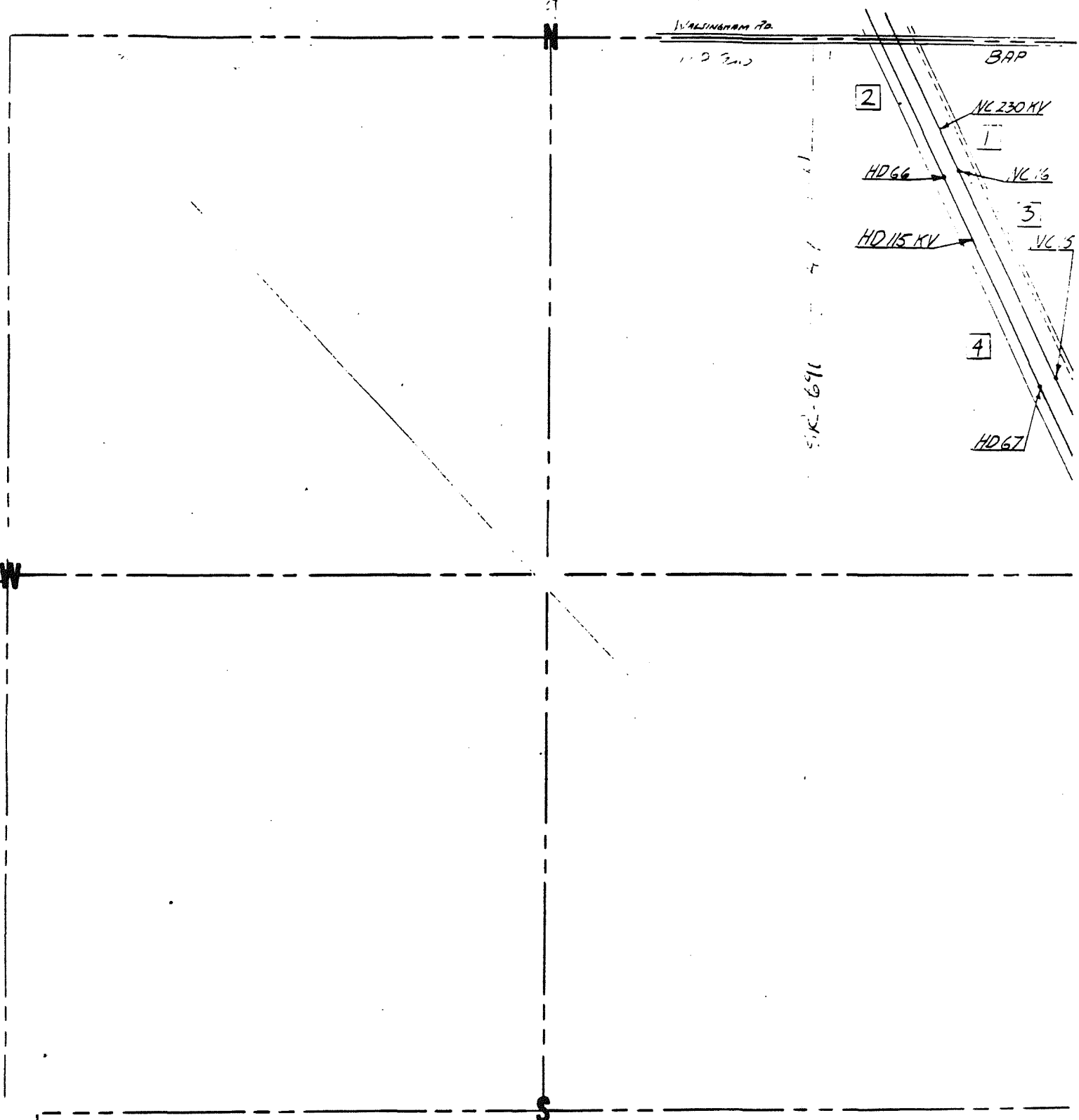
MAP 21

SECTION PL A

High-voltage line 115 KV (HD)
Northeast-Central 230 KV (NC)

LINE: Bartow-Anclote pipeline (BAP) SEC 16 TWP 30S RGE 16E Pinellas COUNTY

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



Rev. 5/77

SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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313 315
2/8/79
JCS

EASEMENT LIST

SEC. 16 TWP 30 South RGE. 16 East, Pinellas COUNTY

- [1] E 8/9/62 (NC) *521142*
Nelson, Lawrence, TR
125' thru E $\frac{1}{2}$ of NE $\frac{1}{4}$ (Farms 1,
16, 17 & 32, PINELLAS FARMS)
& W $\frac{1}{2}$ of NE $\frac{1}{4}$ of NE $\frac{1}{4}$
8/21/62 OR 1492/348
- [2] E 6/24/54 (HD) *521145*
Fisher, John T.
100' thru W $\frac{1}{2}$ of NE $\frac{1}{4}$ of NE $\frac{1}{4}$ &
SE $\frac{1}{4}$ of NE $\frac{1}{4}$ of NE $\frac{1}{4}$, PF
1/11/55 DB 1514/36
- [3] SE 2/14/73 (BAP)
Blawar Investments, Inc. *521147*
E 30' of 225' R/W thru
Farms 16, 17, 32 & port of
FL assoc Model Farm #3 in
SW $\frac{1}{4}$ of NE $\frac{1}{4}$ of NE $\frac{1}{4}$, PF
3/16/73 OR 4004/1642
- [4] SE 1/29/73 (BAP)
Commercial Development Corp.
125' thru E $\frac{1}{2}$ of NE $\frac{1}{4}$ (Farms 1, 16,
17 & 32), & W $\frac{1}{2}$ of NE $\frac{1}{4}$ of NE $\frac{1}{4}$, PF
2/20/73 OR 3995/913 *521151*



Florida
Power
and
Light
Company

MAP 22

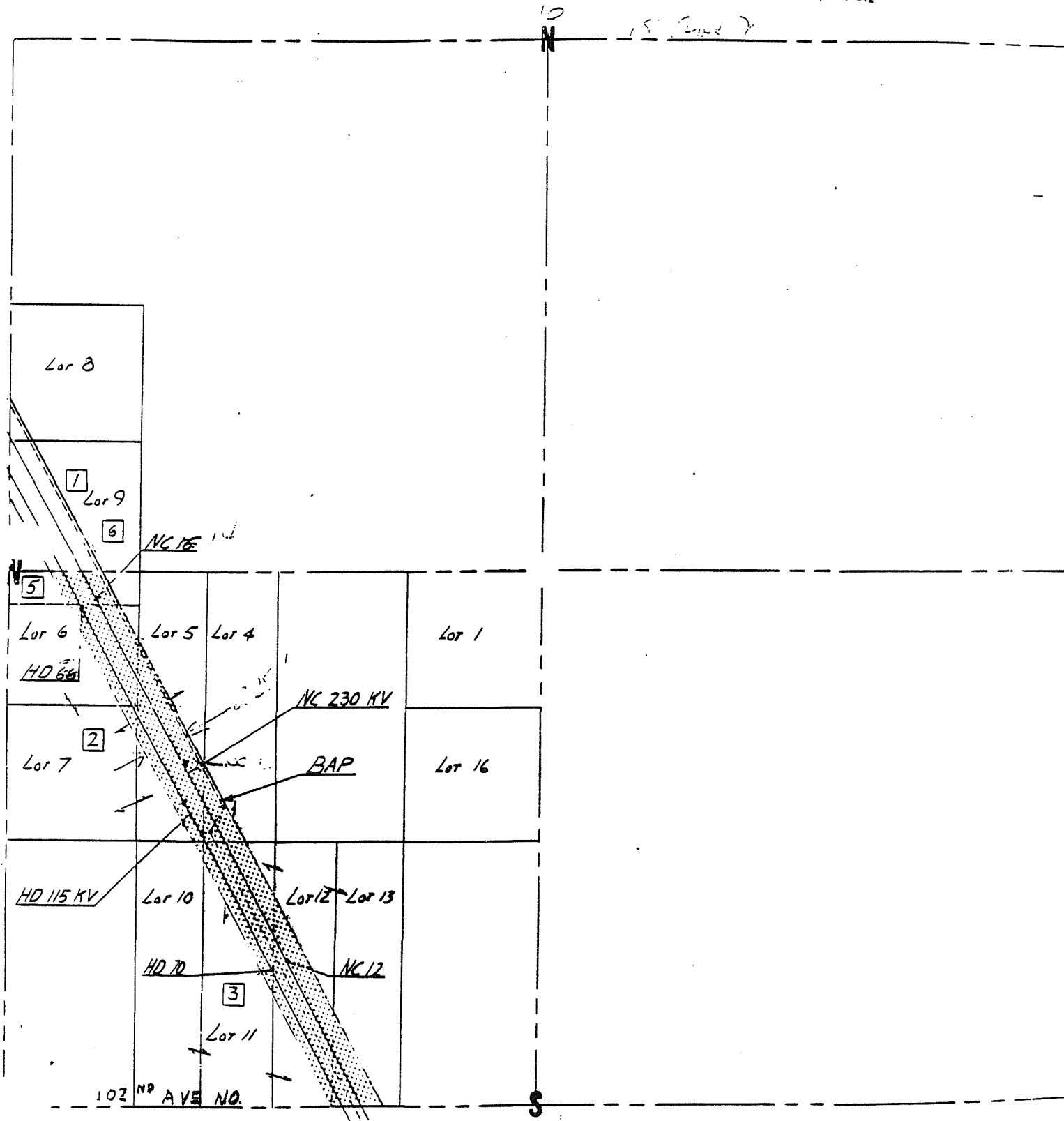
SECTION PL

Flintston-Largo 230 KV (UL)

Wiggins-Dixton 115 KV (HD)

Barlow-Anclote pipeline (BAP) SEC 15 TWP 30S RGE 16E Pinellas COUNTY

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch: { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



Rev. 5/77

SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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913 513
2/7/79
DCS

EASEMENT LIST

SEC. 15 TWP 30 S RGE. 16 E, Pinellas COUNTY

- [1] E 8/9/62 (NC)
Nelson, Lawrence, TR *521142*
125' thru Lots 8 & 9, PINELLAS
GROVES, PB 1/55, in SW $\frac{1}{4}$ of NW $\frac{1}{4}$
8/21/62 CR 1492 348
- [2] D 11/15/61 (NC)
Lohse, Henry, Co., Inc. *521143*
225' thru Lots 5-7, PG, in SW $\frac{1}{4}$
11/22/61 CR 1297, 713
- [3] D 12/8/61 (NC)
Lohse, William *521144*
225' thru Lots 4, 10-13, PG, in SW $\frac{1}{4}$
12/18/61 CR 1313, 480
- [4] E 6/24/54 (hD)
Fisher, John T. *521145*
100' thru Lots 8 & 9, PG,
in SW $\frac{1}{4}$ of NW $\frac{1}{4}$
1/11/55 DB 1514/36
- [5] D 5/27/63 (NC)
Brann, Edward R. *521146*
225' thru N $\frac{1}{4}$ of Lot 6, PG,
in NW $\frac{1}{4}$ of SW $\frac{1}{4}$
6/7/63 CR 1694, 646
- [6] SE 2/14/73 (BAP) *521147*
Blawar Investments, Inc.
E 30' of 225' R/W thru
Lots 8 & 9, PG, in
SW $\frac{1}{4}$ of NW $\frac{1}{4}$
3/16/73 CR 4004/1642
- 7 E 9/6/90 (HD 115KV Loop to Gateway Sub-S)
Barger Interests *521148*
S 10' of Lots 1, 2, Tr B & portion Lot 3
& 10' x 70' guy strip, ST. MARY BUSINESS
PARK, PB 102/24
9/26/90 CR 7387/812
- 8 GE 8/6/90 (HD Loop) *521149*
US Home of FL, Inc.
E 20' of N 60' of Lot 1, THE LAKES,
UNIT 2, PHASE 4, Sec. 1, PB 85/47
in SW $\frac{1}{4}$
8/24/90 CR 7361, 1836
- 9 .GE 9/11/90 (HD Loop) *521150*
Waste Management of Pinellas County
W 20' of S 60' of S 330' of Farm 12,
PINELLAS GROVES, PB 1/55 less S 15' etc
9/26/90 CR 7387/1002



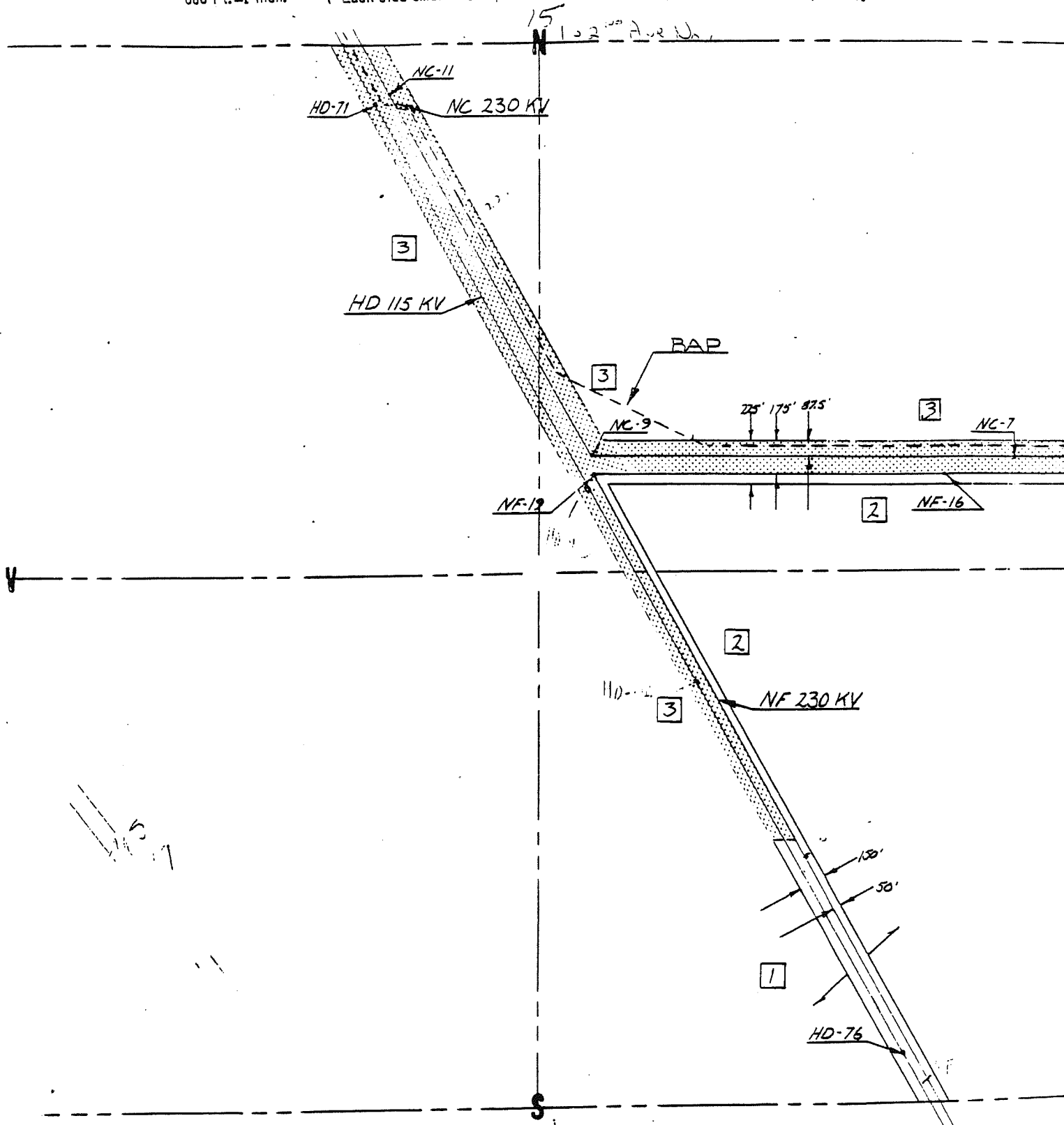
Northeast-Panther 230 KV (NF)

Northeast-Dunbar 230 KV (NC)

Virginia-Disson 115 KV (HD) SEC 22 TWP 3CS RGE 16E Pinellas COUNTY

MAP 23

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



Rev. 3/77

SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft.= 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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313 3.5
DC 5
8/31/73

EASEMENT LIST

SEC. 22 TWP 30 S RGE. 16 E, Pinellas COUNTY

- 1 E 9/15/69 (NF) *5200*
Miele, Anthony P.
150' thru S $\frac{1}{2}$ of SE $\frac{1}{4}$
10/20/69 OR 3186/680
- 2 E 5/20/69 (NF) *5200 71a*
Behring West, Inc.
50' in N $\frac{1}{2}$ of SE $\frac{1}{4}$ & S $\frac{1}{2}$ of NE $\frac{1}{4}$
7/21/69 OR 3124 706
- 3 D 3/20/62 (NF) *5200 71a*
Wright, Ed C.
varied widths thru N $\frac{1}{2}$ of SE $\frac{1}{4}$
& SE $\frac{1}{4}$ of NE $\frac{1}{4}$ & W $\frac{1}{2}$ of NE $\frac{1}{4}$ &
E $\frac{1}{2}$ of NW $\frac{1}{4}$
3/22/62 OR 1388/48
- 4 E 1/30/73 (BAP)
Leadership Homes of W FL, Inc.
Comm NE cor, etc. *5200 71a*
2/20/73 OR 3995/916

FLORIDA POWER CORPORATION
NORTHEAST PINELLAS COUNTY RESOURCE RECOVERY (NR) 230 KV
NORTHEAST-DISSTON UG - NDUG

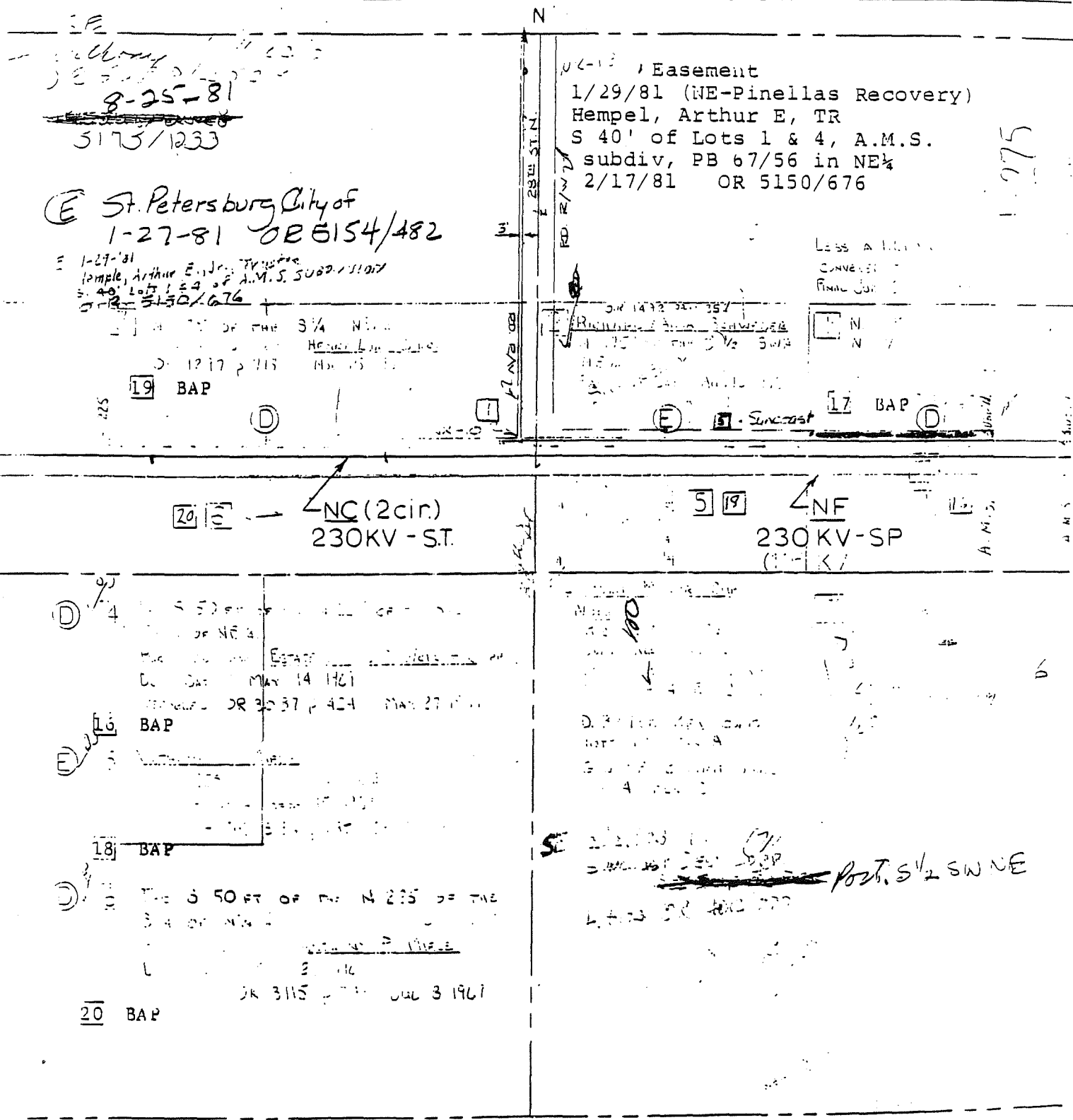
BARTOW-ANCLOTE PIPELINE (BAP)
NORTHEAST-FORTIETH STREET 230 KV LINE

PINELLAS COUNTY COMPLEX - NORTHEAST-ULMERTON 2 CIRCUITS, ST, 230 KV

MAP 24

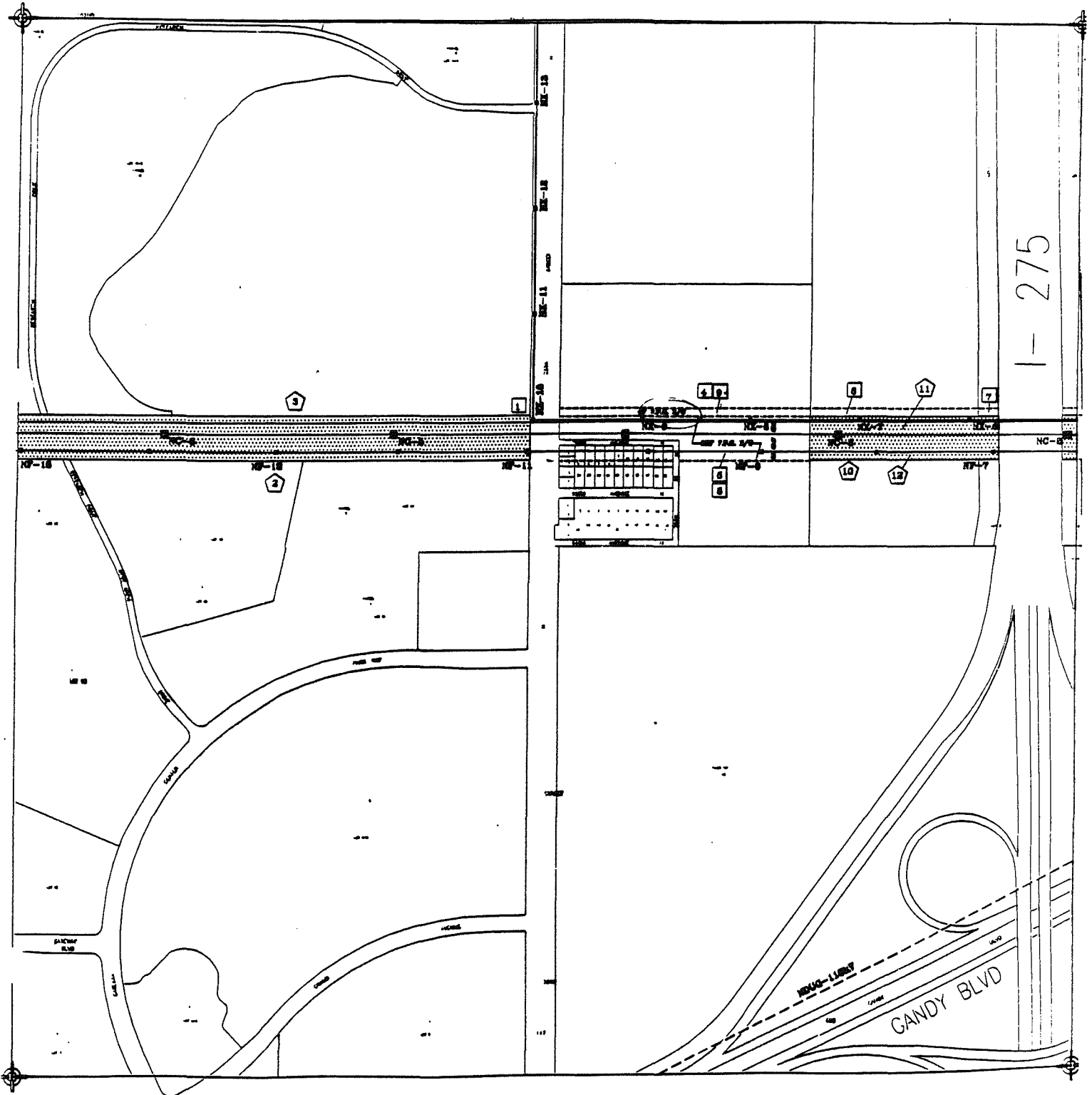
Page

SECTION 23 TOWNSHIP 30 S RANGE 16 E COUNTY PINELLAS



SCALE FOR SECTION, Each side large blue squares = 20 chains, 80 rods, 1320 feet; area of square 40 acres.
360 Ft = 1 inch. Each side small red squares = 5 chains, 20 rods, 330 feet; area of square 2 1/2 acres.

23 / 305 / 16 E

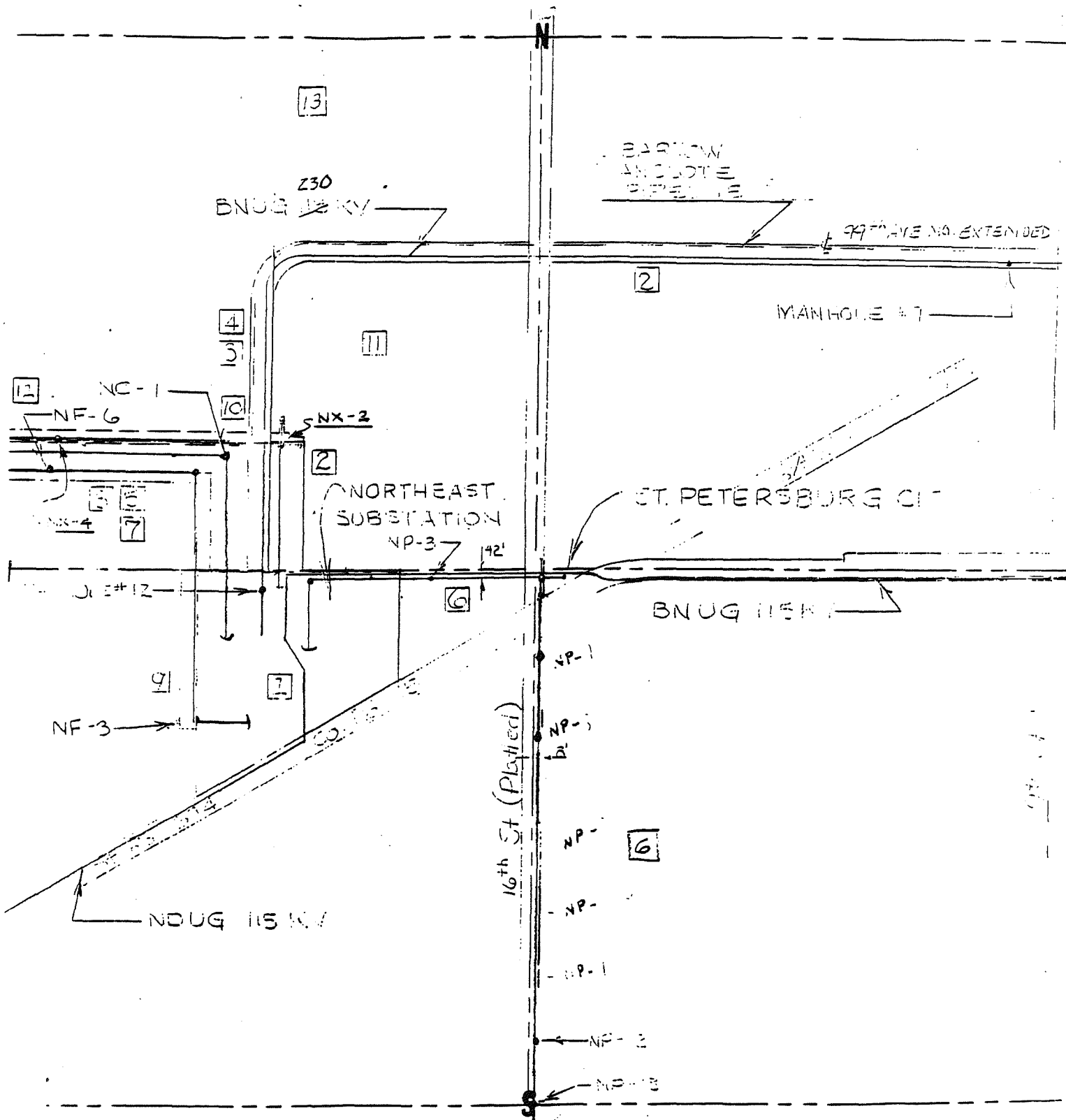


EASEMENT LIST

SEC. 23 TWP 30 S RGE. 16 E, Pinellas COUNTY

- | | |
|---|---|
| <p>[1] GE 7/7/81 (NX)
Miele, Anthony P. <i>521173</i>
10 x 100' strip in SE$\frac{1}{4}$ of NW$\frac{1}{4}$
8/3/81 OR 5228/2023</p> <p>[2] D 5/26/69 (NF)
Miele, Anthony P., TR
50' thru S$\frac{1}{4}$ of NW$\frac{1}{4}$
7/8/69 OR 3115/748</p> <p>[3] D 11/15/61
Lohse, Henry Co., Inc.
N 175' of S$\frac{1}{4}$ of NW$\frac{1}{4}$
11/22/61 OR 1297/718</p> <p>[4] E 3/16/81 (NX)
Miele, Anthony P. <i>521175</i>
40' wide strip thru SW$\frac{1}{4}$ of NE$\frac{1}{4}$
4/14/81 OR 5175/1233</p> <p>[5] E 9/15/69 (NF)
Miele, Anthony P., TR <i>521170</i>
N 225' of S$\frac{1}{4}$ of SW$\frac{1}{4}$ of NE$\frac{1}{4}$
10/20/69 OR 3186/680</p> <p>[6] E 1/27/81 (NX) <i>521176</i>
St. Petersburg, City of
S 40' of E$\frac{1}{4}$ of NE$\frac{1}{4}$ less
N 225' of S$\frac{1}{4}$ of SE$\frac{1}{4}$ of NE$\frac{1}{4}$
2/26/81 OR 5154/482</p> <p>[7] E 1/29/81 (NX) <i>521180</i>
Hempel, Arthur E., TR
S 40' of Lots 1 & 4, A.M.S.
SUBDIV., PB 67/56, in NE$\frac{1}{4}$
2/17/81 OR 5150/676</p> <p>[8] E 8/10/62
Schweder, Richard <i>Superseded by [5] (Miele)</i>
N 175' of S$\frac{1}{4}$ of SW$\frac{1}{4}$ of NE$\frac{1}{4}$ <i>521177</i>
8/21/62 OR 1492/352</p> <p>[9] SE 2/21/73
Suncoast Development Corp.
Port. S$\frac{1}{4}$ of SW$\frac{1}{4}$ of NE$\frac{1}{4}$ <i>521178</i>
4/4/73 OR 4012/777</p> <p>[10] QCD 3/6/62
Grove Land & Timber Co. <i>521179</i>
Filed: Wright, Ed C OR 1338/54
release of mineral rights
3/20/62 OR 1385/647</p> | <p>[11] D 3/20/62
Wright, Ed C. <i>521172a</i>
N 175' of S$\frac{1}{4}$ of SE$\frac{1}{4}$ of NE$\frac{1}{4}$
3/26/62 OR 1333/43</p> <p>[12] D 5/14/69
Kirby, Ruth B. <i>521181</i>
S 50' of N 225' of S$\frac{1}{4}$ of
SE$\frac{1}{4}$ of NE$\frac{1}{4}$
5/27/69 OR 3087/424</p> |
|---|---|

100 1st 133 115 115
 Bartow - Northeast 115 KV (Double Circuit, Underground) MAY 25 - E.C. J.N. LA
 Northeast - Pillsbury 115KV (NP) Northeast-Pinellas Resource Recovery Facilities (NK 130 KV)
 Northeast-Curlew 230 KV (NC)
 Northeast-Fortieth 230KV (NF) SEC 24 TWP 30S RGE 16E Pinellas COUNTY
 SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
 660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2 1/2 acres.



Rev. 5/77

SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
 330 Ft. = 1 Inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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EASEMENT LIST

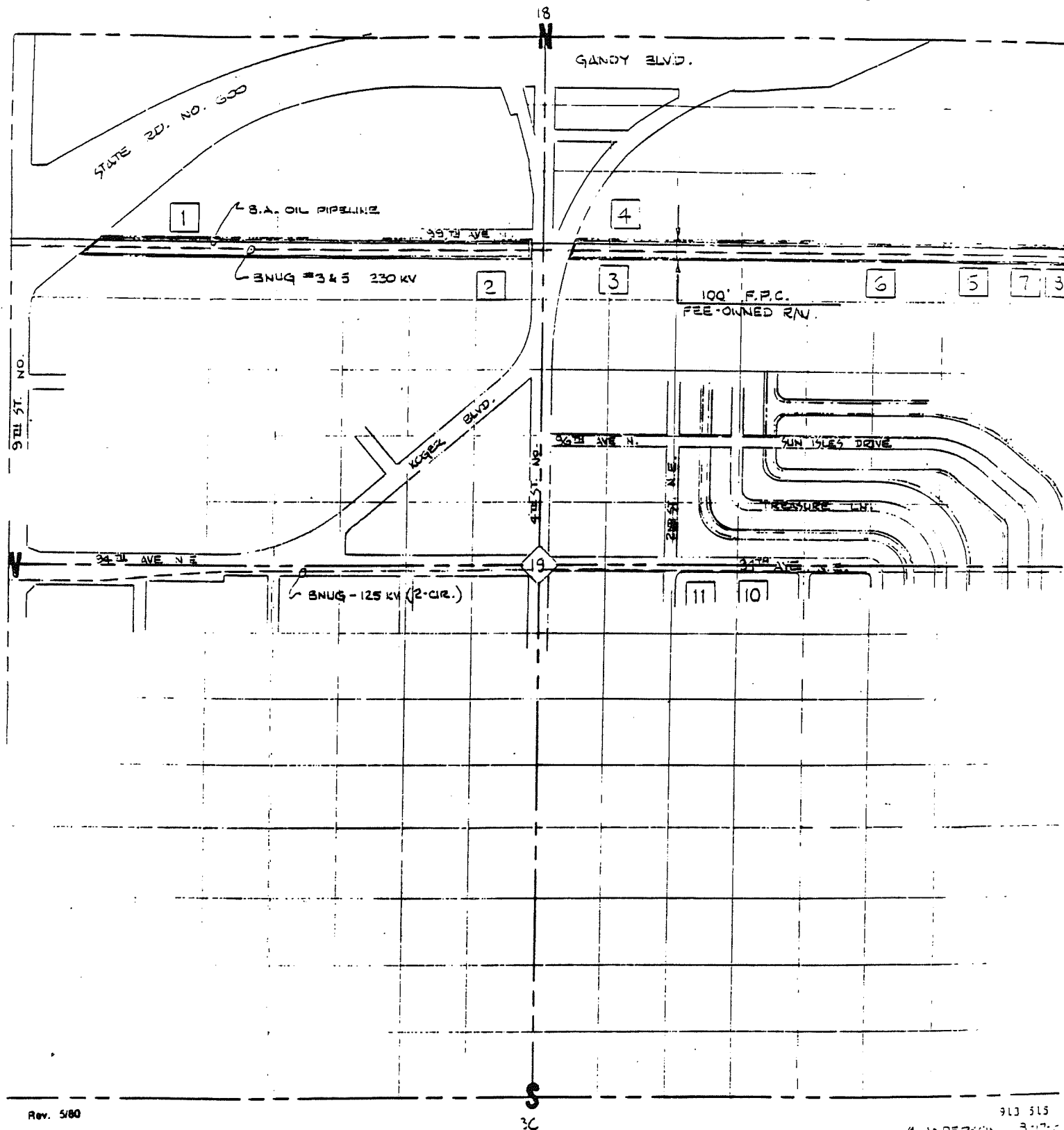
SEC. 24 TWP 30 S RGE. 16 E, Pinellas COUNTY

- | | |
|---|---|
| <p>[1] D 9/8/58 (Northeast Substation)
Loughridge, J. H., Realty Corp.
Part of W$\frac{1}{2}$ of NE$\frac{1}{4}$ of SW$\frac{1}{4}$; part
of E$\frac{1}{2}$ of E$\frac{1}{2}$ of NW$\frac{1}{4}$ of SW$\frac{1}{4}$ N of SR
9/9/58 OR 412/408 <i>5211720</i></p> <p>[2] D 8/21/61 (BNUG)
Aguila, Rafael <i>521183</i>
175' thru S$\frac{1}{2}$ of E-3/4 of N$\frac{1}{2}$ of N$\frac{1}{2}$
less E 100'; W 175' of SW$\frac{1}{4}$ of
SE$\frac{1}{4}$ of NW$\frac{1}{4}$
8/30/61 OR 1240/731</p> <p>[3] E 8/25/81 (NX) <i>521184</i>
Hennessy, L. J., TR
S 40' of W$\frac{1}{2}$ of NW$\frac{1}{4}$
9/11/81 OR 5246/26</p> <p>[4] D 3/20/62 (BNUG) <i>521185</i>
Wright, Ed C.
100' thru S-5/8 of E$\frac{1}{2}$ of W$\frac{1}{2}$ of NW$\frac{1}{4}$
3/22/62 OR 1338/54</p> <p>QCD 3/6/62 (BNUG) <i>521179</i>
Grove Land & Timber Co.
release of mineral rights
Filed: Wright, Ed C. above
3/20/62 OR 1385/647</p> <p>[5] D 3/20/62 (NU) <i>5211720</i>
175' thru SW$\frac{1}{4}$ of NW$\frac{1}{4}$
3/22/62 OR 1388/48</p> <p>[6] E 8/14/63 (NP) <i>5211860</i>
Weaver, A. V.
61' thru N$\frac{1}{2}$ of N$\frac{1}{2}$ of E$\frac{1}{2}$ of NE$\frac{1}{4}$
of SW$\frac{1}{4}$; 66' thru W$\frac{1}{2}$ of W$\frac{1}{2}$ of SE$\frac{1}{4}$
8/23/63 OR 1749/57</p> <p>[7] D 5/14/69 (NF) <i>521181</i>
Kirby, Ruth B., Exec
strip 128.62' N & S thru port. S$\frac{1}{2}$
of SW$\frac{1}{4}$ of NW$\frac{1}{4}$; 50' wide E & W thru
port. S$\frac{1}{2}$ of SW$\frac{1}{4}$ of NW$\frac{1}{4}$
5/27/69 OR 3037/424</p> <p>[8] TP 6/23/70 (NF) <i>521187</i>
Weaver, Otto L.
N 790.2' of W$\frac{1}{2}$ of E$\frac{1}{2}$ of NW$\frac{1}{4}$ of SW$\frac{1}{4}$
(less E 75')
11/3/70 OR 3423/258
Filed: Weaver, Otto L. 3381/921</p> | <p>[9] D 6/23/70 (NF) <i>521188</i>
Weaver, Otto L.
75' W of N 790.20' of W bou
dary of NE Substa in N$\frac{1}{2}$ of
8/24/70 OR 3381/921</p> <p>[10] E 10/15/74 (BAP) <i>521187a</i>
Hennessy, L. J., TR
triangle in SE cor. N$\frac{1}{2}$ of
SW$\frac{1}{4}$ of NW$\frac{1}{4}$
11/6/74 OR 4231/522
Pipeline is not located in
this easement area.</p> <p>[11] GE 9/17/81 (NX) <i>521190</i>
North Gate Limited
N$\frac{1}{2}$ of SE$\frac{1}{4}$ of NW$\frac{1}{4}$
11/2/81 OR 5267/136</p> <p>[12] E 9/2/82 (NX)
Hennessy, L. J., TR
S 40' of W$\frac{1}{2}$ of NW$\frac{1}{4}$ N of
225' R/W <i>521191</i>
9/17/82 OR 5402/1137
supersedes OR 5246/26</p> <p>[13] E 1/29/81 (NX) <i>521189</i>
Kempel, Arthur E., TR
NW$\frac{1}{4}$
2/17/81 OR 5150/676</p> |
|---|---|



LINE: Bartow Anclote Pipeline (BAP) SEC 19 TWP 30 S RGE 17 E , Pinellas COUN

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch: { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



Rev. 5/80

SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft. = 1 inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

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913 515

A. ANDERSON 3-17-6

EASEMENT LIST

SEC. 19 TWP 30 South RGE. 17 East, Pinellas COUNTY

- | | |
|---|---|
| <p>[1] D 3/8/62 (BNUG) <i>521561a</i>
Wright, Ed C.
N 100' of S$\frac{1}{2}$ of N$\frac{1}{2}$ of
NW$\frac{1}{4}$ less E 200'
3/15/62 OR 1382/108</p> <p>[2] D 7/11/61 (BNUG) <i>521562</i>
Dibbell, Charles B.
E 200' of N 125' of S$\frac{1}{2}$ of
SE$\frac{1}{4}$ of NE$\frac{1}{4}$ of NW$\frac{1}{4}$
7/12/61 OR 1208/739</p> <p>[3] D 8/8/61 (BNUG) <i>521563</i>
Weintraub, Max W.
S 70' of N 100' of S$\frac{1}{2}$
of SW$\frac{1}{4}$ of NW$\frac{1}{4}$ of NE$\frac{1}{4}$
E of Gandy Blvd less
E 155'
8/10/61 OR 1228/226</p> <p>[4] D 7/25/61 (BNUG) <i>521564</i>
Rickton, Inc.
N 30' of S$\frac{1}{2}$ of SW$\frac{1}{4}$ of
NW$\frac{1}{4}$ of NE$\frac{1}{4}$ & E 30' of
N 100' of S$\frac{1}{2}$ of SW$\frac{1}{4}$
of NW$\frac{1}{4}$ of NE$\frac{1}{4}$ less N 30'
7/27/61 OR 1218/739</p> <p>[5] D 7/27/61 (BNUG) <i>521565</i>
Lawrence-Wilson, Inc.
N 100' of SW$\frac{1}{4}$ of SE$\frac{1}{4}$
of NE$\frac{1}{4}$ of NE$\frac{1}{4}$
8/3/61 OR 1223/622</p> <p>D 7/27/61 (BNUG)
S 70' of N 100' of W 125'
of E 155' of S$\frac{1}{2}$ of SW$\frac{1}{4}$ <i>521566</i>
of NW$\frac{1}{4}$ of NE$\frac{1}{4}$ & N 100'
of S$\frac{1}{2}$ of SE$\frac{1}{4}$ of NW$\frac{1}{4}$ of NE$\frac{1}{4}$
7/31/61 OR 1220/727</p> <p>[6] D 8/25/61 (BNUG) <i>521567</i>
Bermuda Realty, Inc.
N 100' of S$\frac{1}{2}$ of SW$\frac{1}{4}$
of NE$\frac{1}{4}$ of NE$\frac{1}{4}$
8/30/61 OR 1240/728</p> <p>[7] D 7/27/61 (BNUG) <i>521568</i>
Wilson, J. J., and Associates, Inc.
N 100' of SE$\frac{1}{4}$ of SE$\frac{1}{4}$ of
NE$\frac{1}{4}$ of NE$\frac{1}{4}$ less E 111'
8/3/61 OR 1223/626</p> | <p>[8] D 8/15/61 (BNUG) <i>521569</i>
Alderson, Hiley E. J.
E 111' of N 100' of SE$\frac{1}{4}$ of SE$\frac{1}{4}$
of NE$\frac{1}{4}$ of NE$\frac{1}{4}$
8/21/61 OR 1235/142</p> <p>[9] D 9/26/61 <i>51B</i>
Greene, Elizabeth G.
Lot 4 less S 10' & S 10' of Lot 1
Blk "E", TROPIC HILLS UNIT 1,
PB 57/41
9/28/61 OR 1260/181</p> <p>[10] E 6/15/59 (BNUG) <i>521570</i>
Baynard, Bessie Lee
N 30' of W$\frac{1}{2}$ of NW$\frac{1}{4}$ of NE$\frac{1}{4}$ of SE$\frac{1}{4}$
& N 30' of NE$\frac{1}{4}$ of NW$\frac{1}{4}$ of SE$\frac{1}{4}$
6/16/59 OR 626/671</p> <p>[11] E 7/3/59 (BNUG)
Wadsworth Homes Co. <i>521570</i>
N 20' of Lot 21, Blk A; Lot 20,
Blk B; Lot 19, Blk C; Lot 21,
Blk D & Lot 25, Blk E, SUN
PLAZA ISLES SECTION 3, PB 41/5
7/6/59 OR 641/440</p> <p>12 Agreement 6/11/59 <i>521571</i>
same
OR 626/665</p> |
|---|---|

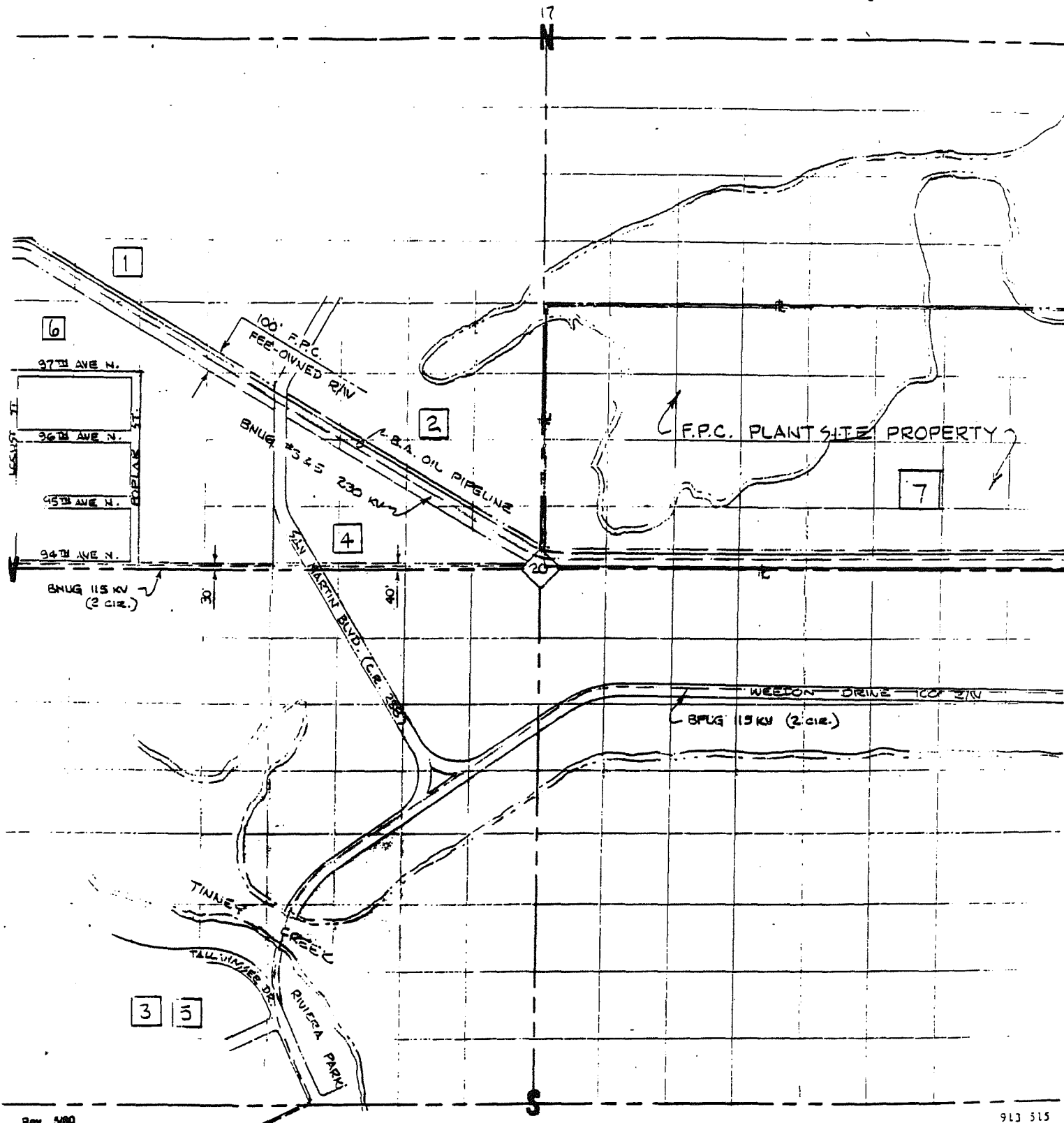


Bartow_Northeast UG (BNUG)

LINE: Bartow Anclote Pipeline (BAP) SEC 20 TWP 30 S RGE 17 E, Pinellas COUNTY

MAP 27

SCALE FOR SECTION, { Each side large blue squares=20 chains, 80 rods, 1320 feet; area of square 40 acres.
660 Ft.=1 Inch. { Each side small red squares= 5 chains, 20 rods, 330 feet; area of square 2½ acres.



Rev. 5/60

SCALE FOR QUARTER SECTION, { Each side large blue squares= 10 chains, 40 rods, 660 feet; area of square 10 acres.
330 Ft. = 1 inch { Each side small red squares=2.5 chains, 10 rods, 165 feet; area of square .625 of 1 acre.

PRONTO LAND MEASURE 660-330 MAP SHEET

PRONTO LAND MEASURE

Copyright, 1987, James Hamilton Adair, Flint, Michigan

913 515

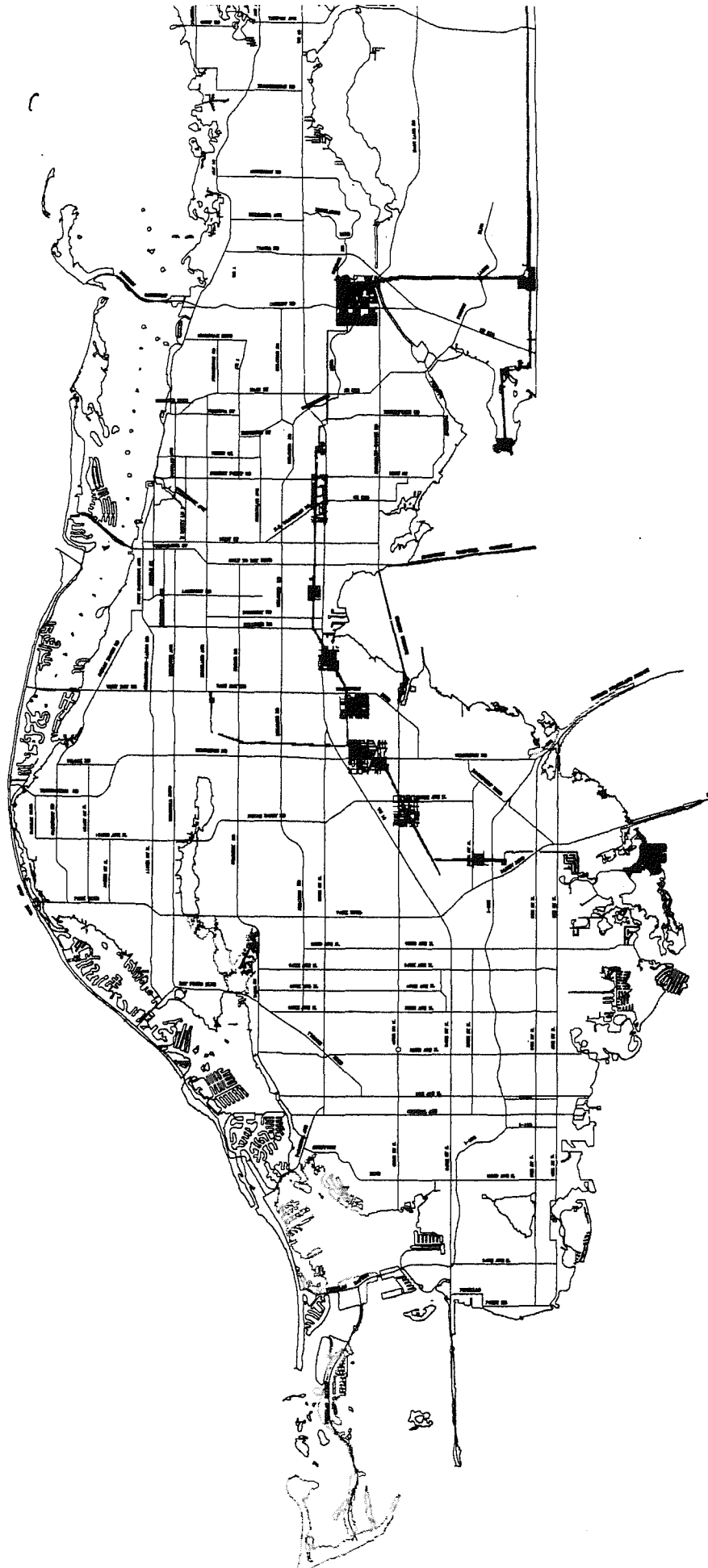
A. ANDERSON 3/25/

EASEMENT LIST

SEC. 20 TWP 30 South RGE. 17 East, Pinellas COUNTY

- [1] D 8/21/61 (BNUG) *520572*
St. Petersburg Kennel Club
100' thru S $\frac{1}{2}$ of NW $\frac{1}{4}$ of NW $\frac{1}{4}$;
N $\frac{1}{2}$ of W $\frac{1}{2}$ of SW $\frac{1}{4}$ of NW $\frac{1}{4}$;
N $\frac{1}{2}$ of E $\frac{1}{2}$ of SW $\frac{1}{4}$ of NW $\frac{1}{4}$;
& NW $\frac{1}{4}$ of NW $\frac{1}{4}$ of SE $\frac{1}{4}$ of NW $\frac{1}{4}$
W of CR-258
8/21/61 OR 1237/465
- [2] D 9/15/61 (BNUG)
Holton, Jack J. *520573*
100' thru SE $\frac{1}{4}$ of SW $\frac{1}{4}$ S &
E of CR-258
9/15/61 OR 1254/459
- [3] D 2/25/59 (BFUG)
Carey, C. I. *520574*
SW $\frac{1}{4}$ of SW $\frac{1}{4}$
5/8/59 OR 598/326
- [4] E 8/11/59 (BNUG) *11524*
Moorefield, J. L. *520575*
S 30' of E $\frac{1}{2}$ of SW $\frac{1}{4}$
of NW $\frac{1}{4}$
8/12/59 OR 671/210
- [5] E 1/31/58 (BFUG)
Manooch, W. H. *520576*
SW $\frac{1}{4}$ of SW $\frac{1}{4}$
1/31/58 OR 269/80
- [6] E 4/18/88 (BNUG) *520577*
St. Petersburg Kennel Club, Inc.
R/w for 94th Ave. N (a 30' R/W)
together w/ S 30' of Locust Street
(a 50' R/W)
9/26/89 OR 7094/2005
- 7 D 6/25/56 (Bartow, Paul L., Power Plant)
Baynard's Inc. *520558*
S-1/2 of NE-1/4
6/25/56 DB 1604/230

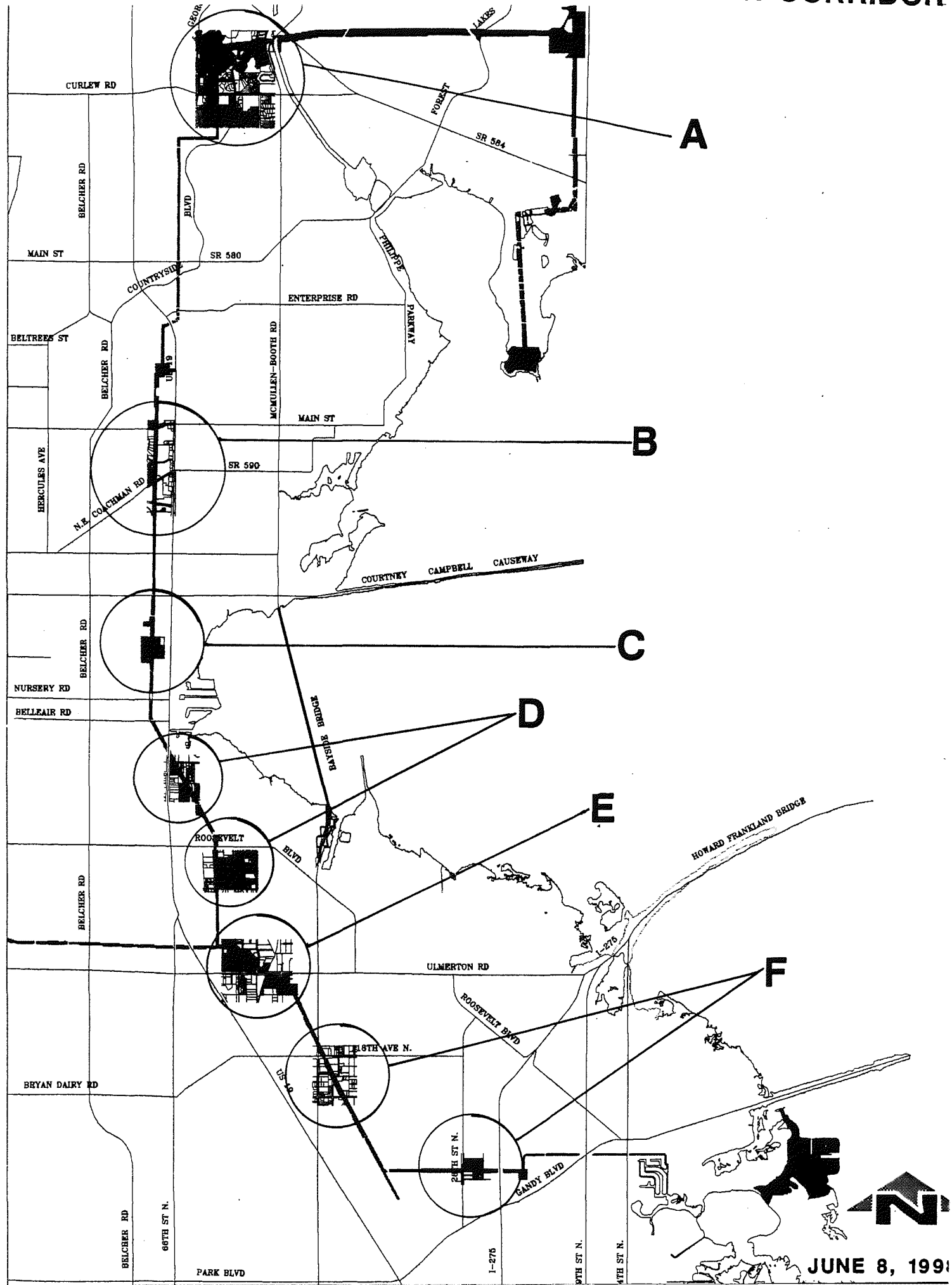
PRIVATE PARCELS WITHIN THE FLORIDA POWER CORRIDOR



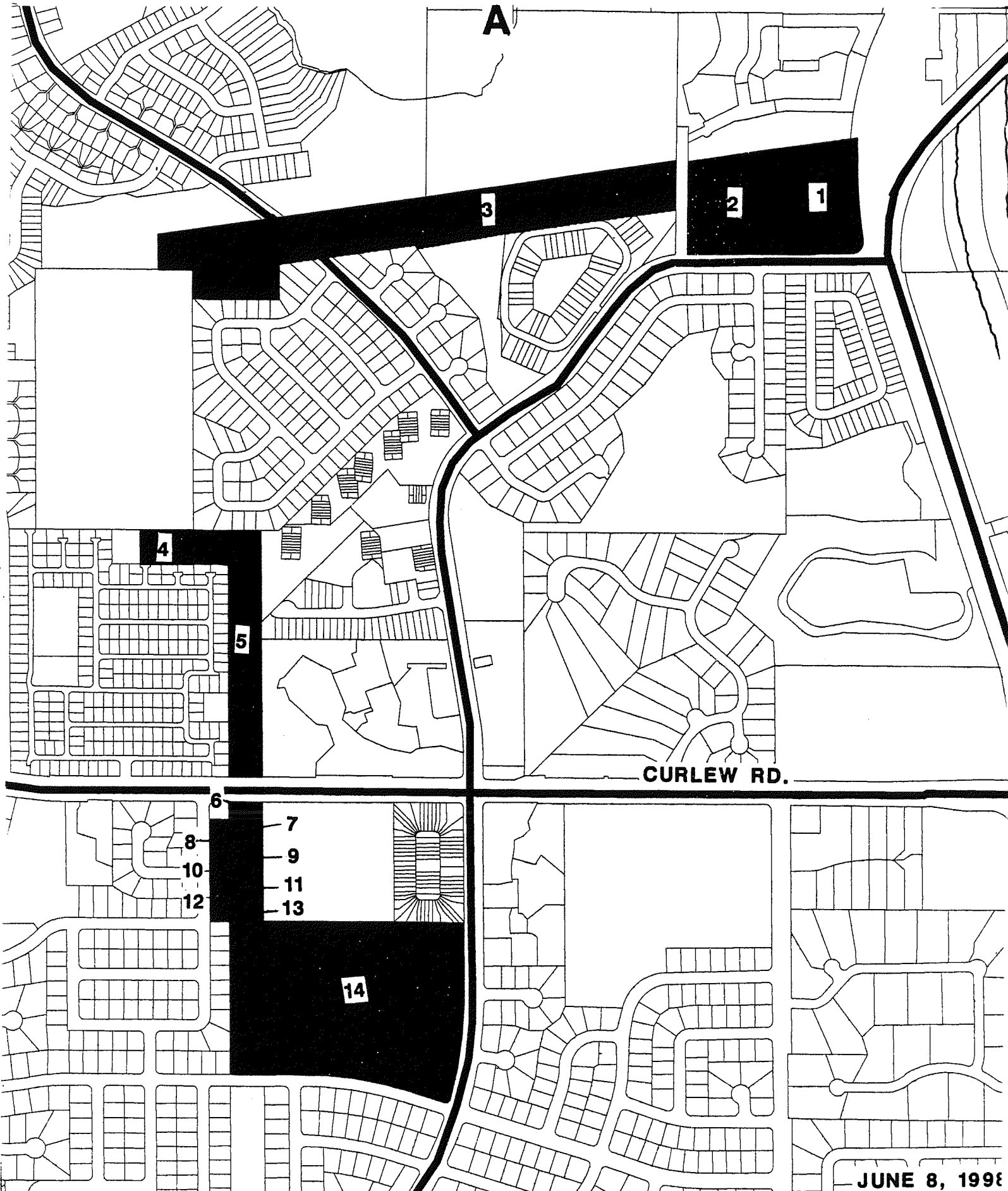
JUNE 8, 1998



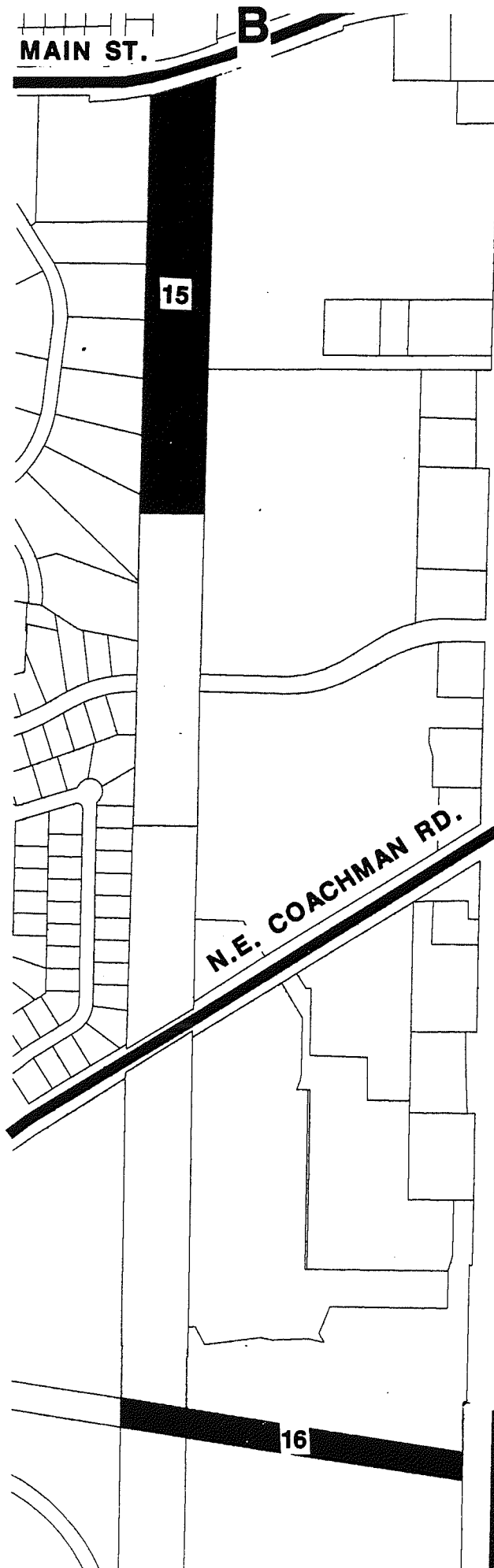
PRIVATE PARCELS WITHIN THE FLORIDA POWER CORRIDOR



PRIVATE PARCELS WITHIN THE FLORIDA POWER CORRIDOR



PRIVATE PARCELS WITHIN THE FLORIDA POWER CORRIDOR

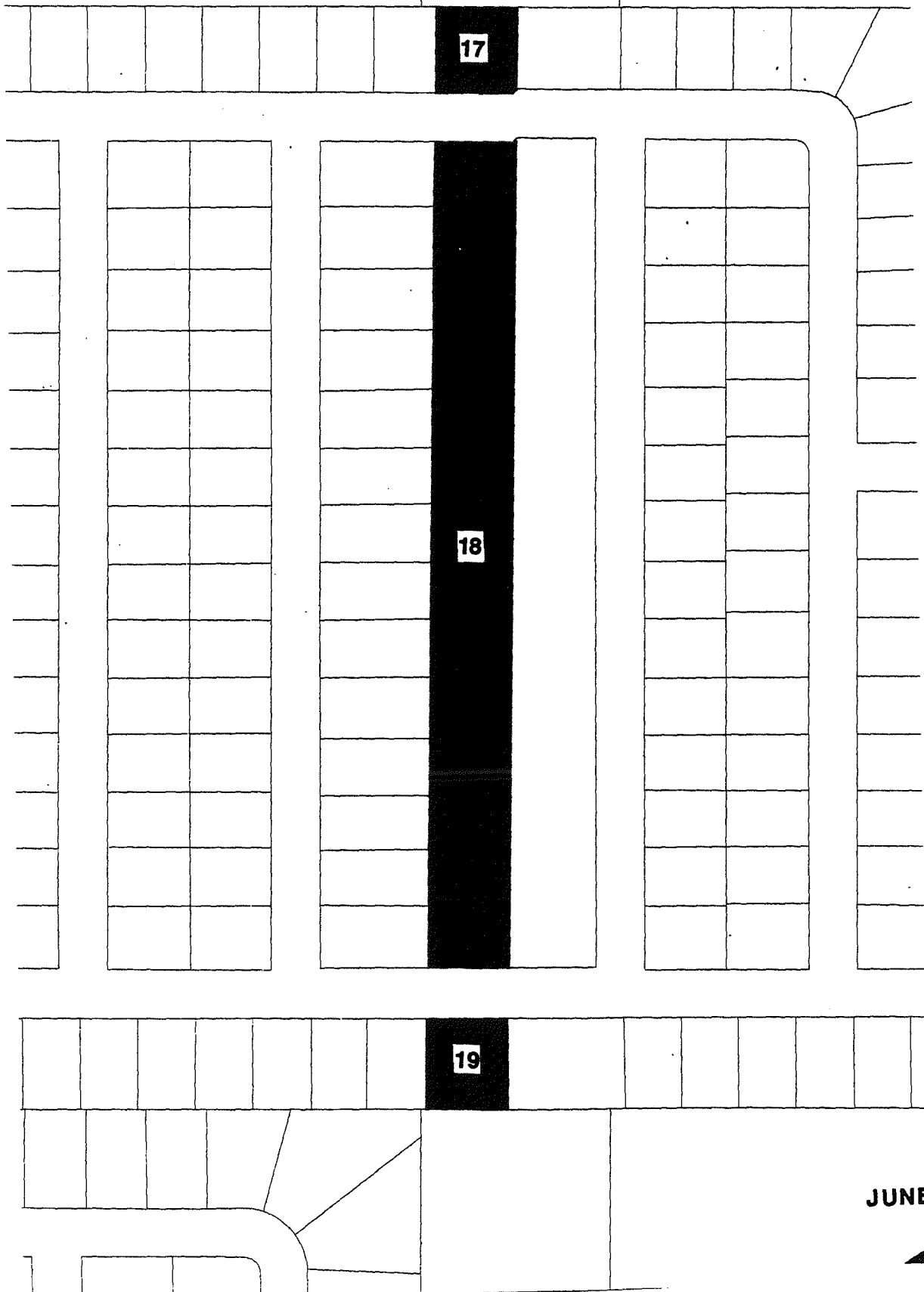


JUNE 8, 1998



PRIVATE PARCELS WITHIN THE FLORIDA POWER CORRIDOR

C



JUNE 8, 1998



20

PRIVATE PARCELS WITHIN THE FLORIDA POWER CORRIDOR

D

21

22

23

24

25

26

U.S. 19

ROOSEVELT BLVD.

JUNE 8, 1998

27



PRIVATE PARCELS WITHIN THE FLORIDA POWER CORRIDOR

E

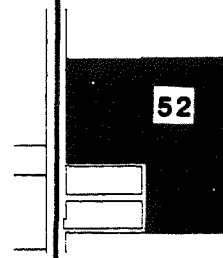
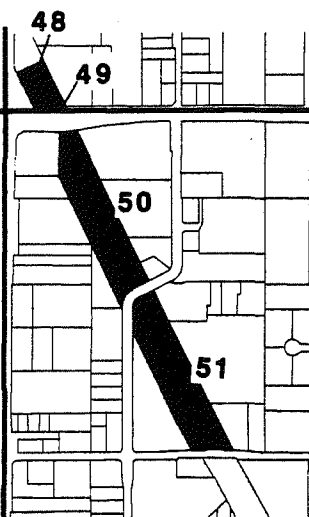


JUNE 8, 1998



PRIVATE PARCELS WITHIN THE FLORIDA POWER CORRIDOR

F



JUNE 8, 1998



	PARCEL NO.	LAND USE CODE	PROPERTY OWNER & ADDRESS	PROPERTY ADDRESS
1	092816000003300300	334 78	BEVERLY HEALTH & REHABILITATION SERV INC. 4675 MAC ARTHUR CT STE 900 NEWPORT BEACH 92660	3825 COUNTRYSIDE BLVD
2	082816000004400300	0 0	BEVERLY HEALTH & REHABILITATION SERV INC 4675 MAC ARTHUR CT STE 900 NEWPORT BEACH 92660	
3	082816000004400200	360 91	COUNTRYSIDE NORTH COMMUNITY ASSOCIATION INC C/O US HOME CORP 26133 US HIGHWAY 19 N CLEARWATER 33763 2019	-
4	172816000002400100	360 91	CURLEW M/H EST ASSN INC 2755 CURLEW RD PALM HARBOR 34684	
5	172816094190002390	360 91	CURLEW MOBILE HOME ESTATES ASSOCIATION INC 2755 CURLEW RD PALM HARBOR 34684	
6	172816186830000840	360 91	PINELLAS COUNTY ATTN: WATER SYSTEM 315 COURT ST CLEARWATER 33756 5165	
7	172816186830000210	210 1	PAVLICK, WILLIAM F PAVLICK, DELPHIA C 3487 MERLIN DR CLEARWATER 33761 1212	3487 MERLIN DR
8	172816186830000220	210 1	FARRELL, TERRANCE D FARRELL, ARLENE S 3481 MERLIN DR CLEARWATER 33761 1212	3481 MERLIN DR
9	172816186830000230	210 1	FLORY, LAWRENCE P II HOPKINS, JENNIFER P 3475 MERLIN DR CLEARWATER 33761 1212	3475 MERLIN DR
10	172816186830000240	210 1	PLANINICH, MICHAEL G 6401 BARTON PINES RD RALEIGH 27614	3469 MERLIN DR
11	172816186830000250	210 1	KEDAN, ELLA 2354 HADDON HALL PL CLEARWATER 33764 7510	3463 MERLIN DR
12	172816186830000260	210 1	LEWIS, JOHN A 3457 MERLIN DR CLEARWATER 33761 1212	3457 MERLIN DR

PARCEL NO.	LAND USE CODE	PROPERTY OWNER & ADDRESS	PROPERTY ADDRESS
13 172816186830000270	210 1	DANA, MAGDELENE L TRE 89 OXFORD PL, #2F STATEN ISLAND 10301 3007	3451 MERLIN DR
14 172816149050000010	230 4	SCHOENECK, F JOHN JR SHOENECK, CAROL M 3437 ROCHELLE CT # 1 CLEARWATER 33761 1330	3437 ROCHELLE CT
15 062916000004100300	360 91	PINELLAS COUNTY ATTN: ENGINEERING R/W DIV 315 COURT ST CLEARWATER 33756 5165	
16 072916000002200300	363 91	CSX TRANSPORTATION INC ATTN: TAX DEPT J-910 500 WATER ST JACKSONVILLE 32202 4422	590 NE COACHMAN RD
17 192916000001100200	360 91	CLEARWATER, CITY OF PO BOX 4748 CLEARWATER 33758 4748	
18 192916000001100300	0 89	CLEARWATER, CITY OF PO BOX 4748 CLEARWATER 33758 4748	
19 192916000001100400	360 91	CLEARWATER, CITY OF PO BOX 4748 CLEARWATER 33758 4748	
20 292916703082002300	360 91	MANGROVE CAY OFFICE JOINT VENTURE C/O WESTFALIA REALTY 13925 58TH ST N CLEARWATER 33760	17755 US HIGHWAY 19 N
21 292916000003200100	342 27	BAIR, LINDA D 235 WINDWARD PSE CLEARWATER 33767 2237	17423 US HIGHWAY 19
22 292916095010010010	0 10	D S A MARINE SALES & SERV 6921 14TH ST W BRADENTON 34207 5812	17335 US HIGHWAY 19 S
23 292916524530000010	342 27	D/L MOTOR CO 10103 9TH ST N STE A ST PETERSBURG 33716 3807	17275 US HIGHWAY 19
24 292916524530000010	360 91	D/L MOTOR CO 10103 9TH ST N STE A ST PETERSBURG 33716 3807	
25 292916524530000030	0 10	D/L MOTOR CO 10103 9TH ST N STE A ST PETERSBURG 33716 3807	

PARCEL NO.	LAND USE CODE	PROPERTY OWNER & ADDRESS	PROPERTY ADDRESS
26 292916616180000280	360 91	HERIE, RAYMOND 602 PALM BLUFF ST CLEARWATER 33755 3053	
27 322916703624001000	920 83	PINELLAS BD OF PUB INST PO BOX 2942 LARGO 33779 2942	6033 150TH AVE N
28 043016775150002100	360 91	ICOT LAND LTD C/O WESTFALIA REALTY ATTN: GENERAL MGR 17757 US HIGHWAY 19 N STE CLEARWATER 33764 6559	
29 043016775150002200	0 40	FIRST SECURITY BANK TRE AUTONATION TRUST 1996-1 79 S MAIN ST SALT LAKE CITY 84111 1901	13600 ICOT BLVD
30 043016775150002502	325 33	ARCADE RESTAURANT LTD 15201 ROOSEVELT BLVD STE 112 CLEARWATER 33760 3559	13563 ICOT BLVD
31 043016775150002500	324 16	KEDAN, ELLA 2354 HADDON HALL PL CLEARWATER 33764 7510	13501 ICOT BLVD
32 043016775150002700	330 17	RUBIN ICOT CENTER LTD C/O WESTFALIA REALTY ATTN: GENERAL MGR 13925 58TH ST N CLEARWATER 33760	13773 58TH ST N
33 043016775150003100	330 17	RUBIN ICOT CENTER LTD C/O DELOITTE & TOUCHE 201 E KENNEDY BLVD STE 700 TAMPA 33602	13630 58TH ST N
34 043016775150002501	331 23	KRIZMANICH, MICHAEL G SEMINOLE FINANCE CORP 5801 ULMERTON RD STE 203 CLEARWATER 33760 3951	5801 ULMERTON RD
35 043016775150002601	312 39	WINN LTD PTNSP NORTH CAROLINA 2209 CENTURY DR STE 300 RALEIGH 27622	13625 ICOT BLVD

PARCEL NO.	LAND USE CODE	PROPERTY OWNER & ADDRESS	PROPERTY ADDRESS
36 043016775150003102	0 96	RUBIN ICOT CENTER LTD C/O WESTFALIA REALTY ATTN: GENERAL MGR 17757 US HIGHWAY 19 N STE CLEARWATER 33764 6559	
37 043016775150002503	0 10	RUBIN ICOT CENTER LTD C/O WESTFALIA REALTY ATTN: GENERAL MGR 17757 US HIGHWAY 19 N STE CLEARWATER 33764 6559	
38 043016775150002600	0 96	RUBIN ICOT CENTER LTD C/O WESTFALIA REALTY ATTN: GENERAL MGR 17757 US HIGHWAY 19 N STE CLEARWATER 33764 6559	
39 043016775150000100	332 18	RUBIN ICOT CENTER LTD C/O WESTFALIA REALTY ATTN: GENERAL MGR 13925 58TH ST N CLEARWATER 33760	
40 093016709922000300	541 48	CARR-RUBIN ASSOCIATES 15201 ROOSEVELT BLVD STE 1 CLEARWATER 33760 3559	13100 56TH CT N
41 093016709922000300	541 48	CARR-RUBIN ASSOCIATES 15201 ROOSEVELT BLVD STE 1 CLEARWATER 33760 3559	13100 56TH CT N
42 093016709922000501	330 17	STONE, J O TRE PO BOX 17860 CLEARWATER 33762 0860	5590 ULMERTON RD
43 093016709922000401	520 41	BELCHER, G WELLS CLIMER, CLYDE H P O DRAWER T ST PETERSBURG 33731	5570 ULMERTON RD
44 093016709922000402	520 41	THOMAS, WILLIAM R TRE THOMAS, MARILYN G TRE C/O MASTER, DAVIT 5560 ULMERTON RD CLEARWATER 33760 4011	5560 ULMERTON RD
45 093016709922000502	330 17	CARR-RUBIN ASSOCIATES 15201 ROOSEVELT BLVD STE 1 CLEARWATER 33760 3559	13191 56TH CT N
46 093016709922000403	330 17	CARR-RUBIN ASSOCIATES 15201 ROOSEVELT BLVD STE 1 CLEARWATER 33760 3559	13161 56TH CT N

PARCEL NO.	LAND USE CODE	PROPERTY OWNER & ADDRESS	PROPERTY ADDRESS
47 093016709922000300	541 48	CARR-RUBIN ASSOCIATES 15201 ROOSEVELT BLVD STE 1 CLEARWATER 33760 3559	13100 56TH CT N
48 093016709924001210	360 91	PINELLAS COUNTY ATTN: ENGINEERING R/W DIV 315 COURT ST CLEARWATER 33756 5165	
49 093016709924001210	360 91	PINELLAS COUNTY ATTN: ENGINEERING R/W DIV 315 COURT ST CLEARWATER 33756 5165	
50 163016779810000370	360 91	S M B P PROPERTY OWNERS ASSOCIATION INC 11500 47TH ST N CLEARWATER 33762	
51 163016779810000380	360 91	S M B P PROPERTY OWNERSASSOCIATION INC 11500 47TH ST N CLEARWATER 33762	
52 233016000001300100	00089	ST. PETERSBURG, CITY OF P.O. BOX 2842 ST. PETERSBURG 33731-2842	

MPO\CORR4.RPT

PINELLAS TRAIL						
S-T-R	MAP #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
9-28S-16E	1-West	1	1	Beverly Heath & Rehab	CC-297	None
8-28S-16E	2-East	2	2	Beverly Heath & Rehab	CC-298	R/W Util. For Bonaventure CR & ponds. SWFWMD outfall canal.
					CC-299	Esmt & R/W Util w/ County for 60" water main on S. side R/W
CROSS LAKE ST. GEORGE DRIVE						
8-28S-16E	2-West		2	Beverly Heath & Rehab	LTC-21	
CURLEW SUB						
17-28S-16E	3-North	3		FPC	N/A	Landscape Agmt w/Clearwater on east side of sub.
17-28S-16E	3-North	3	4	Curlew Mo. Ho. Est.	NC-87	R/W w/Curlew Development for parking for rec. vehicles
17-28S-16E	3-North	3	5	Curlew Mo. Ho. Est.	NC-86	30' wide Esmt w/County for utilities along S'ly R/W. 4478/832
17-28S-16E	3-North	3	5	Curlew Mo. Ho. Est.	NC-85	R/W Util w/County for gravity sewer
						30' wide Esmt w/County for utilities along S'ly R/W. 4478/832
CROSS CURLEW ROAD						
17-28S-16E	3-South	3	14	John & Carol Schoeneck	NC-84	30' wide Esmt w/County for utilities along S'ly R/W. 4478/832
						60" & 64" watermain on E. side R/W
						R/W Util w/Clearwater for WRA
						City of Clearwater 12" sanitary sewer
						10' wide R/W Util on west side R/W w/Clearwater. 6/82
CROSS MEADOW WOOD						
17-28S-16E	3-South	3		FPC	NC-83	30' wide Esmt w/County for utilities along S'ly R/W. 4478/832
						County 60" watermain
						R/W Util w/Clearwater for 10' strip on West side R/W
						Esmt & R/W Util w/County for 60" water main
						R/W Util w/Clearwater for 12" force main, 10/90
20-28S-16E	3-South	6		FPC	NC-82	Esmt Agmt w/Clearwater for 12" sanitary sewer
						R/W Util w/US Home for 30" storm sewer
						10' wide R/W Util on north side R/W w/Clearwater. 6/82
						10' wide R/W Util (N & S) w/Clearwater. 6/82
						R/W Util w/Clearwater for 12" force main, 10/90
20-28S-16E	3-South	6		FPC	NC-81	R/W Util w/Clearwater for 12" force main, 10/90
CROSS NORTHRIDGE DRIVE						

Page 2						
S-T-R	MAP #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
20-28S-16E	4-North	6		FPC	NC-80	Esmt & R/W Util Agmt w/County for 60" water main. 20' wide R/W Util w/Clearwater for 12" force main, 10/90 Esmt Agmt w/Clearwater for 8" sewer line. 7/78
20-28S-16E	4-North	6		FPC	NC-79	Esmt Agmt w/Build America for 8" sanitary sewer main. 12/88 R/W Util w/Clearwater for 12" force main, 10/90 Esmt Agmt w/Clearwater for 8" sewer line. 7/78 & 9/76 Esmt & R/W Util Agmtw/ County for 60" water main. 9/91 Note: GTE Media Ventue S & E side R/W buried cable; no agmt found
CROSS NORTHSIDE DRIVE						
20-28S-16E	4-North	6		FPC	NC-78	R/W Util w/Clearwater for 12" force main, 10/90 Esmt Agmt w/ Gen Tel for 10' wide (E & W) for underground cable. 1/78 Amen. To Drainage Esmt w/ Clearwater for 20' wide (E&W). 1/77 storm drain crossing. Org. Esmt 4514/1959
20-28S-16E	4-North	6		FPC	NC-77	Amen. To Drainage Esmt w/ Clearwater for 20' wide (E&W). 1/77 storm drain crossing. Org. Esmt 4514/1959 Drainage Esmt w/County 20' wide (E & W). 1/77. 4514/1959
20-28S-16E	4-South	6		FPC	NC-76	Drainage Esmt w/County 20' wide (E & W). 1/77. 4514/1959
20-28S-16E	4-South	6		FPC	NC-75	Drainage Esmt w/County 20' wide (E & W). 1/77. 4514/1959
29-28S-16E	5-North	7		FPC	NC-74	Drainage Esmt w/County 20' wide (E & W). 1/77. 4514/1959
					conflict	
29-28S-16E	5-North	7		FPC	NC-73	10' wide (E & W) esmt w/General Tel for service wire. 4/86 LA w/ Clearwater for parking & drainage. 11/95
29-28S-16E	5-North	7		FPC	NC-72	
CROSS SR 580						

Page 3						
CROSS COUNTRYSIDE BLVD						
S-T-R	MAP #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
29-28S-16E	5-South	7		FPC	NC-71	Esmt Agmt w/Clearwater for subsurface drainage & 3' high gravity wall. 8/89
						LA w/ County for 54" water main on W side R/W. 10/81.
						LA w/Bellwether Prop. for Countryside Mall parking. 4/87
29-28S-16E	5-South	7		FPC	NC-70	Esmt Agmt w/Time Warner underground conduits (E & W). 4/96
						Esmt Agmt w/Vision Cable for underground CATV cables (E&W). 5/83.
						R/W Agmt w/US Home for parking, tennis court, fence, landscaping, jogging path & driveway on W side R/W
29-28S-16E	5-South	7		FPC	NC-69	Esmt Agmt w/Inverness Condominium for parking on W. side R/W
						LA w/ County for 54" water main on W side R/W. 10/81.
						R/W Agmt w/US Home for parking, tennis court, fence, landscaping, jogging path & driveway on W side R/W
CROSS ENTERPRISE ROAD						
32-28S-16E	6-North	9		FPC	NC-68	Amend. To LA w/Aid Assoc. for Lutherans for parking on E side
					conflict area	R/W. 11/94.
						LA w/ Oxford/Concord Investments for 10x20 tract for sign on E. side R/W. 5/89. (AKA Cypress Pt. Holding Corp.)
						LA w/Chilli's for 64'x300' tract for parking on S. side R/W. 8/83.
						LA w/ County for 54" water main on W side R/W. 10/81.
						LA w/ Cypress Pt. Holding Corp. for 44 employee parking spaces. 10/83 (Chilli's)
						Note: TGIF parking lot in ANL R/W which is easement held
						Note: No agmt found for GTE
CROSS US 19						
31-28S-16E	6-North	10		FPC	NC-67	10' wide Esmt Agmt w/ Clearwater for 6" force main (N & S). 10/91.
						LA w/ County for 54" water main on W side R/W. 10/81.
						LA w/Sidney Colen for golf course. 9/67.
						Note: Chain link fence on FPC property on E side R/W
31-28S-16E	6-North	10		FPC	NC-66	LA w/ County for 54" water main on W side R/W. 10/81.
						LA w/Sidney Colen for golf course. 9/67.

Page 4	S-T-R	MAP #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
	31-28S-16E	7-South	10		FPC	NC-65	LA w/ County for 54" water main on W side R/W. 10/81.
							LA w/Sidney Colen for golf course. 9/67.
	CLEARWATER SUBSTATION						
	31-28S-16E	7-South	10		FPC	NC-64	LA w/ County for 54" water main on W side R/W. 10/81.
	31-28S-16E	7-South	10		FPC	NC-63	LA w/ County for 54" water main on W side R/W. 10/81.
						HD-22	
						ANL-90	
	31-28S-16E	7-South	10		FPC	NC-62	LA w/ County for 54" water main on W side R/W. 10/81.
						HD-23	Pond
							LA w/ Sidney Colen for spray irrigation system. 9/81
							Esmt Agmt w/GTE for two 4 in. conduits. (E & W) 2/94
	6-29S-16E	8-North	11		FPC	NC-61	LA w/ County for 54" water main on W side R/W. 10/81.
						HD-24	Drainage Esmt w/ Parkway Condo Mgmt Co. 4' wide (E & W) 7/70.
	6-29S-16E	8-North	11		FPC	NC-60	LA w/ County for 54" water main on W side R/W. 10/81.
						HD-25	LA w/ US Home for storm drainage outfall pipe & swale system; 9/73
							Lettr of no objection to Rev. Kirstein to use w. 20' for garden. 10/66.
							Pond
	CROSS SUNSET POINT ROAD (aka Main Street)						

Page 6	S-T-R	MAP #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
	7-29S-16E	9-South	12		FPC	NC-54	LA w/ County for 54" water main on W side R/W. 10/81.
						HD-31	Esmt w/County for drainage ditch (Alligator Creek)
							Esmt Agmt w/Clearwater for 26' wide bike path across R/W
							No objection letter to Clearwater for handicapped parking on E. side R/W (Carpenter Field)
							Esmt Agmt w/Clearwater for 34' wide strip for road w/18" RCP across R/W
							LA w/ Clearwater for parking. 12/86.
	7-29S-16E	9-South	12		FPC	NC-53	LA w/ County for 54" water main on W side R/W. 10/81.
						HD-32	Access Esmt w/Clearwater (2510/115 50' across R/W)
							Esmt Agmt w/ Gen Tel for 10' wide strip for OH cable across R/W
	7-29S-16E	9-South	12		FPC	NC-52	LA w/ County for 54" water main on W side R/W. 10/81.
						HD-33	LA w/Vision Cable (Time Warner) for parking & fence across R/W. 9/89
							Esmt w/Clearwater for 20' wide strip for water & gas mains
							OR 2510/111
							Esmt Agmt w/Target & Vision Cable for 2 paved street, culverts & water main in 150' wide strip across R/W
							Esmt Agmt w/Clearwater for 20' wide strip for 8" water line
	CROSS DREW STREET						
	18-29S-16E	10-North	13		FPC	NC-51	No objection letter to GTE FL for mutiple underground cables
						HD-34	across R/W
						conflict	LA w/County for 54" water main. 10/81.
						area	Access Esmt w/Hyprops for 2 60' areas. 7264/786
							LA w/ SPJC for 341 paved spaces & 136 turf spaces w/ d/w, sidewalks, drainage. 11/88
							Esmt Agmt w/ American Diversified Captial for 8" water main crossing
							Esmt Agmt w/ Post Court for landscaping, irrigation, lighting, driveway
	18-29S-16E	10-North	13		FPC	NC-50	LA w/Greek Orthodox Church for overflow parking (entire R/W
						HD-35	So of NC-50 to SR 60). 6/92
						conflict	
						area	
	CROSS GULF TO BAY BLVD						

Page 7						
S-T-R	MAP #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
18-29S-16E	10-South	13		FPC	NC-49	Esmt Agmt w/Clearwater for traffic signal system & sidewalk
					HD-36	10' wide water main esmt 2343/615 adj. S. SR 60.
CROSS DRUID ROAD						
18-29S-16E	10-South	13		FPC	NC-48	Esmt Agmt w/Clearwater for 10' wide strip for underground
					HD-37	storm drain along E. side R/W
						Esmt Agmt w/Gen. Tel for 1" underground cable in 5' strip
						along E. Side R/W
						LA w/County for 54" water main. 10/81.
						Esmt Agmt w/South Gate Park for 10" water main in 10' strip
						(N & S)
BAYVIEW SUBSTATION						
19-29S-16E	11-North	14		FPC	NC-47	R/W Util Agmt w US Home drainage ditch on E side R/W
				(E 1/2 R/W only)	HD-38	LA w/County for 54" water main. 10/81
						Emst Agmt w/Clearwater for detention ponds. 2/82.
CROSS BURNICE						
19-29S-16E	11-North	14		FPC	NC-46	Esmt Agmt w/Clearwater for detention pond &
				(E 1/2 R/W only)	HD-39	fence in 130' wide strip. 2/82. 5328/1351. Use in conjunction
						w/ existing ponds in 943/562.
						No objection letter to County to install 48" culvert pipe on W.
						side R/W. 4/76
						R/W util Agmt w/Pinellas Public Works for drainage pipe. 12/76.
CROSS BRENTWOOD DRIVE						
	11-North					deep ditch across R/W
CROSS HARN BLVD						
19-29S-16E	11-North	14		FPC	NC-45	LA w/Morningside-Meadows HOA to install columns on N & S
					HD-40	side Ham Blvd. 3/96.
						8' wide Esmt on E side R/W for sewer 1402/578
						LA w/County for 54" water main. 10/81.
						Esmt w/Clearwater for portion Ham Blvd.

Page 8						
S-T-R	MAP #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
19-29S-16E	11-South	14		FPC	NC-44	Esmt Agmt w/Clearwater for 8" SDR 18 along E side R/W. 4/83. 5578/1206
					HD-41	LA w/County for 54" water main. 10/81.
				Lake		Lmt. LA w/ James Gills for 60' x 250' tract for unpaved parking. 8/87.
						Esmt Agmt w/Premier Shelters for 5' x 50' strip for berm, sewer & manhole on W side R/W
						Esmt w/Ray E. Lee for underground sanitary form main. 7/63.
						Drainage Esmt Agmt w/Clearwater to incorporate underground pipes into an existing lake. 3/75. 4273/528
CROSS NURSERY ROAD						
19-29S-16E	11-South	14		FPC	NC-43	Esmt Agmt w/Clearwater for 2 detention ponds
					HD-42	
19-29S-16E	11-South	14		FPC	NC-42	
CROSS BELLAIR ROAD						
30-29S-16	12-North	15		FPC	NC-41	Orange grove owned by Al Repetto, Orange Blossom Groves (Phone: 392-1277)
					HD-43	Mr. Repetto will allow 15' swath thru grove in exchange for fencing grove.
30-29S-16	12-North	15		FPC	NC-40	LA w/Angel Properties & Jersey Jim for 22' high sign
					HD-45	LA w/ Jersey Jim for parking
START OF ALTERNATE ROUTE ONE						
CROSS US 19						
						LA w/Happy Days RV Sales for Parking (50' x 150')
29-29S-16E	12-North					
CROSS COVE CAY DRIVE						
29-29S-16E	12-North	16		FPC	NC-39	R/W Util Agmt w/David S. Allen, Jr. for retention basin
					HD-46	LA w/ Cove Cay Country Club for tree & sod nursery. 5/86
						R/W Util w/Mangrove Cay Joint Venture for access drive & parking
						Emst w/Gen Tel for underground cable 170' S of N. bdry of Cove Cay
						Esmt Agmt w/The Ervin Company for paved access, wall & landscaping
CROSS HAINES BAYSHORE						
END OF ALTERNATE ROUTE ONE						

Page 9						
S-T-R		FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
29-29S-16E	13-South	16		?	NC-38	Esmt Agmt w/Largo across R/W (N & S)
					HD-47	60' wide access Esmt w/ John Todd off Cardinal Road
						Sewer Esmt w/Largo (10' wide along E. side R/W)
						Access Esmt w/Tony D. Davis (24' wide) off E side Allendale Drive. 11/78
29-29S-16E	13-South	16		FPC (sold W 1/2)	NC-37	R/W Util Agmt w/Robert Kendzie for cul de sac & storm drain
					HD-48	Sidewalk esmt w/County
CROSS WHITNEY ROAD						
32-29S-16E	14-North	17		FPC	NC-36	Esmt Agmt w/ Cleawater for raw water & well sites on W side R/W
					HD-49	Esmt for Abilities parking lot and access
32-29S-16E	14-North	17		FPC	NC-35	Esmt Agmt w/ Cleawater for raw water & well sites on W side R/W
					HD-50	Esmt Agmt w/Largo for sewer on E. side of R/W
						Esmt Agmt w/County for sidewalk on N side Whitney Road
						Garden Permit w/William Rose
TRI-CITY SUBSTATION						
CROSS 82ND STREET						
32-29S-16E	14-North	17		FPC	NC-34	
					HD-50	
32-29S-16E	14-North	17		FPC	NC-33	Esmt w/Gen Tel for buried cable (10' wide across R/W)
						Letter of no objection w/Longbranch Apart to install gate on R/W
						LA w/ Roger Broderick for sign at Longbranch Apartments
						LA w/ Longbranch Apart for paved parking & sidewalk
						Esmt Agmt w/Longbranch Apart for paved road & water main (50'wide)
CROSS ROOSEVELT						
32-29S-16E	14-South	17		FPC	NC-32	Sewer Esmt Agmt w/ Donald V. Missling for 10' wide strip on S side
						Roosevelt Blvd.
						Letter of Agmt w/School Board for sign & landscaping
						Nursery in R/W
						Esmt w/Florida Assoc., LP for sign, access & sidewalk. 9163/1167
						60' sold to County for Road. 1821/651, 11/63
32-29S-16E	14-South	17		FPC	NC-31	Water Main Esmt w/Russel Lampe for 10' strip crossing R/W
				70'x 1187' sold to School		
				Bd. 2879/657		

Page 10						
S-T-R	MAP #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
CROSS 150TH AVE						
5-30S-16E	15-North	18		FPC	NC-30	Letter of no objection w/Kenneth Ferqueron for driveway crossing
						Letter of no objection w/church of God for shelled roadway
						LA w/Trader Publications for parking
CROSS 146TH AVE						
5-30S-16E	15-North	18		FPC	NC-29	Emst Agmt w/Largo 10" sewer line (10' strip along E side R/W)
						Sidewalk esmt w/County
						LA w/ Cox Trader Publications for 175'x353' tract for retention
						landscaping and flood lighting. & LA for paved parking. 6/89
						LA w/V. J. Properties 75'x125' tract for retention
						Esmt Agmt w/Wm. Gehrand for waterline crossing (10' wide strip)
						Letter of no objection w/Wm. Gehrand to construct road across R/W
						Esmt Agmt w/Gen Tel for 10' wide strip across R/W for cable
						Esmt Agmt w/Count for drainage diagonally across R/W
						Esmt Agmt w/Mears Industrial Complex for 61' strip for
						detention pond & underground piping
						Access Esmt w/Wayne Wetzel (30' across R/W)
5-30S-16E	15-North	18		FPC	NC-28	LA w/Peter Brown for Sanitary sewer manhole & conection along
						E side R/W
						Esmt Agmt w/C & W Properties for 8" underground water main
						w/hydrant. (10' wide strip across R/W)
						Emst Agmt w/Largo 10" sewer line (10' strip along E side R/W)
						Roadway Esmt w/R. H. Ower Sand & Rock Transport.
						LA w/V. J. Properties for access, parking, pipes, fire hydrant
CROSS 142ND AVE						
5-30S-16E	15-South	18		FPC	NC-27	Letter of no objection w/Pleasure World for swale
					& UL-6	
5-30S-16E	15-South	18		FPC	NC-26	R/W Util Agmt w/ R. F. Properties for parking along E'ly side R/W
						& landscaping
						R/W Util Agmt w/R. F. Properties for paved drive, parking
CROSS ICOT BLVD						
						sidewalk & landscaping. 11/85.
						sidewalk & landscaping

PAGE 11		MAP 4				
S-T-R	MAP #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
5-30S-16E	15-South	18	38	Rubin ICOT Center	NC-25	R/W Util Agmt w/ R. F. Properties for parking along E'ly side R/W
						R/W Util Agmt w/R. F. Properties for paved parking & drainage.
						7/86 & 12/85
						R/W Util Agmt w/Rubin Dev. for rd & utility crossings, retention
CROSS 58TH STREET						
4-30S-16E	Not	19	39	Rubin ICOT Center	NC-24	R/W Util Agmt w/R. F. Properties for road & Parking. 5/85
	shown					R/W Util Agmt w/Largo for 12" DIP effluent pipe S side 58th Street. 5/88
	on map					R/W Util Agmt w/R. F. Properties for d/w for mail box delivery. 8/88
						R/W Util Agmt w/R. F. Properties for parking, d/w & drainage. 5/86
						R/W Util Agmt w/Rubin Dev. for rd & utility crossings, retention
CROSS ULMERTON						
BEGIN ALTERNATE ROUTE 3						
9-30S-16E	16-North	20	42	J. O. Stone, Tr.	NC-23	R/W Util Agmt w/James Rosseway for chain fence
						R/W Util Agmt w/ Carr Rubin Assoc for 8" water main crossing
						R/W Util Agmt w/ Carr-Rubin Assoc for 12" paved d/w & drain pipes
						R/W Util Agmt w/Davit Master for retention pond
						R/W Util Agmt w/Gen Tel for buried cable at Rubin Center
						R/W Util Agmt w/Carr-Rubin Assoc. for lighting, landscaping & sign
						R/W Util Agmt w/Bruce Grimes for parking & detention areas
						Letter of no objection w/county for Belcher Machine runoff
						R/W Util Agmt w/Oliver Boat Lift for retention & parking
						Letter of no objection w/ Cathleen Daniel for landscaping & roadway
ULMERTON SUBSTATION						
9-30S-16E	16-North	20		FPC	NC-22	
					UL-1	
9-30S-16E	16-North	20		FPC	NC-21	
END ALTERNATE ROUTE ROUTE 3						
9-30S-16E		20		FPC	NC-20	LA w/Robert Gorby for fence & d/w crossing. 9/76
					HD-62	
CROSS 126TH AVE						
9-30S-16E	16-South	20		FPC	NC-19	Roadway Esmt w/County for 126th Ave
					HD-63	LA w/Robert Gorby for 2 paved d/w & parking
9-30S-16E	16-South	20		FPC	NC-18	LA w/Driggers Engineering Services for 54x221' tract for fenced parking
					HD-64	
BEGIN ALTERNATE ROUTE 4						
CROSS 49TH STREET						
9-30S-16E		20		FPC	NC-17	Esmt for road w/Carolina Freight Carriers (60' wide)
CROSS 118TH AVE						
END ALTERNATE ROUTE 4						

Page 12						
S-T-R	Map #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
16-30S-16E	17-North	21	50	SMB Property Owners	NC-16	R/W Util Agmt w/Terra Excavating for 50'wide water esmt along
				Assoc.	HD-66	W side R/W
						R/W Util Agmt w/ Pinellas Park for retention, parking & pipe
						R/W Util Agmt w/ Silver Ridge Assoc. for retention, parking & pipe
CROSS 47TH STREET						
16-30S-16E	17-North	21	51	SMB Property Owners	NC-15	R/W Util Agmt w/ Flagship Bank Pinellas for ?
				Assoc.	HD-67	
15-30S-16E	18-West	22			off R/W	
TRAIL TURNS EAST AND RUNS ON NORTH SIDE 110th DRIVE						
	18-East				off R/W	
	19-North				off R/W	
PROPOSED TIE W/PINELLAS PARK TRAIL						
15-30S-16E	18-West				NC-14	
					HD-68	
15-30S-16E	18-West				NC-13	
					HD-69	
15-30S-16E	18-West				NC-12	
					HD-70	
END OF PROPOSED TIE W/ PINELLAS PARK TRAIL						
22-30S-16E	20-West	23		FPC	NC-6	R/W Util Agmt w/ Roger Broderick rd. crossing, ditch, utilities &
						2 retention ponds
						Esmt Agmt w/Gateway Centre Devl. District for road crossings,
						utilities & ditch
						Agmt w/St. Petersburg for spreading excess mulch
						Esmt Agmt w/ Anthony Miele for streets, water & sewer lines across
						R/W
22-30S-16E	20-West	23		FPC	NC-5	R/W Util Agmt w/ Roger Broderick rd. crossing, ditch, utilities &
						2 retention ponds
						Agmt w/St. Petersburg for spreading excess mulch
						Esmt Agmt w/Gateway Centre Devl. District for road crossings,
						utilities & ditch
						Roadway Esmt w/ County
						Esmt Agmt w/ Anthony Miele for streets, water & sewer lines across
						R/W

Page 13						
S-T-R	Map #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
CROSS 28TH STREET						
23-30S-16E	20-East	24		FPC	NC-4	R/W Util Agmt w/ Roger Broderick rd. crossing, ditch, utilities & 2 retention ponds
23-30S-16E	20-East	24		FPC	NC-3	
CROSS I-275						
23-30S-16E	20-East	24		FPC	NC-2	
24-30S-16E	21-West	25		FPC	NC-1	Esmt Agmt w/ Struthers Excavating for 25' wide access strip across R/W
						Esmt Agmt w/L. J. Hennessy for 25' wide access strip across R/W
24-30S-16E	21-West	25		FPC	NX-2	R/W Util Agmt w/ Lampe & Son for parking
						Grady Pridgen parking
NORTHEAST SUBSTATION						
24-30S-16E	21-West	25		FPC	BNUG	LA w/Gandy/275 Assoc. for 7'x790' & 7'x341' strip for landscape screen
CROSS 18TH STREET						
CROSS 16TH STREET						
24-30S-16E	21-East	25			BNUG	Esmt Agmt w/ Gandy/275 Assoc for 2 storm drains, 60' wide strip NE 18th St., 8" water main, 100' wide strip for 16th St. & 12" water main
						LA w/ Barnett Bank for landscaping & access rd
						Esmt Agmt w/ Struthers Excavating for 25' wide area for access rd.
CROSS GANDY BLVD						

Page 14						
S-T-R	MAP #	FILE #	PARCEL #	OWNER	STR. #	TYPE OF AGREEMENT
19-30S-17E	22-West	26		FPC	BNUG 3	LA w/ Koger Properties for 675' long strip for landscaping & irrigation
						Esmt Agmt w/ Gen Tel for 10' wide strip for 2 4" PVC conduits
						crossing R/W
						Esmt Agmt w/ St/ Petesburg for 60' wide strip for 8" sewer line crossing (N & S)
						Roadway Esmt w/ Koger Properties for 60' wide crossing
						LA w/ Teleprompter Southeast for 2' x 1500' strip along N side R/W
Cross 5th Street North					BNUG	for cable
	22-West					LA w/Koger Properties for sign
						Esmt w/ St. Petersburg for 10' wide utility strip for sewer
						LA w/ William Charls Venzke for 7x20' strip for storage of 16'
						Catamaran. 12/86. (Active)
						Esmt Agmt w/ J. J. W., Inc. for 80' wide strip for 12" culvert & 2
						24' d/w
						Esmt w/ Gen Tel for 5' wider strip for cable
Cross 4th Street North						
	22-East			FPC	BNUG	Esmt Agmt w/ People Gas System for 10' wide strip for 6" gas main
						along S side R/W
						Esmt w/ St. Petersburg for 2nd Street North
						Letter to J.J. W. Construction, Inc. to use R/W for hibiscus hedge
20-30S-17E	23-West	17		FPC	BNUG	Esmt Agmt St. Petersburg for 10' wide strip for 42" sewer line
						West side San Martin Blvd
						Esmt Agmt w/ People Gas System for 10' wide strip for 6" gas main
						along S side R/W
						Esmt Agmt w/ GTE FL for 10' wide strip on S side R/W
CROSS SAN MARTIN BLVD.						
9/30/98						

FLORIDA POWER TRAIL PROGRAM

Concept of the Trail

The Florida Power Trail furthers Pinellas County's commitment to provide a significant alternative means of transportation for the residents and visitors of Pinellas County. The Trail forms a key link in the Trail loop that is intended to completely encompass Pinellas County. The loop was originally begun with the establishment of the Pinellas Trail in the western corridor of the County. This Power Line Trail completes that loop on the east side of the County. The Florida Power Line will provide a significant transportation corridor in that it ties many varied urban land uses together within the County. There are a large number of schools, large and small industries, retail shopping areas, churches, and social recreation activity points that are connected by this alignment. In addition, the Trail will also serve as a significant recreation and exercise facility.

Due to the strategic placement of the Trail and its linkage of the large number of trip generators, this facility is anticipated to provide a significant diversion for travel of people that would either normally use an automobile or who do not have the choice of an automobile. It is anticipated, as the facility is developed, there will be supporting facilities provided along the corridor making it easy for people to use or take advantage of the Trail. It should be noted that the Florida Power Corporation has permitted this joint use of their property with the assumption that the placement of the Trail facility in this Florida Power line corridor will not open up the Corporation's properties to activity beyond the actual use of the Trail corridor. It is for this reason that any support activity or facilities will be focused at crossover roadway points or adjacent public or private properties.

General Corridor Alignment

Incorporated in the MPO's Long Range Transportation Plan is a 20.6-mile corridor of the Florida Power Corporation corridor that will contain a bike/pedestrian trail. This corridor begins at the intersection of Tampa Road and McMullen-Booth Road. The corridor proceeds west and then south, eventually crossing U.S. 19 from east to west just south of Enterprise Road. The corridor then proceeds southerly on the west side of U.S. 19 to just below Belleair Road. At that point, the corridor crosses U.S. 19 from west to east diagonally proceeding southeast, crossing East Bay Drive and then Ulmerton Road. The corridor continues in a southeastern direction to approximately 110th Avenue, at which point the Trail then utilizes County property to the east along the 110th Avenue alignment. The Trail then proceeds south along the 34th Street alignment along a drainage way to the point where it crosses the Florida Power corridor. At that point, the Trail proceeds east within the Florida Power corridor, crossing I-275 and terminating at the Weedon Island Preserve.

Trail Agreements

Florida Power Corporation has agreed to construct the Trail under contract with Pinellas County and then turn that Trail over to the County through a 99-year lease, with a \$150,000 proviso annually. Realistically, this will be accomplished in segments. Therefore, the procedure that will be used is to put in place an Agreement to Lease. Then, as each segment is constructed and completed by Florida Power, the Lease Agreement will then be effectuated by the Board of County Commissioners to include that segment. The third document, the Joint Partnership Agreement, defines how Florida Power will perform the work of plan, engineering, and constructing the Trail. This work will be funded by the Penny for Pinellas Extension which includes funds for such Trail improvements, with a limit of up to \$15 million. This Trail corridor is included in the MPO Plan.

Trail Description

The Florida Power Corporation corridor is a 20-mile corridor that varies in width but is approximately 200-feet wide in most areas. This is an actively-used corridor with large erector towers placed several hundred feet apart. The Trail has been positioned through this corridor to avoid conflicts with existing design features of the power company or natural features such as ponds or previously-approved uses such as parking lots. The Trail will be a 15-foot wide paved area, with a grassy area of several feet on each side, which would be routinely mowed.

The entire corridor is recognized to have four significant highway crossings that will require an overpass for the safe passage of the Trail users. These overpasses are identified to be:

- A) The U.S. 19 crossing just south of Enterprise Road;
- B) The U.S. 19 crossing just south of Belleair Road;
- C) The Ulmerton Road crossing in the Icot area; and
- D) The I-275 crossing just north of Gandy Boulevard.

There are other highway crossings that exist in the corridor but they are considered less intensive and are positioned in the manner that either a traffic signal exists at that location or some other procedure could be utilized for a crossing. Due to the situation around these overpass areas, it is presumed that a compact design should be utilized for the overpasses. That is, rather than utilizing an extended ramp approach, the design will be condensed into a spiral or a design where there is minimum lateral size. Presumption of a compact design may not necessarily apply to the I-275 overpass, where a ramp approach may be considered acceptable. It is further considered that the overpasses would utilize the 12-foot width standard that has been established for such overpasses on the existing Pinellas Trail facility.

Trail Development

The Trail is divided into five segments. These segments are divided by the four necessary overpasses for the program.

Segment A begins at Tampa Road and McMullen-Booth Road and ends at U.S. 19 in the Enterprise Road area. This segment is 4.5 miles long. Segment A passes through the unincorporated area of Pinellas County and the municipal jurisdiction of the City of Clearwater.

Segment B begins at U.S. 19 in the Enterprise Road area and terminates at the U.S. 19 crossing just below Belleair Road for a length of 5.5 miles. This segment proceeds through the unincorporated area of Pinellas County and the municipal jurisdiction of the City of Clearwater.

Segment C begins at the U.S. 19 crossing just south of Belleair Road and proceeds to the Ulmerton Road crossing in the vicinity of Icot Boulevard for a length of 2.8 miles. This segment lies entirely within the unincorporated area of Pinellas County.

Segment D begins at Ulmerton Road and Icot Boulevard and proceeds to I-275 for a length of 5.3 miles. This segment falls within the jurisdiction of the unincorporated area of Pinellas County and the municipal jurisdiction of the City of Pinellas Park.

Segment E begins at I-275 and ends at San Martin Boulevard at the Weedon Island Preserve for a length of 2.5 miles. This segment falls within the municipal jurisdiction of the City of St. Petersburg and the unincorporated area of Pinellas County.

Therefore, there are four jurisdictions that are affected or involved with this corridor. These are: the unincorporated area; the City of Clearwater; the City of Pinellas Park; and the City of St. Petersburg. Pinellas County will be developing and maintaining the Trail as a part of the Pinellas Trail network. Pinellas County will do this in cooperation with the jurisdictions along the corridor. The five segments that are indicated will be developed with an at-grade Trail facility, with appropriate provisions made for highway crossings for the Trail users. The Trail will be designed to accommodate cyclists, rollerbladers, joggers, and pedestrians.

It is recognized that the Trail can be initially developed along these segments and made available for public use in advance of the construction of the identified overpasses for this program. Each of these segments can be completed within themselves tying logical destinations together. However, it would be prudent to build the overpasses as soon as possible since there will be Trail users who will attempt to traverse these areas and will be doing so in an unsafe manner.

There are four overpasses identified as initially needed for the corridor. Overpass A provides for a crossing of U.S. 19 just south of Enterprise Road. This area contains a cluster of commercial uses on the east side of U.S. 19 and a vacant area leading to the Top of the World property on the west side. A compact flyover design is indicated at this location due not only to the proximity of other uses and a configuration of the Florida Power facilities but this is also necessary to provide reasonable and strategic accesses for people from the U.S. 19 corridor itself.

Overpass B provides for a crossing of U.S. 19 just south of Belleair Road in the Allen's Creek area. This overpass could include the boardwalk provisions necessary to cross the Allen's Creek area and position the Trail for the U.S. 19 crossing. The compact design is necessary for the overpass in this area due to the lack of room available for an extended ramp entrance and also due to the need to provide strategic accessibility to people in the U.S. 19 corridor due to the controlled access design of this facility.

Overpass C is intended to provide a crossing for the corridor at Ulmerton Road in the Icot Boulevard area. This design will need to be compact in nature due to the limited area available to place the entry points or ramping for the overpass. There are environmental limitations and structure placement limitations at this location. In addition, the compact design will make strategic access by the people in the Ulmerton Road corridor workable.

Overpass D is the corridor crossing located at I-275. This overpass can utilize the more extended ramp design since there is considerable room to work with at this point in the corridor. There is also no access requirement for people to enter from the Interstate corridor.

Trail Amenities

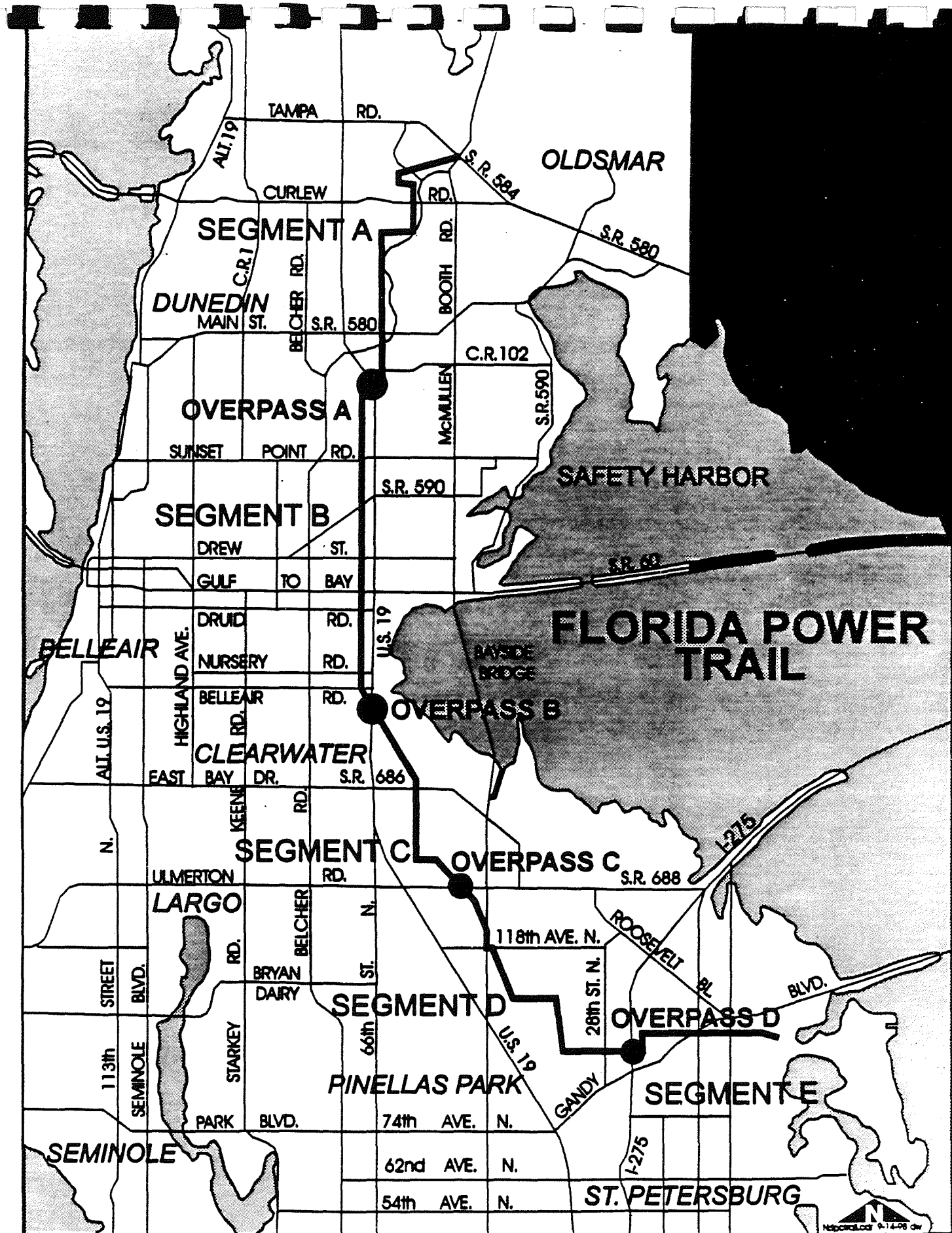
The Trail is a permitted use along the Florida Power corridor, with limited access by the Trail users to other parts of that corridor. Therefore, amenities such as benches, shelters, and water fountains will be located at crossover roads or other facilities that are adjacent to the corridor but not on the Florida Power lands.

Connecting Trail Coordination

The Pinellas MPO has an approved Bikeways Plan identifying a network of trails throughout Pinellas County. The Florida Power Line Trail is an integral part of that network and provides an eastern north/south corridor parallel to the existing Pinellas Trail.

There are several connecting community trails along the Florida Power line. At the northern terminus of this Trail is an Oldsmar Trail that connects to the Power Line Trail. This juncture is anticipated to be made at Canal Park. A second connecting trail is anticipated to be located on Curlew Road providing an east/west connection. The next connecting trail is the Enterprise Road, which proceeds east from the power line through Safety Harbor tying into the Bayshore Trail. Proceeding south, the next connecting trail is the Clearwater East/West Trail. This is a significant trail being developed by the City of Clearwater that connects on the west to the Pinellas Trail and on the east to the Bayshore Trail. The next connecting trail is the Druid Road Trail, which connects the Pinellas Trail and crosses the power line and connects to U.S. 19. The next connecting trail is the Belleair Road Trail, which connects the Pinellas Trail crossing the power line to U.S. 19. Proceeding south, the next connecting trail is the 142nd Avenue Trail of the City of Largo. Proceeding south, there are two connecting trails for the City of Pinellas Park. The first is the Pinebrook Trail and the second is the Freedom Lake Trail. The final connection is at the end of the Power Line Trail. At this point, there is a tie-in to the Weedon Island Trail and the Friendship Trail. The Friendship Trail incorporates the regional linkage to Hillsborough County utilizing the Gandy Causeway Park, which is being developed jointly by Pinellas and Hillsborough Counties. That Friendship Trail eventually terminates in Tampa at Picnic Island. It should be noted there is a third connection at this same location of the terminus of the Power Line Trail. This is the Rio Vista Trail that proceeds south to downtown St. Petersburg.





AGREEMENT TO LEASE

This Agreement, made and entered into this 3 day of December, 1999, is between Florida Power Corporation (FPC) and Pinellas County, FL (County).

Whereas, the parties intend to jointly create an extension of the Pinellas Trail on FPC properties and other properties as required, as more specifically described in the Concept Plan attached and identified as Exhibit "A" (the Trail Utilization Area); and

Whereas, the County intends to lease the Trail after the trail improvements are constructed by FPC or its subcontractors; and

Whereas, the parties desire to commit themselves to this joint project prior to the time when construction is complete and the Lease is appropriate;

Now, therefore, in consideration of the mutual covenants herein contained, the parties agree as follows:

1. Phases: It is mutually agreed by the parties that the design and construction of the Trail may be completed in phases.
2. Other Agreement: During design and construction, the respective roles of the parties shall be governed by the Joint Partnership Agreement.
3. Subsequent Lease: Following design and construction of the initial phase, the parties will enter into a Lease.
4. Transition: As each phase of design and construction is completed, the aforementioned Lease will be amended to add the real property for that phase to the Lease.
5. Interim Use: Prior to completion of any phase of the Trail, there shall be no public access or utilization of the FPC property. The determination that any phase is complete and ready for use will be by mutual agreement of the parties, signified by execution of the Lease and each subsequent Lease amendment.
6. Additional Property Rights: The County will be responsible for obtaining additional rights of way and easements outside the boundaries of FPC properties and easements, that are necessary to construct the Trail. FPC as an agent for County under the Joint Partnership Agreement may provide the service for obtaining additional rights and easements at County's expense within the boundaries of FPC properties and easements that are necessary to construct the Trail.

7. Termination: If County materially fails to comply with any of the provisions of this Agreement or defaults in any of its obligations hereunder, and fails within sixty (60) days after written notice from FPC to correct such default or noncompliance, FPC may at its option correct any such defective performance hereunder and invoice County for all expenses incurred by FPC in such correction, or terminate this Agreement. In the event that County has notified FPC in writing that correction of such default or noncompliance cannot reasonably be accomplished within such sixty (60) day period, and County demonstrates to FPC's reasonable satisfaction that County is diligently completing correction of such default or noncompliance, FPC may extend such sixty (60) day period.

If FPC materially fails to comply with any of the provisions of this Agreement or defaults in any of its obligations hereunder, and fails within sixty (60) days after written notice from County to correct such default or non-compliance, County may at its option correct any such defective performance hereunder and invoice FPC for all expenses incurred by County in such correction, or terminate this Agreement. In the event that FPC has notified County in writing that correction of such default or noncompliance cannot reasonably be accomplished within such sixty (60) day period, and FPC demonstrates to County's reasonable satisfaction that FPC is diligently completing correction of such default or non-compliance, County may extend such sixty (60) day period.

If the Utility Industry undergoes changes such that FPC decides at some future date to sell it's fee owned and easement interests, FPC may terminate this Agreement.

FPC fee owned property is subject to a lien under the indenture. FPC may terminate this Agreement if it is in default under the indenture.

The County may terminate this Agreement upon a decision by future Pinellas Board of County Commissioners to discontinue the operation of the Trail, or in the event that funds are not budgeted to fund this Agreement subject to Florida Statute 129.07, 129.08, and 129.09.

8. Consideration: Consideration for this Agreement shall be an annual payment of the sum of One hundred fifty thousand dollars (\$150,000.00) payable by the County to FPC, in exchange for the promise of FPC to forbear encumbering its easement and fee owned utility corridor for collocating the Trail.

Annual payments under this Agreement will be due on the anniversary date of the execution of the Agreement, except that the initial payment will be due thirty (30) days from execution.

9. Term: This Agreement shall be for an initial term of three (3) years, subject to fiscal funding by the County, unless terminated by the parties in accordance with the guidelines contained in this Agreement or in the Lease itself, or unless terminated at such time as the final phase of the Trail is completed, as agreed to by the parties.

Payments under the Lease will replace payments due under this Agreement after the first phase of the Trail is completed, as agreed to by the parties signified by execution of the Lease.

10. Assignability: The parties shall not assign, transfer, or sublet the privileges or duties of this Agreement without the prior consent in writing from the other party, which consent shall not be unreasonably withheld.
11. Successors and Assigns: Subject to provision of Section 10 hereof, the Agreement shall extend to and include the successors and assigns of the parties hereto.
12. Other Permits and Uses: Nothing herein shall be construed to affect in any manner any rights or privileges previously conferred by FPC, by contract or otherwise, to others to use the FPC owned properties described by this Agreement, and FPC shall have the right to continue and extend such rights and privileges. Upon request by County, FPC shall provide copies of all such contracts and arrangements. The privileges herein granted shall at all times be subject to such existing contracts and arrangements, including any renewals or extension thereof. The privileges herein granted shall be non-exclusive, and FPC shall have the right to grant privileges to any person, firm or corporation, provided such grant shall not unreasonably interfere with County's utilization of the FPC properties. In the event of closure of the Trail Utilization area by FPC, County shall be notified in advance.

In the event that a permittee of FPC needs to work on their facilities near or within the Trail Utilization area, permittee will contact County in advance. Failure of permittee to do so will not incur any liability for FPC.

13. Required Permits, Licenses or Easements: County acknowledges that the Trail Utilization Area contains property or lands in which persons other than FPC may own or control an interest and over which FPC may not have lawful authority to permit the construction and maintenance of the Trail. County shall take all necessary steps to secure and maintain legally sufficient easements, permits, licenses or releases, from all holders of interests in such property. Upon reasonable request by FPC, County shall provide to FPC copies of any and all such easements, permits, licenses or releases authorizing County's use of the Trail Utilization Area. County shall secure and maintain legally sufficient easements, permits or licenses from all owners of property upon which the Trail Utilization Area or Approved Temporary Construction Area may be located as necessary to survey, construct, operate and maintain the Trail.

County shall comply with all applicable federal, state and local laws, rules and regulations (including FPC Trail Collocation Guidelines dated July 1, 1998, and FPC Specific Requirements described in Exhibit "B" attached hereto) with respect to the use of any FPC easement or fee owned properties. FPC shall secure and maintain all permits, licenses and/or approvals from all federal, state and local entities or agencies necessary to the construction operation and maintenance of the

Trail. County agrees to cooperate with FPC in any application process for such necessary permits licenses and/or approvals. FPC shall assign all applicable permits, licenses and/or approvals to County at the completion of each phase and execution of each Lease amendment.

County shall make special note of guidelines involving FPC oil pipeline as described in Exhibit "C" and comply fully with said guidelines.

14. Non-Waiver of Terms: Failure to enforce or insist upon compliance with any of the terms or conditions of this Agreement shall not constitute a general waiver or relinquishment of any such terms or conditions, but the same shall be and remain at all times in full force and effect.
15. Governing Law: This Agreement and each of its provisions shall be governed by and construed in accordance with the laws of the State of Florida.
16. Severability: The provisions of this Agreement are intended to be severable, and the invalidity or unenforceability of any provision shall in no manner affect the validity or enforceability of any other provision.
17. Notices: Any notice or demand required under this Agreement by law, shall be in writing and shall be deemed to have been delivered upon hand delivery or five (5) days following the mailing of such notice by regular mail and addressed to the following:

Florida Power Corporation
Attn: Property Records and Data Mgmt
2600 Lake Lucien Drive, Suite 400
Maitland, FL 32751

Pinellas County
Attn: Office of the County Administrator
315 Court Street
Clearwater, FL 33756

IN WITNESS WHEREOF, the parties have caused this Agreement to Lease to be executed the day and year first above written.

Debra M. Oliveira
Witness

John A. West
Witness

FLORIDA POWER CORPORATION


By [Signature]
Its: Vice President



ATTEST:

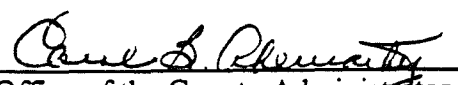
PINELLAS COUNTY, FLORIDA, by and
Through its Board of County Commissioners

KARLEEN F. DeBLAKER, CLERK

By: 
Deputy Clerk
(seal)

By: 
Chairman

APPROVED AS TO FORM:


Office of the County Administrator
Attorney

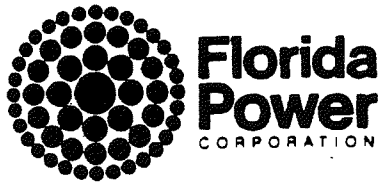


EXHIBIT B

FLORIDA POWER CORPORATION RECREATIONAL TRAIL COLLOCATION GUIDELINES JULY 1, 1998

1. OBJECTIVE

The purpose of these guidelines is to identify minimum requirements for the potential collocation of recreational trail facilities located adjacent to or inside of Florida Power Corporation Substation facilities, transmission corridors or installations on vacant lands. All design and construction approval must be secured through the Right-of-Way Utilization Committee.

2. LOCATION GUIDELINES

- A. Certified drawing plans of all the proposed trail facilities shall be submitted for review to FPC prior to any construction.
- B. Copies of site plans, permits, etc. shall be submitted for review and approval to FPC.
- C. No excavation to take place without prior approval from FPC. Identification of underground facilities will be required prior to construction.
- D. All future trail related additions or modifications shall comply with the requirements of these guidelines.
- E. No conflicts with future Florida Power Corporation system expansion plans on the right-of-way.
- F. No fences are allowed without prior consent by Florida Power Corporation.

3. DESIGN GUIDELINES

Each applicant for trails will furnish Florida Power Corporation detailed information showing that they meet the required criteria prior to construction. This detailed information must account for existing facilities as well as future facilities.

- A. Trail installation design shall meet the safety requirements of the NEC and the latest edition of the IEEE Standard 80.
- B. Trail design shall meet the appropriate Federal, State and local codes and requirements.

4. CONSTRUCTION GUIDELINES

- A. The trail applicant shall provide FPC with the proposed construction schedule one (1) month prior to construction.
- B. The trail applicant shall provide a designated person to be the liaison between the trail applicant and FPC to coordinate all construction activities.
- C. Trail applicant and applicant's representatives must adhere to the following codes and requirements:
 - National Electrical Code - NEC
 - Occupational Safety and Health Administration - OSHA
 - Federal, State and Local Requirements
- D. Trail applicant shall secure all necessary permits or licenses from Federal, State or Local Government and must provide copies to Florida Power Corporation prior to construction.
- E. Notify Florida Power Corporation Right-of-Way Inspector at (407) 475-2223 seven (7) days prior to construction.
- F. When working adjacent to energized facilities trail applicant or applicant's representatives electrical safety inspector is required at the site until installation is complete. A full safety program is required to inform and train workers concerning work near electrical facilities, including signs posted along the trail utilization area.
- G. During construction, if 15V induced voltage is exceeded adequate protection or procedures will be utilized. Temporary gradient control mats or grids must extend beyond the work area.
- H. All metallic structures must be grounded and rubber tire vehicles will be grounded by straps and by rod, if stationary. Must bond vehicles before and during fueling.
- I. Must cease work during high winds, storms and pre-arranged switching and rubber protection will be worn where required. Gradient control wires/mats will be installed, as needed.
- J. Lift equipment to comply with NESC clearances.
- K. Trail applicant or applicant's representative shall not use, store, dispose, treat or generate any hazardous substances in Florida Power Corporation's utilization area.
- L. Ground elevation must not be changed more than two (2') feet from existing grade. Trail applicant is responsible for restoration of the utilization area to equal or better condition as prior to construction.
- M. Notification of completion of construction on the designated utilization area must be made to the transmission right-of-way inspector at (407) 475-2223 within thirty (30) days. Following completion, a complete set of as-built drawings, locating and describing towers sealed by a registered engineer will be submitted to Florida Power Corporation within one hundred eighty (180) days.
- N. A Florida Power Corporation construction inspector and BA pipeline inspector will be assigned to verify terms of the agreement and to coordinate requirements by Florida Power Corporation to adjust and modify facilities to allow construction of the trail facilities. All costs are to be paid by the trail applicant. The construction manager (or designated representative) will make reports to the Transmission Right-of-Way inspector, Jim Talbot at (407) 475-2223 regarding any damages to Florida Power Corporations' property, private or public facilities. Trail applicant agrees to pay all expenses incurred while repairing such damages.

5. OPERATION/MAINTENANCE

- A. The grounds associated with the trail facility shall be maintained by the trail applicant. The trail applicant shall contract with FPC or FPC approved contractors for all maintenance or modification activities involving collocations with utility poles that have been changed out or any attachments to existing utility transmission or distribution structures. FPC will periodically update and provide the trail applicant with an approved contractors list.
- B. The trail applicant further hereby agrees that if any member of the public, or any employee of the trail applicant or the Trail applicant's agents, contractors, representatives or other persons engaging in the Trail applicant's activities upon the Right-of-Way are injured, or if any property including Florida Power Corporation's or the public is damaged in the course of work being performed under the provisions of the Agreement, the Trail applicant shall first notify Jim Talbot at (407) 475-2223 and if unable to reach, contact Florida Power Corporation's Claims Department at (813) 826-4373 during regular business hours from 8 a.m. to 5 p.m., Monday through Friday, or (813) 826-4001 at night, weekends or holidays. Such notification shall be made immediately upon knowledge, in person or by telephone and promptly confirmed in writing within twenty-four (24) hours and shall include all pertinent data such as name of injured party, location of accident, description of accident, nature of injuries, names of witnesses, disposition of injured or deceased person.
- C. Personnel entering FPC electrical facilities need to be trained in the safety requirements associated with being inside such facilities.
- D. Within one hundred eight (180) days of completion of trail construction, Florida Power Corporation will require the Trail applicant to furnish one (1) set of As-Built drawings.
- E. The trail applicant will provide signage at trail head and crossovers to remind users of electrical safety rules when using the trail. Signs to be approved by Florida Power Corporation prior to installation.

6) SPECIFIC REQUIREMENTS

- A) That other than LESSOR's facilities, no overhead wires, poles, light standards, dumpsters, signs, trees, buildings, structures or obstacles shall be located, constructed or installed within the Trail Utilization Area without written approval by LESSOR.
- B) That any shrubbery planted by LESSEE within the Trail Utilization Area shall be of a variety not exceeding twelve feet (12') in height at maturity without written approval by LESSOR.
- C) That any sprinkling system which may be installed by LESSEE within the Trail Utilization Area shall spray no closer than twenty-five feet (25') from the nearest edge of LESSOR's structures or guying and to a maximum height of ten feet (10') without written approval by LESSOR.
- D) That any and all piping and/or culverts (cables) installed by LESSEE within the Trail Utilization Area shall have sufficient earth cover to prevent breakage due to the operation of LESSOR's vehicles and heavy equipment within the right-of-way. LESSEE shall not install the above without written approval by LESSOR.
- E) That no below ground grade drainage or facilities be installed by LESSEE within the Trail Utilization Area without LESSOR's review and written approval.
- F) That LESSEE agrees that no refueling operations take place within the right-of-way.
- G) That no parking or storage shall occur under or near LESSOR's facilities.

- H) That all parked vehicles within the Trail Utilization Area shall be motor vehicles of an operative and transient nature; mobile homes, office trailers or carriers of explosive materials are prohibited.
- I) That LESSEE agrees to furnish and install permanent markers showing the location of the underground facilities within the Trail Utilization Area. Said markers shall be LESSEE's responsibility at all times.
- J) That LESSEE shall notify LESSOR's representative of Transmission Underground (i.e., Paul T. White or his designated alternate, telephone 813/893-9240, St. Petersburg) at least forty-eight (48) hours prior to LESSEE's performing any excavation within the Trail Utilization Area in order that LESSOR may arrange for an authorized representative to be on site to assist in establishing the existing location of LESSOR's underground cable(s) and to observe all excavation activities in proximity to said cable(s) within the Trail Utilization Area.
- K) That LESSEE shall not install cathodic protective devices without first notifying LESSOR's Material Technology Engineer, (i.e., Alvan L. Hite, or his designated alternate, telephone 904/ 795-4811, Crystal River).
- L) That LESSEE shall make special note of Guidelines involving LESSOR's oil pipeline attached as Pages of and of Exhibit "B", by this reference incorporated herein and made a part hereof, and LESSEE shall comply fully with said Guidelines.
- M) That LESSEE is responsible for performing an endangered/threatened species study prior to alteration of terrain.
- N) That LESSEE shall notify LESSOR's representative for Underground Fuel Oil Pipeline (i.e., Yvonne Ponce or her designated alternate, telephone, (727) 826-4268, St. Petersburg) at least forty-eight (48) hours prior to LESSEE's performing any excavation within LESSOR's right-of-way easement strip in order that LESSOR may arrange for an authorized representative to be on site to assist in establishing the existing location of LESSOR's underground fuel oil pipeline and to observe all excavation activities in proximity to said pipeline within LESSOR's right-of-way.
- O) All mowing and maintenance in the Trail Utilization Area shall comply with all local, county, state ordinances.
- P) The LESSOR requires 24 hour a day, 7 days a week access to LESSOR's facilities. During normal maintenance activities the Lessor will provide 24 hour notice to LESSEE at _____ prior to accessing LESSOR's facilities through the Trail Utilization Area. Under emergency situations (i.e. storm damage, etc.) the LESSOR shall have immediate access without prior notification. In certain situations sections of the Trail Utilization Area may need to be temporarily closed to any and all Trail related uses.
- Q) The LESSOR shall use the Trail Utilization Area for access to LESSOR's facilities to maintain and operate LESSOR's system.
- R) LESSEE shall provide LESSOR with a contact person to coordinate Trail shutdowns when necessary.
- S) LESSOR shall keep all closures of the Trail Utilization Area to the minimum possible interference.
- T) Buffer

Exhibit C

GUIDELINES FOR CONSTRUCTION CROSSING OF THE B/A OIL PIPELINE

The Florida Power Corporation-Bartow to Anclote Thermal Insulated oil transport line has been installed and is operated in compliance with Title 49-Part 195 Federal D.O.T. - Transportation of Liquids by pipeline. The line is 14" in diameter coated and insulated for a total outside diameter of 17.5".

1. Buried Structures - Crossing Situations

The minimum clearances required for crossing situations of the B/A oil line and proposed other lines or structures follows:

- a. Maintain 12 inches of clearance between the oil line and other pipes or structures being buried and crossing the line.
- b. When 12 inches of clearance is not practical; provisions can be negotiated with Florida Power Corporation for a substitution of the soil barrier with an approved barrier.
- c. Contact Carney Cameron, (727) 827-8247 or Yvonne Pence, (727) 825-4288 48 hours prior to construction. A representative of FPC must be present to inspect any crossings that are made.

2. Buried Structures - Paralleling Structures

There are two standards of acceptability for paralleling situations:

- a. Florida Power Corporation R/W, easements and other controlled areas.
 1. All paralleling of buried structures and other pipes to the B/A oil line must maintain a minimum of three (3) feet surface to surface clearance.
- b. All paralleling situations outside the control of the Florida Power Corporation should request the minimum three (3) feet surface to surface clearance.

3. Minimum Soil Cover Over B/A Oil Line

The minimum soil cover requirements at any location along this Bartow to Anclote oil pipe line has been established to be 48 inches, as referenced from top of the pipe to ground level, road bed, river bottom, drain ditch bottom, etc. Any variance in this minimum protective soil cover must be negotiated with and approved by the Florida Power Corporation. A substitute, protective, equivalent concrete structure is shown on attached Drawings #A-S-34-A-1, and AB-P2-A. Florida Power Corporation must be contacted to determine the appropriate concrete structure.

4. Roadways

Roadways of the oil pipeline must be cased and vented from R/W line to R/W line. Driveways which can be closed to allow for maintenance on the pipeline will not normally be required to provide casing, but will be required to comply with item 3 above.

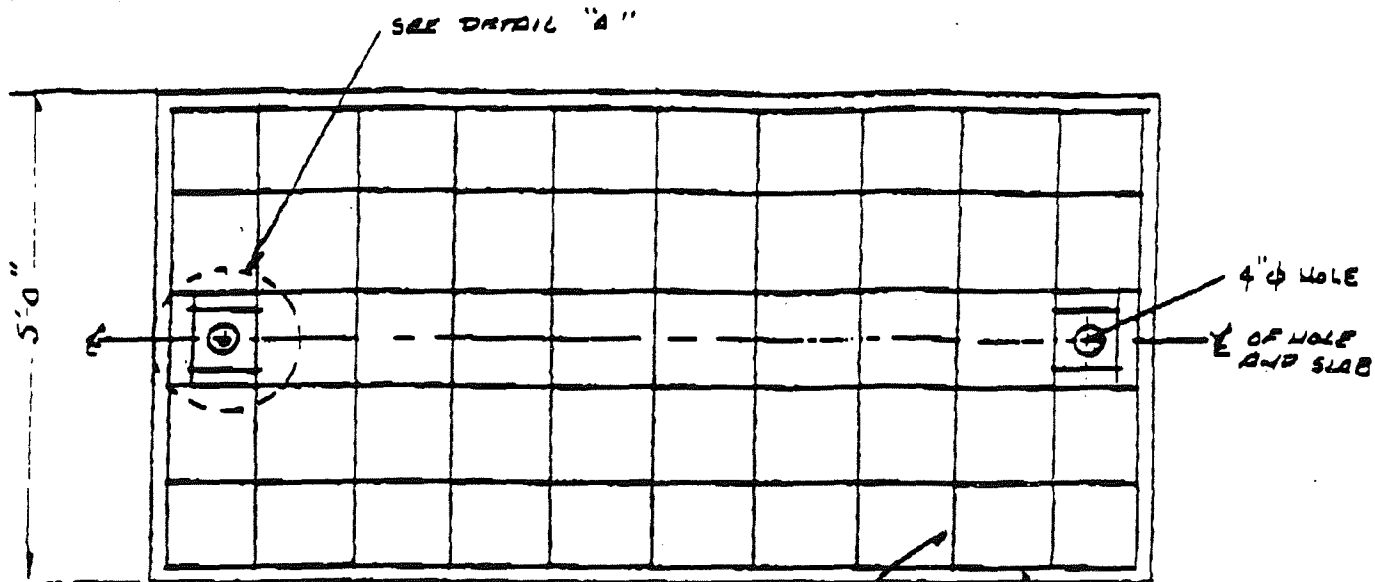
Note: Actual casing requirements will be determined by conditions at location of the crossing.

5. Building, Structures, Retention Ponds, Wells, Fences

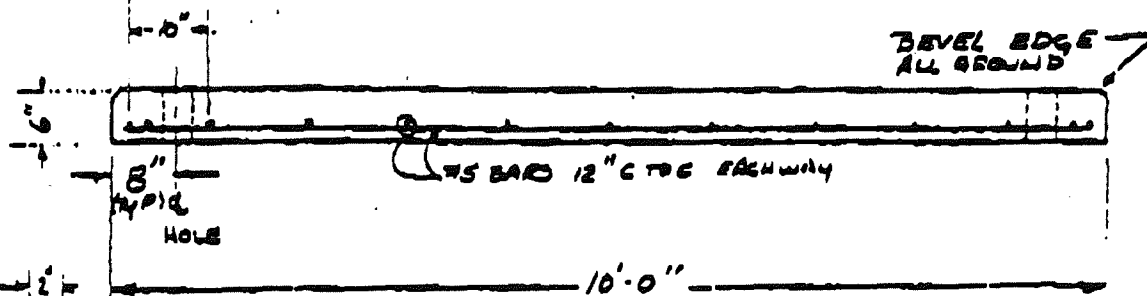
- a. All structures, building retention ponds or wells, must maintain a minimum of fifteen (15) feet from B/A oil pipeline centerline. If existing foundation, retention ponds or wells are closer than fifteen (15) feet, designate them as existing and submit information to FPC on design drawings. For retention ponds, the fifteen- (15) feet requirement applies from the top of bank to centerline of pipe.
- b. Fencing should be constructed in a way that poles straddle either side of line. The fence shall be grounded. Grounding rods shall be 15 feet away from the B/A oil pipeline.

6. Construction Equipment

Equipment or material used in construction activities must not be parked or stored within ten (10) feet of the B/A oil pipeline centerline.



• PLAN •
SCALE: 1" = 2'-0"



• ELEVATION •
SCALE: 1" = 2'-0"

NOTES:

1. Conc. should have min. strength of 3000 P.S.I.
2. Rebar is ASTM A615 grade 40.
3. Conc. should be cast on smooth surface (Form)
4. Min. 2" conc. cover all around.

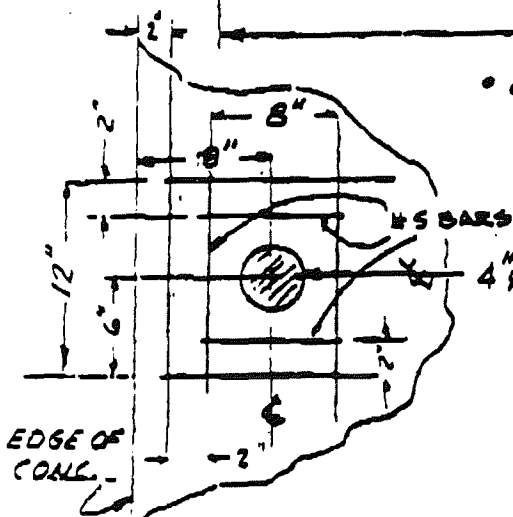
MATERIAL LIST FOR EACH SLAB

CONCRETE : 0.93 yd³

REINFORCING STEEL

- 6 - No. 5 bars at 9'-6" (total footage) = 58'-0"
- 11 - No. 5 bars at 4'-8" (total footage) = 51'-4"
- 4 - No. 5 bars at 10" (total footage) = 3'-4"
- 2 - No. 5 bars at 1'-2" (total footage) = 2'-4"

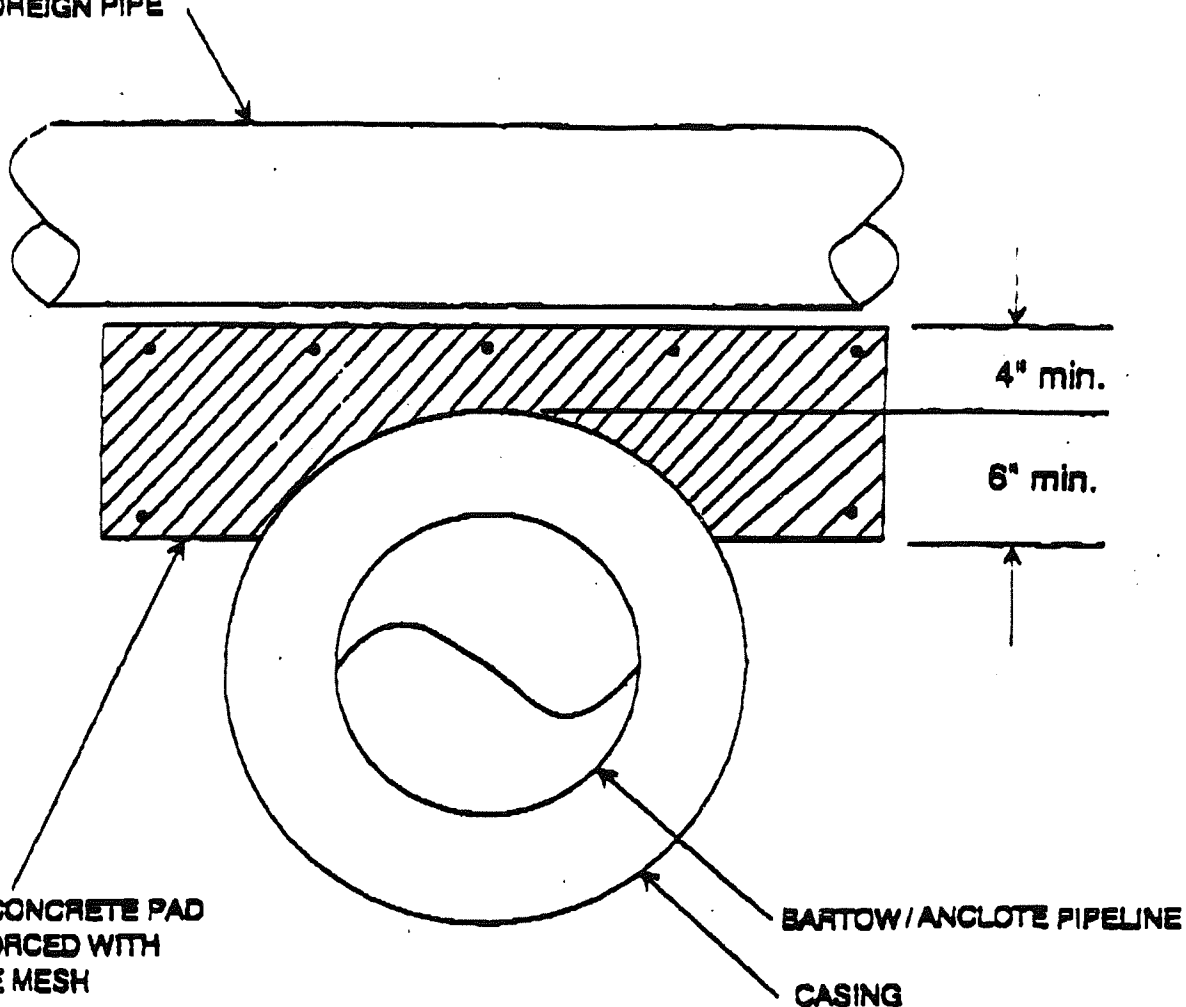
No. 5 bars total steel footage 115'-0"



DETAIL "A"
SCALE 1" = 1'-0"

CONCRETE PIPELINE COVER SLABS					
PROJECT BARTON-ANCLOTE PIPELINE					
FLORIDA POWER CORPORATION ST. PETERSBURG, FLORIDA					
NO.	DATE	REVISION	BY	CK.	APP.
324-75	ADDED MATERIAL LIST	PHS	JM	NT	
2	3/10/75	ISSUED FOR CONSTRUCTION	JM	JM	FT
DATE 3/10/75 SCALE NOTED BY JM CK JM APP OUT					
DRAWING NO. A-534-A-1					

FOREIGN PIPE

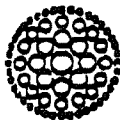


2' X 3' CONCRETE PAD
REINFORCED WITH
6" WIRE MESH

BARTOW/ANCLOTE PIPELINE

CASING

DO NOT ALLOW THE WIRE MESH TO CONTACT CASING PIPE

NO.		DESCRIPTION		DRAWN	CHKD	APPR.	DATE
1		ISSUED FOR S/A PIPELINE PROJECT		PAR	RTH	ALH	8/96
REVISIONS							
 FLORIDA POWER CORPORATION				ENERGY SUPPLY			
				BARTOW - ANCLOTE PIPELINE			
				FOREIGN LINE CROSSING ON Cased PIPE			
ALH	DRN	CHKD	NTS	7/11/96		NTS	
P		SHEET		DATE		SCALE	
1		AB-P2-A		1		4x	

FILE ABP7A DMC/CP1

VAULT CODE: A-2-20

TRAIL LEASE

This Lease is made and entered into as of the _____ day of _____, 1999, by and between Florida Power Corporation ("FPC"), as Lessor and Pinellas County, A political subdivision of the State of Florida by and through its governing body, the Board of County Commissioners ("COUNTY"), as Lessee.

WHEREAS, COUNTY intends to operate and maintain a trail ("Trail") from the west side of CR 611 south of Tampa Road and ending at San Martin Boulevard at the Weedon Island Preserve and desires to locate all or part of such Trail within electric transmission, distribution, substation, and Bartow/Anclole hot oil pipeline, easements and fee owned properties held by FPC as more specifically described in Exhibit "A" ("Trail Utilization Area"); and

WHEREAS, FPC or its subcontractors intends to survey and construct, in phases, a fifteen (15") foot wide trail, including paving and boardwalks, except for special circumstances such as crossovers or bulb outs where an additional amount of reasonable footage may be allocated after such design is approved by FPC and COUNTY engineering. Limited landscaping in the area described in Exhibit A ("Trail Utilization Area") may also be allocated for at grade sodding.

WHEREAS, FPC intends to lease in phases to COUNTY each completed portion of the Trail upon the terms stated below for the purpose of making available to the public FPC's easement and fee owned properties for recreational purposes without charge to the public. FPC and the COUNTY agree that this Lease will be amended to incorporate the addition of the description of the newly constructed portions of the Trail;

WHEREAS, FPC shall permit COUNTY to operate, inspect, maintain, repair, remove and replace Trail facilities within the leased portion of the Trail Utilization Area.

NOW, THEREFORE, in consideration of the mutual covenants, terms and conditions herein contained, FPC and COUNTY do hereby agree as follows:

1. RIGHT TO USE FPC PROPERTY. FPC, for annual lease payments plus the annual insurance premium and other good and valuable consideration, ("Lease Payment") hereby agrees to grant COUNTY the right to access, operate, inspect, maintain, repair, and remove the Trail within the Trail Utilization Area. The Lease Payment, due on the execution date of the Agreement to Lease shall be in the amount of One Hundred Fifty Thousand (\$150,000) dollars per year plus the annual insurance premium. Future payments shall be due annually on the above-referenced anniversary date. The base rent for each twelve (12) month period subsequent to the first complete twelve (12) month period occurring following termination of the Agreement to Lease shall be computed by multiplying the current base rent by a fraction whose numerator shall be the Consumer Price Index (All Urban Consumers - All items) issued by the United States Bureau of Labor Statistics of the United States Department of Labor for

the third (3rd) month prior to the respective anniversary dates in the current year, and whose denominator shall be said Consumer Price Index (All Urban Consumers - All items) for the third (3rd) month prior to the respective anniversary date in the prior year. In no event, however, shall the annual rental for any lease year during the lease term be decreased by the current annual rental rate by the foregoing computation. The percentage of any increase is subject to a minimum of two (2%) percent and a maximum of five (5%) percent per year. If publication of the Consumer Price Index is discontinued, the most nearly comparable successor index shall be used. The annual rental for each lease year determined by the foregoing computation shall be due and payable on the anniversary date of the first lease payment.

The as built legal description for the area to be used for the Trail will be established after each phase of the Trail is completed, and shall be incorporated into this Lease by way of amended Exhibit "A". COUNTY further agrees to cooperate regarding relocation of the Trail Utilization Area such that within the first ten (10) years, FPC will pay for relocation expenses, and thereafter, the expense for relocation will be paid for by the COUNTY.

The right to use the Trail Utilization Area includes permission to use adjacent portions of the FPC easement and fee properties identified by FPC as temporary workspace and for ingress and egress to the Trail Utilization Area as necessary for operation, maintenance, repair and replacement of the Trail during the term of this Lease. COUNTY acknowledges that the use of the FPC properties herein granted by FPC is non-exclusive. FPC and others so authorized by FPC may occupy, traverse or otherwise utilize any portion of the FPC properties, including that portion designated herein as the Trail Utilization Area, provided that FPC and/or others authorized by FPC to utilize such FPC properties shall comply with all applicable laws, rules and regulations and shall not unreasonably interfere with COUNTY's occupation and utilization of the Trail Utilization Area, operations or safety of its facilities, and shall promptly repair any damage to the facilities.

2. TERM. This Lease shall become effective upon its execution by the parties. Utilization of the Trail Utilization Area will only commence following completion by FPC or its subcontractors of the first phase of the Trail, and the acceptance of such phase by the COUNTY. Unless terminated or amended in accordance with any of the terms of this Lease, the Lease shall continue in effect for a term of ninety-nine (99) years.

3. REQUIRED PERMITS, LICENSES OR EASEMENTS. COUNTY acknowledges that the Trail Utilization Area contains property or lands in which persons other than FPC may own or control an interest and over which FPC may not have lawful authority to permit the construction and maintenance of the Trail. Upon reasonable request by FPC, COUNTY shall provide to FPC copies of any and all such easements, permits, licenses or releases authorizing COUNTY's use of the Trail Utilization Area. COUNTY shall maintain legally sufficient easements, permits or licenses from all owners of property upon which the Trail Utilization Area may be located as necessary to operate and maintain the Trail.

COUNTY shall comply with all applicable federal, state and local laws, rules and regulations (including FPC Trail Collocation Guidelines dated July 1, 1998, and FPC Specific Requirements described in Exhibit "B" attached hereto) with respect to the use of any FPC easement or fee owned properties. COUNTY shall maintain all permits, licenses and/or approvals from all federal, state and local entities or agencies necessary to the operation and maintenance of the Trail.

COUNTY shall make special note of guidelines involving FPC oil pipeline as described in Exhibit "C" and comply fully with said guidelines.

4. DAMAGE AND REPORTS. Each party shall exercise all reasonable precautions to avoid damage to the FPC easements and fee owned properties, the Trail and the facilities of FPC and of others located on the FPC easements and fee owned properties. COUNTY or FPC, shall immediately report to other Party's nearest local office the occurrence of any such damage to FPC's or COUNTY's facilities. COUNTY shall reimburse FPC for all expenses incurred in making repairs to FPC facilities or properties for losses arising out of the negligence of the COUNTY. FPC shall reimburse COUNTY for all expenses incurred in making repairs to COUNTY facilities or properties for losses arising out of the negligence of FPC.

5. LIMITATIONS OF LIABILITY. To the extent permitted by Florida law, the County and FPC mutually covenant not to undertake any lawsuit against the other for any liability arising out of this agreement that is not covered by the insurance policy or exceeds the limits of the insurance policy.

6. THIRD PARTY LIABILITY. Each Party claims statutory limitations on liability to third parties, as follows:

(A) FPC, as fee owner, asserts that it owes no duty of care to third parties arising out of or in connection with the construction, maintenance, or use of the Trail or ownership of the property, pursuant to F.S. 375.251.

(B) To the extent permitted by Florida law, County affirmatively agrees not to undertake any action or allow any action to be taken that will negate F.S. 375.251.

(C) In the event that F.S. 375.251 is revised, repealed or found to be inapplicable to this situation by a court of competent jurisdiction, FPC retains the right to renegotiate or terminate the contract.

(D) County, as a subdivision of the state, asserts that it is entitled to the limitations contained in F.S. 768.28.

7. INSURANCE. FPC shall procure a General Liability insurance policy on behalf of COUNTY with FPC as an additional insured. The General Liability insurance policy shall be in the amount of Fifty (50) Million dollars, as described in Exhibit "D" and shall be effective upon the execution of the Trail Lease. The limit on this policy shall be reviewed every five (5) years by FPC and COUNTY to determine if the limits are appropriate. FPC and COUNTY shall mutually agree to increase the policy limit if deemed insufficient. FPC shall

maintain all insurance policy premiums up to date and shall provide COUNTY with sixty (60) days notice of termination of policy, material change in terms, or cancellation of the policy, which may be grounds for termination of the Lease.

8. SIGNAGE. COUNTY shall be responsible to print, install and maintain signage within the Trail Utilization Area. The signage will contain language mutually developed by FPC and COUNTY. The signage shall be located every one-quarter (1/4) of a mile along side the Trail and at every road crossing. Additionally, the COUNTY shall provide to the public educational brochures about the Trail. The language for these brochures will be a collaboration between the COUNTY and FPC. In the event of closure of the Trail Utilization area by FPC, COUNTY shall be notified in advance, and FPC shall provide appropriate signage and detours for the affected area.

9. TERMINATION. If COUNTY materially fails to comply with any of the provisions of this Lease or defaults in any of its obligations hereunder, and fails within sixty (60) days after written notice from FPC to correct such default or noncompliance, FPC may at its option correct any such defective performance hereunder and invoice COUNTY for all expenses incurred by FPC in such correction, or terminate this Lease. In the event that COUNTY has notified FPC in writing that correction of such default or noncompliance cannot reasonably be accomplished within such sixty (60) day period, and COUNTY demonstrates to FPC's reasonable satisfaction that COUNTY is diligently completing correction of such default or noncompliance, FPC may extend such sixty (60) day period.

If FPC materially fails to comply with any of the provisions of this Lease or defaults in any of its obligations hereunder, and fails within sixty (60) days after written notice from COUNTY to correct such default or non-compliance, COUNTY may at its option correct any such defective performance hereunder and invoice FPC for all expenses incurred by COUNTY in such correction, or terminate this Lease. In the event that FPC has notified COUNTY in writing that correction of such default or noncompliance cannot reasonably be accomplished within such sixty (60) day period, and FPC demonstrates to COUNTY's reasonable satisfaction that FPC is diligently completing correction of such default or non-compliance, COUNTY may extend such sixty (60) day period.

If the Utility Industry undergoes changes such that FPC decides at some future date to sell it's fee owned and easement interests, FPC may terminate this Lease. COUNTY would be afforded right of first refusal to purchase the parcels covered by this Lease that FPC would make available for sale.

FPC fee owned property is subject to a lien under the indenture. FPC may terminate this Lease if it is in default under the indenture.

The COUNTY may terminate this Lease upon a decision by future Pinellas Board of County Commissioners to discontinue the operation of the Trail, or in the event that funds are not budgeted to fund this Lease subject to Florida Statute 129.07, 129.08, and 129.09.

Upon termination of this Lease, or after approval authority to abandon the Trail has been granted by the Pinellas Board of County Commissioners, COUNTY, at COUNTY's sole expense, shall cease use of and remove the Trail from the FPC easements and fee owned properties in a manner consistent with all applicable federal, state and local laws and regulations. Concurrently with said removal, COUNTY at COUNTY's sole expense, shall restore the affected portion of the FPC properties to a condition reasonably identical to that which existed prior to COUNTY's utilization of FPC owned properties. If COUNTY fails to remove the Trail and/or restore the FPC properties, and then FPC shall have the right to remove all or portions of such Trail and/or restore the FPC right-of-way at COUNTY's expense and without any liability for such removal or restoration. In the event this Lease is terminated under this Section, COUNTY shall not be entitled to any refund of any amounts paid or due to FPC by COUNTY pursuant to this Lease.

10. NON-WAIVER OF TERMS. Failure to enforce or insist upon compliance with any of the terms or conditions of this Lease shall not constitute a general waiver or relinquishment of any such terms or conditions, but the same shall be and remain at all times in full force and effect.

11. OTHER PERMITS AND USES. Nothing herein shall be construed to affect in any manner any rights or privileges previously conferred by FPC, by contract or otherwise, to others to use the FPC owned properties described by this Lease, and FPC shall have the right to continue and extend such rights and privileges. Upon request by COUNTY, FPC shall provide copies of all such contracts and arrangements. The privileges herein granted shall at all times be subject to such existing contracts and arrangements, including any renewals or extension thereof. The privileges herein granted shall be non-exclusive, and FPC shall have the right to grant privileges to any person, firm or corporation, provided such grant shall not unreasonably interfere with COUNTY's utilization of the FPC properties or COUNTY's safe operation and maintenance of the Trail under this Lease.

12. ASSIGNABILITY. COUNTY shall not assign, transfer or sublet the privileges hereby granted without the prior consent in writing from FPC, which consent shall not be unreasonably withheld.

13. SUCCESSORS AND ASSIGNS. Subject to the provision of Section 12 hereof, the Lease shall extend to and in the successors and assigns of the parties hereto.

14. GOVERNING LAW. This Lease and each of its provisions shall be governed by and construed in accordance with the laws of the State of Florida.

15. SEVERABILITY. The provisions of this Lease are intended to be severable, and the invalidity or unenforceability of any provision shall in no manner effect the validity or enforceability of any other provision.

16. ACKNOWLEDGMENT OF ENCUMBRANCE. Uses allowed herein constitute an encumbrance on FPC's easements and fee owned properties and the stated consideration

compensates for that encumbrance, as well as for additional benefits realized by COUNTY in occupying said properties.

17. NOTICES. Any notice or demand required under this Lease or by law, shall be in writing and shall be deemed to have been delivered upon hand delivery or five (5) days following the mailing of such notice by Certified Mail, and addressed to the following:

Florida Power Corporation
Attn: Property Records and Data Management
2600 Lake Lucien Drive, Suite 400
Maitland, FL 32751

Pinellas County
Attn: Office of the County Administrator
315 Court Street
Clearwater, FL 33756

IN WITNESS WHEREOF, the parties have caused this Lease to be executed the day and year first above written.

"LESSOR"

FLORIDA POWER CORPORATION

Witness

Print/Type Witness Name

Witness

Print/Type Witness Name

By:

Attn: Property Records and Data Management
2600 Lake Lucien Drive, Suite 400
Maitland, FL 32751

"LESSEE"

**PINELLAS COUNTY, FLORIDA, by and
Through its Board of County Commissioners**

KARLEEN F. DeBLAKER, CLERK

By: _____
Deputy Clerk

(seal)

By: _____
Chairman

APPROVED AS TO FORM:

Office of the County Administrator

Witness

Print/Type Witness Name

Witness

Print/Type Witness Name

STATE OF FLORIDA
COUNTY OF

The following instrument was acknowledged before me this _____ day of _____, 1999, by _____, acting as _____ for and on behalf of Florida Power Corporation. He/She is personally known _____ or produced identification _____. Type of identification produced _____.

(SEAL)

Notary Public, State of Florida

Print/Type Notary Name

Commission Number: _____

My Commission Expires: _____

STATE OF FLORIDA
COUNTY OF PINELLAS

The following instrument was acknowledged before me this _____ day of _____, 1999, by _____, acting as _____ for and on behalf of Pinellas County, a political subdivision of the State of Florida by and through its governing body, the Board of County Commissioners. He/She is personally known _____ or produced identification _____. Type of identification produced _____.

(SEAL)

Notary Public, State of Florida

Print/Type Notary Name

Commission Number: _____

My Commission Expires: _____

JOINT PARTNERSHIP AGREEMENT FOR CONSTRUCTION OF THE PINELLAS
RECREATIONAL TRAIL EXTENSION ON FLORIDA POWER CORPORATION
RIGHT OF WAY

THIS JOINT PARTNERSHIP AGREEMENT, entered into this 3 day of Dec.,
1999, by and between PINELLAS COUNTY, FLORIDA, a charter County, hereinafter referred to
as the COUNTY, and Florida Power Corporation, a Florida Corporation, hereinafter referred to as
FPC.

WITNESSETH, That:

WHEREAS, the COUNTY has intended to construct a recreational trail on or near FPC, right
of way from East Lake Road, the southern terminus of the existing Trail near John Chestnut, Sr.
Park, along the west side of U.S. 19 south, and then east to Weedon Island in the Gateway area,
hereinafter referred to as the PROJECT, and

WHEREAS, the FPC wants to expedite the PROJECT and requests to assist the COUNTY
by designing and causing it to be surveyed and constructed, and

WHEREAS, the COUNTY and FPC have determined that it would be in the best interest of
the general public and to the economic advantage of both parties to coordinate and cooperate in their
efforts to facilitate development of the PROJECT.

NOW THEREFORE, the COUNTY and FPC, in consideration of the mutual promises herein
contained, and for other good and valuable consideration, receipt of which is hereby acknowledged
by all parties, it is hereby agreed by and between the parties as follows:

SECTION 1 THE PROJECT

1.1 FPC will survey, design, construct and inspect the Trail on or near FPC right of way, from East Lake Road, the southern terminus of the existing Trail near John Chestnut, Sr. Park, along the west side of U.S. 19 south, and then east to Weedon Island in the Gateway area, as described in the Concept Plan dated November 1, 1999, attached and identified as Exhibit A, hereafter known as the PROJECT. The services to be provided by FPC for this PROJECT shall include but not be limited to the survey, master plan, conceptual design, final design, preparation of construction plans and specifications, applications for federal, state, and local permits and the construction of the PROJECT. Preparation of construction plans and specifications shall include but not be limited to pavement, structures, bridges, drainage structures, wetland mitigation facilities, stormwater treatment/attenuation facilities, erosion protection, and maintenance of traffic. Project management, contract administration, and construction engineering and inspection will be performed by FPC personnel. Survey, design, and construction will primarily be contracted out to private parties

1.2 The COUNTY shall participate in design reviews and pre-construction meetings for the PROJECT. The COUNTY may provide input as requested by FPC during contractor selection and during plan development for conformance of plans and specifications to COUNTY standards. FPC standard contractor selection procedures shall be utilized.

SECTION 2 PROJECT FUNDING

2.1 FPC will advance payment for all costs related to the survey, design, construction, and inspection of the PROJECT. The COUNTY will reimburse FPC for all costs related to the survey, design, construction, and inspection of the PROJECT as described below, up to an amount not to exceed \$15,000,000. In the event that actual or anticipated costs appear to exceed \$15,000,000, the parties may agree to reduce the scope of the project or amend this agreement to increase the spending limit.

2.1.1 Should the relocation or modification of existing FPC regulated facilities be required, a separate Authorization Agreement with FPC will be required and the charges for this work will be billed in accordance with this FPC standard Authorization Agreement procedures. Current version of which is incorporated by reference.

2.1.2 The County will reimburse FPC for the actual cost of the professional services contracts such as: survey and design plus 15 percent for FPC.

2.1.3 The COUNTY shall reimburse FPC for the actual salary times 2.6 times the actual hours charged to the project by FPC personnel for design and construction engineering and inspection.

2.1.4 The COUNTY shall reimburse FPC for the actual construction cost of the PROJECT plus a 10 percent surcharge for contract administration.

2.2 Upon FPC's final payment to the Contractor for construction of the PROJECT, FPC shall within three hundred sixty (360) days have its final and complete billing delivered to the COUNTY of all costs incurred in connection with the work performed hereunder. All cost records and accounts shall be subject to audit by a representative of the COUNTY for a period of three (3) years after the Board's final approval and acceptance of the PROJECT.

2.3 The COUNTY retains the right to inspect all work to verify compliance with the Contract Documents. The COUNTY may appoint such assistants and representatives as desired to be designed as their inspectors. They shall be authorized to inspect all work done and all materials furnished. This right of inspection in no way means or implies COUNTY control or other supervision over the work done or the work site. This right is solely for the COUNTY'S benefit and in no way imposes any duties or responsibilities on the COUNTY and confers no rights on any other parties. Such inspection may extend to all or any part of the work and to the manufacture, preparation or fabrication of the materials used. Such inspectors shall not be authorized to revoke, alter, or waive any requirements of the Contract Documents.

SECTION 3 PAYMENT SCHEDULE

3.1 FPC shall invoice the COUNTY on a monthly basis for reimbursement of costs outlined in Section 2.1 during performance of the services. During survey and design invoices shall be prepared by FPC and sent to the COUNTY for verification and payment. During construction, estimates of the work completed will be prepared monthly by the FPC inspector. Invoices for construction, engineering and inspection shall be prepared by FPC and sent to COUNTY for verification and payment on a monthly basis. The Contractor's project representative will be required to review, and approve by signature, these estimates with the FPC inspector. Copies of approved Pay Requests shall be attached as back up for the request.

3.2 The COUNTY shall make payments to FPC for invoiced work in accordance with § 218.70, et. Seq., the Florida Prompt Payment Act. Both parties agree that the authority to resolve all disputes shall rest solely with the FPC Bulk Power Services Manager of Contracts and Proposals.

2.3 All invoices and notices to the COUNTY shall be sent to:

Mr. Jerry Herron, Fiscal Manager
Pinellas County Public Works
440 Court Street, Fourth Floor
Clearwater, Florida 33756

2.4 All payments and notices to FPC shall be sent to:

Mr. Ken Lord, Manager, Contracts and Proposals
Florida Power Corporation
Bulk Power Services
2600 Lake Lucien Drive, Suite 400
Maitland, Florida 32751

SECTION 4 TRANSFER OF RESPONSIBILITY

Upon acceptance of the work by FPC, the PROJECT will be tendered to the COUNTY. Upon acceptance by the COUNTY, it shall assume operation and maintenance responsibility for the PROJECT. Lease acceptance by the COUNTY shall be evidenced by execution of a lease or a lease amendment.

SECTION 5 ADDITIONAL SERVICE

The COUNTY or FPC shall not enter into ADDITIONAL SERVICES that would require COUNTY or FPC reimbursement without advance written approval by the COUNTY and FPC.

SECTION 6 EFFECTIVE DATE

This Agreement shall be filed with the Clerk of the Circuit Court of Pinellas County after execution by the parties and shall be effective upon filing.

SECTION 7 TERMINATION OF AGREEMENT

This Agreement shall be terminated upon the earlier of mutual consent of the parties or performance of the parties obligations hereunder. The COUNTY reserves the right to terminate this Agreement upon a decision of a future Board of County Commissioners to discontinue the construction of the Trail or in the event that funds are not budgeted to fund this Agreement which is subject to Sections 129.07, 129.08, 129.09, Florida Statutes. Both parties reserve the right to terminate this Agreement upon occurrence of any event of termination under the Agreement to Lease between FPC and the COUNTY. In the event that this Agreement is terminated under the provisions herein and the PROJECT'S construction contract is terminated early, the total and complete compensation due FPC shall be agreed to by the parties based on its determination of the costs of work effort completed to date of termination, such compensation shall be paid to FPC by the COUNTY within sixty (60) days of termination.

SECTION 8
MISCELLANEOUS PROVISIONS

8.1 Any amendment to or modification of the Agreement or any alteration, extension, supplement or change of time or scope of the work shall be in writing and signed by both parties.

8.2 If any word, clause, sentence or paragraph of the Agreement is held invalid, the remainder of this Agreement would continue to conform to the intent of this Agreement.

8.3 This Agreement shall be governed and construed in accordance with the laws of the State of Florida.

8.4 Nothing herein shall be construed to create any third party beneficiary rights in any person not a party to this Agreement.

8.5 This document embodies the whole Agreement of the parties. There are no promises, terms, conditions or allegations other than those contained herein and this document shall supersede all previous communications, representations and/or agreements, whether written or verbal between the parties hereto. This Agreement shall be binding upon the parties, their successors, assigned and legal representatives. All disputes between the parties with regard to the terms and conditions of this contract shall be resolved by Mr. Ken Lord, Manager, Contracts and Proposals, Florida Power Corporation.

IN WITNESS WHEREOF, the parties hereto have caused these present to be executed by their duly authorized officers, and their official seals hereto affixed, the day and year first above written.

FLORIDA POWER CORPORATION, a
Florida Corporation

By: [Signature] 11-17-99
Vice President, Bulk Power
Delivery Date

PINELLAS COUNTY, a political
subdivision of the State of Florida

By: [Signature] 12/3/99
Chairman Date

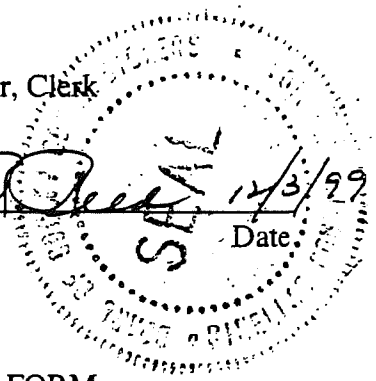
ATTEST

By: [Signature] 11-17-99
Date

ATTEST

Kathleen F. DeBlaker, Clerk

By: [Signature] 12/3/99
Deputy Clerk Date



APPROVED AS TO FORM:

By: [Signature] 11-17-99
Date

APPROVED AS TO FORM:

By: [Signature] 12/1/99
County Attorney Date

Fred E. Marquis Pinellas Trail

FLORIDA POWER RIGHT-OF-WAY



Preliminary
Engineering
Evaluation and
Master Plan

November 1, 2001

Submitted to:



Pinellas County

Submitted by:



Vanasse Hangen Brustlin, Inc.

in association with:



Cumbey & Fair, Inc. • Borrelli & Associates • Nodarse & Associates, Inc. • Rails-to-Trails Conservancy



MEMORANDUM PUBLIC WORKS ENGINEERING
TRANSPORTATION DIVISION

To: J. Keith Wicks, P.E., Public Works Director

Thru: Jan Herbst, P.E., Director of Engineering
Jim Collins, P.E., Division Engineer
Ivan Fernandez, P.E., Senior Engineer

From: Joe DeMoss, PWESII, Project Manager

Distr: Debra Ashman, PWESII, Parks and Structures
Jerry Cummings, Pinellas Trail Supervisor
Robert Davis, PWESI, Transportation Planning
Lee Marsh, Highway Division, Registered Landscape Architect
Deborah Chayet, Park Horticulturist
Donnie Miller, Florida Power Project Manager

Subject: Pinellas County Staff Addendum to
Fred Marquis Pinellas Trail – Extension on Florida Power Right Of Way
Master Plan dated November 1, 2001

Date: August 6, 2002

Pinellas County Staff, consisting of representatives from Public Works Transportation Division, Public Works Transportation Planning Division, Public Works Park and Structures Division, Public Works Highway (Landscape Architect), and the Parks Department (Trail Supervisor and Park Horticulturist), have reviewed the aforementioned Master Plan prepared by the Consulting firm of Vanasse Hangen Brustlin, Inc. for Florida Power. The reviewers recommend that the Master Plan be used as a design concept for the project with the following Addendum:

- The identification of traffic lights and other traffic control measures require a detailed traffic analysis to be completed during the design stage. The empirical data collected will be used to determine appropriate traffic control measures. All traffic control measures shall have the approval of Pinellas County and the municipality having jurisdiction over such devices.
- The use of bollards, guardrails, concrete walls, fences and other such barriers constructed and reimbursed under the terms of the Joint Partnership Agreement shall require approval by Pinellas County prior to its implementation, in accordance with County policies, practices, standards, and Ordinances.
- The proposed landscape designs are theoretical proposals to gauge relative cost, should the County decide at some future date to provide landscape enhancement.
- The Rural, Wetlands, Green Space with Trees, Green Space without Trees, and the Green Space with Water zones should not be considered for landscape enhancement. These are natural areas which generally do not need improvement and also afford the trail user a variety of outdoor natural environments in addition to the man made environments in the other zones.
- Installation and maintenance of landscape materials, especially watering requirements, and the installation of large trees and large plant materials, needs to be evaluated prior to implementation. The following issues should be included in the evaluation: The installation of large materials (Trees and Shrubs) require the use of large heavy equipment which may cause damage to the trail and the surrounding turf areas during the installation process as well as during the ongoing maintenance process. Additionally, these areas will not have installed irrigation capability. The watering required during the installation and the establishment period will entail the use

of heavy watering tank trucks, which likely will cause damage to the trail and the surrounding turf areas. Landscape installation should also look at areas where the adjacent landowner is willing to undertake the watering and limited maintenance of the landscape under an "adoption program".

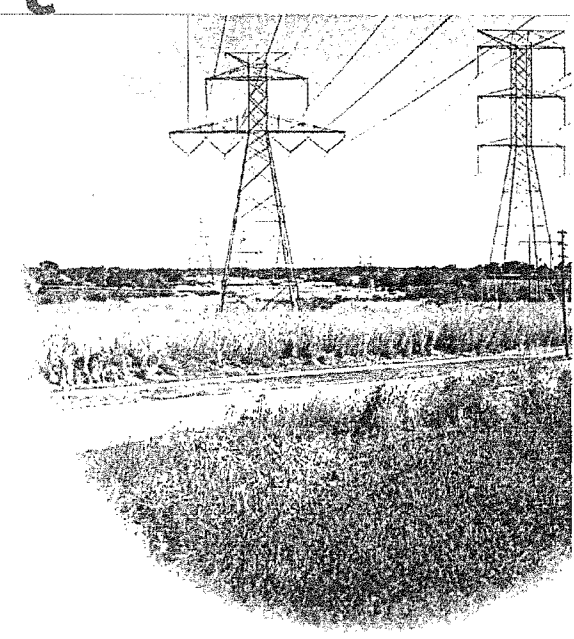
- The use of large plant materials and the close spacing of some of the materials implies an immediate finished product. A more cost-effective option should be considered prior to implementation.
- Installation of plant materials in some areas may create problems to adjacent underground or overhead utilities. Where such utilities exist, extreme care must be taken to prevent any such problems with branch intrusion or upheaval or dislocation due to growing root systems.
- The proposals shown in the Master Plan appear to present the application of a man-made environment rather than attempt to re-establish a portion of the natural environment removed by the construction of the power transmission lines or other man made improvements. The re-establishment of the natural landscaping of the area that existed prior to the installation of the electrical utility is preferred.
- Prior to implementation, the following needs to be considered: The relatively narrow right-of-way past the edge of pavement (5 feet in most cases) severely restricts, if not eliminates, the opportunity to install landscaping, as any landscaping might impinge upon a recovery zone or private property. Access for landscaping and maintenance is limited. Plant selection in the plant palette needs modification to exclude expensive exotic palm species. Additionally, several non-native species on the plant palette have shown invasive qualities and is under consideration for addition to the Exotic Pest Plant Council Invasive Plant List. B&B plant material is not recommended.
- The clear Trail width on bridges and similar structures shall conform to existing standards already established for the Fred Marquis Pinellas Trail, unless otherwise approved by the County.
- The Trail shall have a 15' wide asphalt surface with a 5' grassed buffer located on each side, unless otherwise approved by Pinellas County.
- The final location, commencement, and termination points of the Trail shall be a collaborative decision effort of Pinellas County and Florida Power, in order to provide a safe and effective facility for the public.
- The determination of funding sources to be used for the project will be determined by Pinellas County.
- Florida Power must receive written approval of the various stages of the project prior to its implementation, in accordance with County policies, standards, and practices.
- All elements of the Trail shall be designed and constructed in accordance with the policies, standards, ordinances, and practices of Pinellas County, unless otherwise approved by the County.

- DRUM -



Fred E. Marquis Pinellas Trail

EXISTING FLORIDA POWER RIGHT-OF-WAY



Preliminary
Engineering
Evaluation and
Master Plan

Donnie R. Miller
Project Leader
Transmission/Distribution Svcs.
Energy Delivery Admin. Svcs.

6565 38th Ave. North
EC 37
St. Petersburg, FL 33710

T > 727.384.7815
C > 727.580.1158
donnie.miller@pgnmail.com



ADDENDUM

Submitted to:



Submitted by:



in association with:



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Introduction

This report presents an addendum to the November 1, 2001 Fred E. Marquis Pinellas Trail Extension on Florida Power Right-of-Way Preliminary Engineering Evaluation and Master Plan. The figures and data presented on the following pages represent the current plan of action per Pinellas County directives. This information takes precedence over the data presented in the November 1, 2001 Master Plan.

Following numerous reviews of the construction and right-of-way cost estimates, the Pinellas County Department of Public Works presented the Board of County Commissioners with the option of proceeding with the Fred E. Marquis Pinellas Trail Extension on Florida Power Right-of-Way with Segments B and C only. The Board of County Commissioners has approved this alternative. The following figures, therefore, represent the project location and limits, the estimated project schedule for completion, and the schedule breakdown by cost for Segments B and C only. Please note that the data presented within these exhibits is subject to change as the project proceeds through design and into construction.

Fred E. Marquis Pinellas Trail Extension on Florida Power Right-of-Way

Process to Complete

Segment A
From: *John Chesnut, Sr. Park*
To: *US 19 at Enterprise Road*

✓ *No Activity*

Segment B
From: *US 19 at Enterprise Road*
To: *Belleair Road*

✓ *Preliminary Design & Permit*
✓ *Final Design & Construct*

Segment C
From: *Belleair Road*
To: *SR 688/Ulmerton Road*

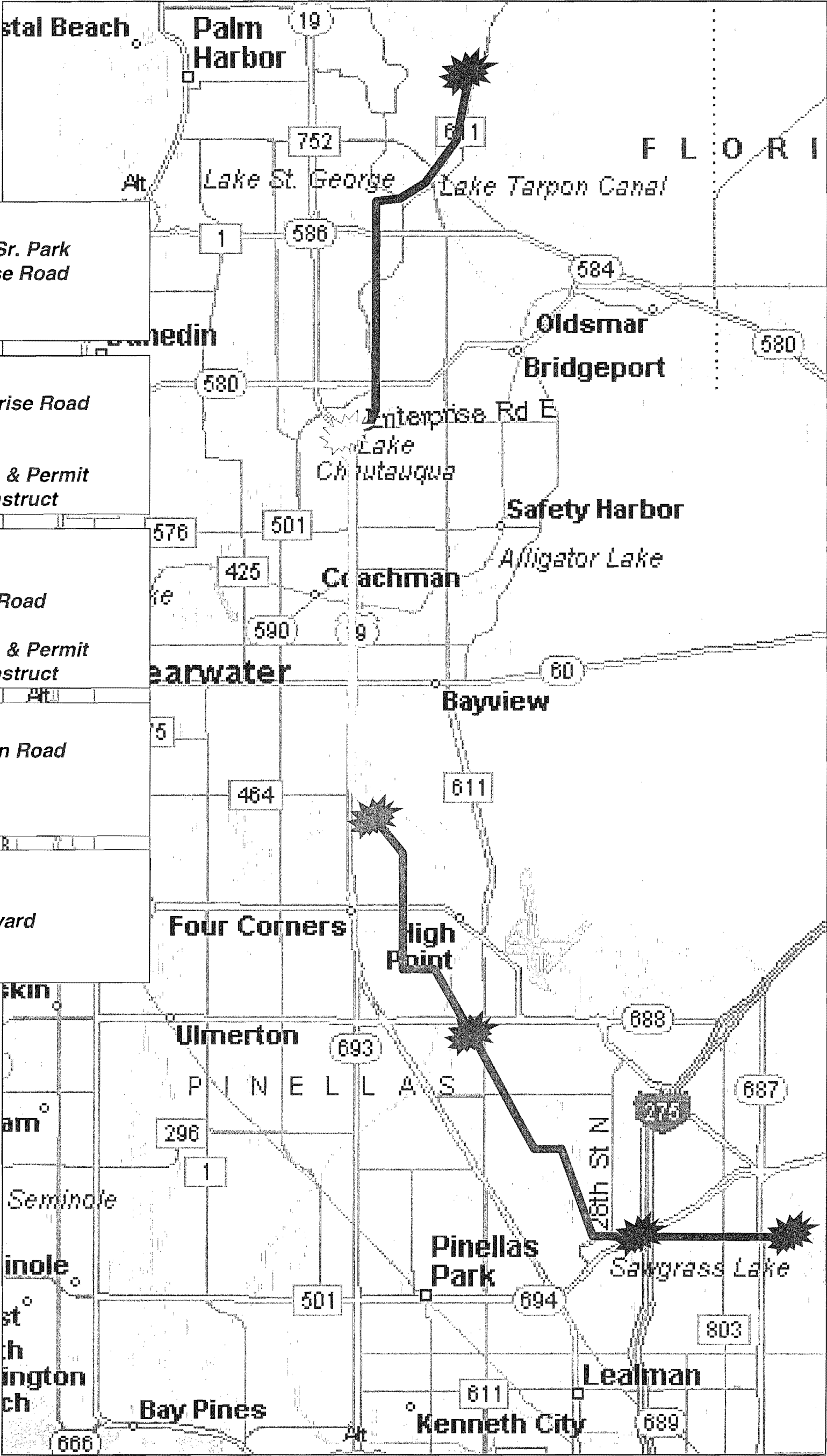
✓ *Preliminary Design & Permit*
✓ *Final Design & Construct*


Segment D
From: *SR 688/Ulmerton Road*
To: *I-275*


✓ *No Activity*

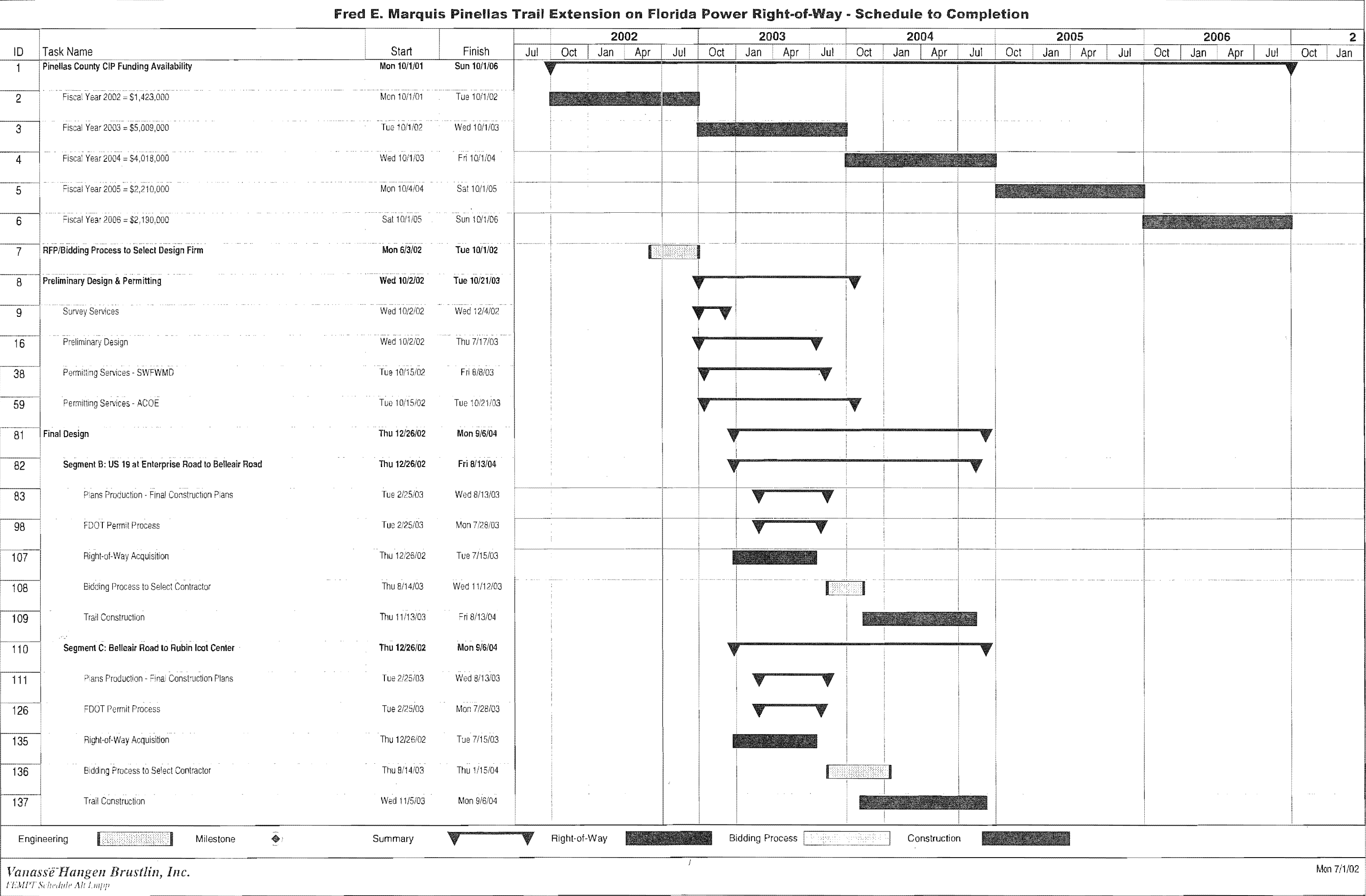
Segment E
From: *I-275*
To: *San Martin Boulevard*

✓ *No Activity*



Produced For:
 **Florida Power**

Produced By:
 **Vanasse Hangen Brustlin, Inc.**



Fred E. Marquis Pinellas Trail Extension on Florida Power Right-of-Way - Schedule to Completion

Note: Fiscal Year Begins in October

July 1, 2002

Trail Segment / Element	From	To	Project Length (miles)	Project Phase	Present Day Cost	Type of Funding by Phase	Proposed CIP Dollars Programmed By Fiscal Year / Inflation Factors ⁽¹⁾						Check Total	Comments
							2001	2002	2003	2004	2005	2006		
							Oct 2000 - Sept 2001	Oct 2001 - Sept 2002	Oct 2002 - Sept 2003	Oct 2003 - Sept 2004	Oct 2004 - Sept 2005	Oct 2005 - Sept 2006		
					(2)		1.000	1.000	1.000	1.000	1.000	1.000		
Master Plan Study			22.59	Master Planning	362,071	Penny	150,000	212,071	0	0	0	0		Master Plan Study complete.
				Total Element Cost	362,071		150,000	212,071	0	0	0	0	362,071	
Permitting & Preliminary Engineering For Segments B & C	John Young Chestnut, Sr. Park	San Martin Boulevard	22.59	Permitting	92,268	Penny	0	0	92,268	0	0	0		Includes Surveying & Permitting Services to obtain Construction Permits for Segments B & C
				Florida Power Engrg Admin	13,840	Penny	0	0	13,840	0	0	0		
				Florida Power Labor	9,227	Penny	0	0	9,228	0	0	0		
				Total Element Cost	115,335		0	0	115,336	0	0	0	115,336	
Segment A: TRAIL DESIGN	John Young Chestnut, Sr. Park	US 19 at Enterprise Road	6.55	Engineering Services	379,137		0	0	0	0	0	0		Requires Future Funding
				R/W Acquisition	3,473,850		0	0	0	0	0	0		Requires Future Funding
				Florida Power Contract Admin	252,758		0	0	0	0	0	0		Requires Future Funding
				Florida Power Engrg Admin	56,871		0	0	0	0	0	0		Requires Future Funding
				Florida Power Labor - Engrg	37,914		0	0	0	0	0	0		Requires Future Funding
				Florida Power Labor - CEI	252,758		0	0	0	0	0	0		Requires Future Funding
				Construction	2,527,579		0	0	0	0	0	0		Requires Future Funding
				Total Element Cost	6,980,867		0	0	0	0	0	0	0	
Segment A: BRIDGE DESIGN	Widen Brooker Creek Bridge; Widen East Lake Road Off-Ramp; New Ped Bridge Over Wetland			Engineering Services	343,801		0	0	0	0	0	0		Requires Future Funding
				R/W Acquisition	-		0	0	0	0	0	0		Requires Future Funding
				Florida Power Contract Admin	229,201		0	0	0	0	0	0		Requires Future Funding
				Florida Power Engrg Admin	51,570		0	0	0	0	0	0		Requires Future Funding
				Florida Power Labor - Engrg	34,380		0	0	0	0	0	0		Requires Future Funding
				Florida Power Labor - CEI	229,201		0	0	0	0	0	0		Requires Future Funding
				Construction	2,292,008		0	0	0	0	0	0		Requires Future Funding
				Total Element Cost	3,180,160		0	0	0	0	0	0	0	
Segment B: TRAIL DESIGN	US 19 at Enterprise Road	Belleair Road	5.46	Engineering Services	249,396	Penny	0	0	249,396	0	0	0		
				R/W Acquisition	10,350	Penny	0	0	0	0	0	0		
				Florida Power Contract Admin	195,605	Penny	0	0	0	195,606	0	0		
				Florida Power Engrg Admin	37,409	Penny	0	0	37,408	0	0	0		
				Florida Power Labor - Engrg	24,940	Penny	0	0	24,940	0	0	0		
				Florida Power Labor - CEI	195,605	Penny	0	0	0	195,606	0	0		
				Construction	1,956,050	Penny	0	0	0	1,956,048	0	0		
				Total Element Cost	2,669,355		0	0	311,744	2,347,260	0	0	2,659,004	
Segment C: TRAIL DESIGN	Belleair Road	SR 688/Ulmerton Road	3.25	Engineering Services	233,869	Penny	0	0	233,868	0	0	0		
				R/W Acquisition	1,338,675	Penny	0	0	1,338,676	0	0	0		
				Florida Power Contract Admin	183,427	Penny	0	0	0	183,426	0	0		
				Florida Power Engrg Admin	35,080	Penny	0	0	35,080	0	0	0		
				Florida Power Labor - Engrg	23,387	Penny	0	0	23,388	0	0	0		
				Florida Power Labor - CEI	183,427	Penny	0	0	0	183,426	0	0		
				Construction	1,834,270	Penny	0	0	0	1,834,272	0	0		
				Total Element Cost	3,832,135		0	0	1,631,011	2,201,124	0	0	3,832,135	
Segment C: BRIDGE DESIGN	Widen Existing US 19 Bridge Over Allen's Creek			Engineering Services	39,589	CMAQ	0	0	0	0	0	0		
				R/W Acquisition	-	CMAQ	0	0	0	0	0	0		
				Florida Power Contract Admin	31,050	CMAQ	0	0	0	0	0	0		
				Florida Power Engrg Admin	5,939	CMAQ	0	0	0	0	0	0		
				Florida Power Labor - Engrg	3,959	CMAQ	0	0	0	0	0	0		
				Florida Power Labor - CEI	31,050	CMAQ	0	0	0	0	0	0		
				Construction	310,500	CMAQ	0	0	0	0	0	0		
				Total Element Cost	422,086		0	0	0	0	0	0	0	
Segment D: TRAIL DESIGN	SR 688/Ulmerton Road	I-275	4.47	Engineering Services	350,312		0	0	0	0	0	0		Requires Future Funding
				R/W Acquisition	2,914,125		0	0	0	0	0	0		Requires Future Funding
				Florida Power Contract Admin	233,541		0	0	0	0	0	0		Requires Future Funding
				Florida Power Engrg Admin	52,547		0	0	0	0	0	0		Requires Future Funding
				Florida Power Labor - Engrg	35,031		0	0	0	0	0	0		Requires Future Funding
				Florida Power Labor - CEI	233,541		0	0	0	0	0	0		Requires Future Funding
				Construction	2,335,411		0	0	0	0	0	0		Requires Future Funding
				Total Element Cost	6,154,508		0	0	0	0	0	0	0	
Segment D: BRIDGE DESIGN	Proportionate Share of Ulmerton Road Bridge Over Cross Bayou Canal; New Ped Bridge Over Cross Bayou Canal			Engineering Services	96,773		0	0	0	0	0	0		Requires Future Funding
				R/W Acquisition	-		0	0	0	0	0	0		Requires Future Funding
				Florida Power Contract Admin	64,515		0	0	0	0	0	0		Requires Future Funding
				Florida Power Engrg Admin	14,516		0	0	0	0	0	0		Requires Future Funding
				Florida Power Labor - Engrg	9,677		0	0	0	0	0	0		Requires Future Funding
				Florida Power Labor - CEI	64,515		0	0	0	0	0	0		Requires Future Funding
				Construction	645,150		0	0	0	0	0	0		Requires Future Funding
				Total Element Cost	895,146		0	0	0	0	0	0	0	
Segment E: TRAIL DESIGN	I-275	San Martin Boulevard	2.86	Engineering Services	183,323	Penny	0	0	0	0	0	0		
				R/W Acquisition	-	Penny	0	0	0	0	0	0		
				Florida Power Contract Admin	122,216	Penny	0	0	0	0	0	0		
				Florida Power Engrg Admin	27,498	Penny	0	0	0	0	0	0		
				Florida Power Labor - Engrg	18,332	Penny	0	0	0	0	0	0		
				Florida Power Labor - CEI	122,216	Penny	0	0	0	0	0	0		
				Construction	1,222,156	Penny	0	0	0	0	0	0		
				Total Element Cost	1,895,740		0	0	0	0	0	0	0	
Segment E: BRIDGE DESIGN	New I-275 Pedestrian Bridge; New Bridge over Ditch (Exec Center Drive); New Ped Bridge Over Drainage Canal			Engineering Services	276,976	CMAQ	0	0	0	0	0	0		Bridge over I-275 NOT included.
				R/W Acquisition	-	CMAQ	0	0	0	0	0	0		
				Florida Power Contract Admin	184,651	CMAQ	0	0	0	0	0	0		
				Florida Power Engrg Admin	41,545	CMAQ	0	0	0	0	0	0		
				Florida Power Labor - Engrg	27,698	CMAQ	0	0	0	0	0	0		
				Florida Power Labor - CEI	184,651	CMAQ	0	0	0	0	0	0		
				Construction	1,846,510	CMAQ	0	0	0	0	0	0		
				Total Element Cost	2,562,032		0	0	0	0	0	0	0	
TOTAL WORK PROGRAM				Master Plan Phase	362,071		150,000	212,071	0	0	0	0	362,071	
				Permitting Phase	92,268		0	0	92,268	0	0	0	92,268	
				Engineering Services	2,153,176		0	0	483,264	0	0	0	454,339	
				R/W Acquisition	7,737,000		0	0	1,338,675	0	0	0	546,607	
				Florida Power Contract Admin	1,496,964		0	0	0	379,032	0	0	1,000,947	
				Florida Power Engrg Admin	336,815		0	0	86,328	0	0	0	1,547,554	
				Florida Power Labor - Engrg	224,545		0	0	57,556	0	0	0	2,548,501	
				Florida Power Labor - CEI	1,496,963		0	0	0	379,032	0	0	4,096,055	
				Construction	14,969,633		0	0	0	3,790,320	0	0	6,644,555	
				Total Project Cost	28,669,436		150,000	212,071	2,058,091	4,548,384	0	0	6,968,546	
				Accumulated Project Costs			150,000	362,071	2,420,162	6,968,546	6,968,546	6,968,546		
				Balance Available			0	1,210,929	4,161,838	3,631,454	5,841,454	8,031,454		
AVAILABLE FUNDING ⁽³⁾				Penny For Pinellas Funds			150,000	850,000	1,375,000	1,825,000	1,950,000	725,000		
				CMAQ Funds			-	573,000	3,634,000	2,193,000	260,000	1,465,000		
Source of Cost Information: Pinellas County Capital Improvement Program- FY 2				Accumulated Capital Improvement Program			150,000	1,573,000	6,582,000	10,600,000	12,810,000	15,000,000		

NOTES: 1. Source of Inflation Factors: Analysis is in Present Day Cost

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Fred E. Marquis Pinellas Trail
EXTENSION ON FLORIDA POWER RIGHT-OF-WAY

Introduction

This report presents a master plan for the 22.59-mile extension of the Pinellas Trail, largely on Florida Power right-of-way, from the existing trail at the John Chesnut Sr. Park to the Weedon Island Preserve. This project has been cooperatively undertaken by Florida Power and Pinellas County. Figure 1 depicts the existing Fred E. Marquis Pinellas Trail along with the proposed Extension on Florida Power Right-of-Way.

History of the Pinellas Trail

The Pinellas Trail began as a vision in 1983. Bert Valery, whose son was killed while riding his bike, helped form the Pinellas County Metropolitan Planning Organization's Bicycle Advisory Committee (BAC), consisting of bicycle enthusiasts. The committee, in conjunction with the Pedestrian Safety Committee, wanted Pinellas County to have a safe place to enjoy bicycle riding, strolling or jogging. In August 1989, the County leased a 47-mile corridor of abandoned CSX railroad right-of-way from Florida Department of Transportation (FDOT). The committees, along with several other supporting organizations, viewed the railroad right-of-way as an opportunity for providing a cross-county bicycle facility.

The voters of Pinellas County demonstrated their support for improving Pinellas County bicycle facilities and amenities in 1989, when they first voted to establish the "Penny for Pinellas," a 1% local option sales tax which funds infrastructure improvements. These Penny funds were used in the construction and expansion of the Pinellas Trail. Voters overwhelmingly approved the Penny in 1997 for another decade.

The committees' dream for a cross-county trail became a reality in mid-1990 when construction began of the first five-mile section of the Pinellas Trail, connecting Taylor Park in Largo to Seminole



**Pinellas Trail
Location Map**

Figure 1

City Hall. In June 1996, FDOT transferred the trail ownership to Pinellas County, by quitclaim deed.

Today, the trail stretches 47 miles from Tarpon Springs to South St. Petersburg, providing a unique greenway corridor linking some of Pinellas County's most picturesque parks, scenic coastal areas and residential neighborhoods. The trail includes eight overpasses, allowing trail-goers to travel above traffic at busy intersections, and one overpass over water. In addition, Pinellas Trails, Inc., a not-for-profit citizen's group, has provided trail amenities including bike racks and refreshment stops. The Pinellas County Park Department, Volunteer Rangers, and various municipalities provide regular patrol of the trail. After nearly two decades of progress, the Pinellas Trail has become one of the nation's most popular trails, attracting an annual average of 90,000 users each month.

On December 2, 2000 a series of celebrations were organized to celebrate the 10-year anniversary of the Pinellas Trail. Concurrent with the 10th Anniversary celebrations, the official name of the trail was changed to the Fred E. Marquis Pinellas Trail in honor of the former Pinellas County administrator.

Fred E. Marquis Pinellas Trail – Extension on Florida Power Right-of-Way

Seeking to build upon the success of the existing Fred E. Marquis Pinellas Trail, the BAC initiated a series of discussions about using a Florida Power (FP) corridor for an extension of the trail in 1996. Like the abandoned railroad corridor used for much of the existing Fred E. Marquis Pinellas Trail, power corridors are generally well suited for adaptation to bicycle and pedestrian use. The FP corridor generally parallels the eastern side of Pinellas County and has potential to form a key link in a loop trail envisioned to completely encompass the County.

In November of 1999, after three years of discussions and field investigations, Florida Power and the Pinellas County Board of County Commissioners signed and executed the Agreement to Lease and Joint Partnership Agreement for the development and construction of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way.

Project Description

The role of this Master Plan is to present the information about the proposed Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way, to evaluate alternatives, and to select one that will achieve the project's objective in a cost-effective manner. The Plan is divided into five parts: *Introduction, Design Approach, Master Plan, Land Use and Landscape Design Approach, and Implementation Plan.*

This *Introduction* provides a description of the Master Plan document as well as an overview of the proposed Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way. The *Design Approach* section presents the key issues and criteria that shaped the design and preferred alignment of the trail. The *Master Plan* section presents a detailed description of each trail segment accompanied by 1:200 scale aerial photographs showing the trail alignment. The *Land Use and Landscape Design Approach* section analyzes the land uses along the corridor and develops approaches to landscape design that are consistent with the trail's environment. Finally, the *Implementation Plan* section provides the framework and cost estimates for the proposed trail construction.

The Planning Process

As a first step to the planning process, a comprehensive inventory of the existing conditions of the Fred E.

Marquis Pinellas Trail - Extension on Florida Power Right-of-Way footprint, including physical and environmental constraints, alignment deficiencies, safety concerns, right-of-way restrictions, and roadway crossings was completed. Potential assets to the trail were inventoried, such as residential and commercial areas, schools, recreational areas, and other existing bicycle trails.

Public involvement was an integral aspect for the evolution of the master plan. A public information workshop was held at the St. Pete Junior College in Clearwater on February 22, 2001, in order to gather input from the public. The workshop was publicized in local media outlets and individual mailings were sent to all property owners within 300 feet of the proposed trail. At the workshop, aerial photographs showing the entire proposed trail alignment, were mounted for attendees to investigate and comment. Approximately 250 persons attended the workshop. Many spoke in favor of the project, some expressed specific concerns that needed to be addressed, and others voiced opposition.

In addition to the public workshop, several meetings and discussions were held throughout the planning process with stakeholder groups including Florida Power, Pinellas County, Cities, private property owners, and individuals with easements through the Florida Power right-of-way.

Overview of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way

The objective of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way project is to develop a multi-use bikeway/pedestrian trail that serves as a north-south connector between John Chesnut Sr. Park to the north and Weedon Island Preserve to the south. The trail will be the first public/private multi-use trail project constructed within an existing privately owned utility company right-of-way

(ROW) in the United States. Figures 2, 3, and 4, on the following pages, show the general alignment of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way in more detail. These figures illustrate connectivity to residential areas, commercial and business areas, schools, and libraries. In addition, existing and future trails, taken from the Pinellas County Metropolitan Planning Organization (MPO) Pinellas Trail Network Map, are shown in relationship to the proposed extension of the Pinellas Trail.

The 22.59-mile corridor connects with the existing Pinellas Trail at the John Chesnut Sr. Park. The corridor proceeds west and then south, eventually crossing U.S. 19 at its intersection with Enterprise Road. The corridor then proceeds southerly on the west side of U.S. 19 from west to east. The trail continues in a southerly direction to approximately 110th Avenue, at which point the trail utilizes County property to the east along the 110th Avenue alignment. The trail then proceeds south along the 34th Street alignment parallel to a drainage way to the point where it crosses the Florida Power corridor. Finally, the trail rejoins the Florida Power corridor and proceeds east, crossing I-275 and terminating at San Martin Boulevard, near the Weedon Island Preserve.

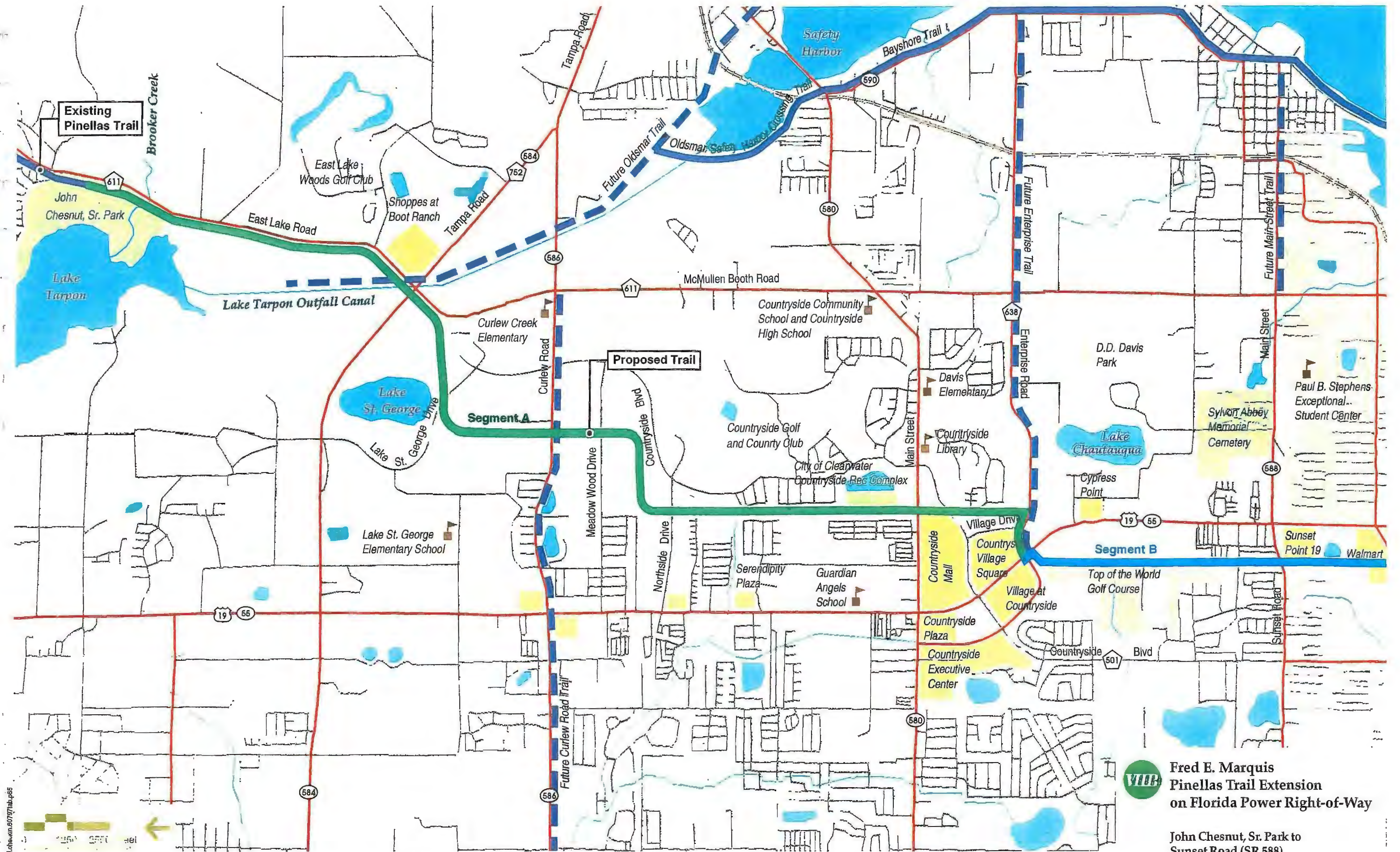
Florida Power has agreed to construct the trail under contract with Pinellas County and then turn the trail over to the County through a 99-year lease. The construction of this project will be accomplished in segments. An Agreement to Lease has been put in place between Florida Power and Pinellas County. As each segment is constructed and completed by Florida Power, the Lease will be effectuated by the Board of County Commissioners to include that segment. A third document, the Joint Partnership Agreement, defines how Florida Power will perform the work of planning, engineering, and constructing the trail. This work will be funded by the Penny for Pinellas, which includes funds for such trail improvements, with a limit of up to \$15 million. This trail corridor is

included in the MPO's Long Range Transportation Plan.

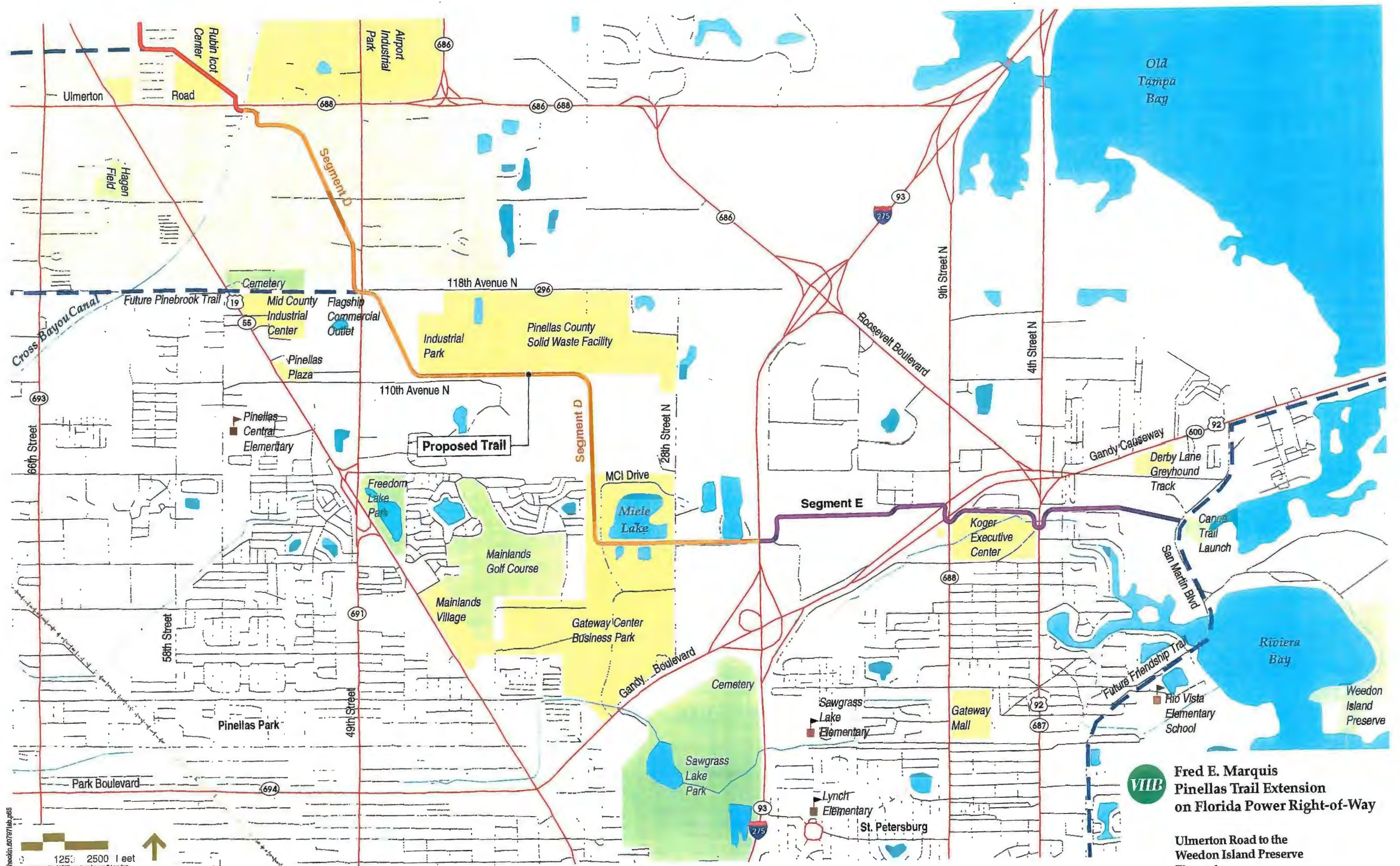
The trail will generally consist of a 15-foot paved surface with 5-foot grassy buffer on each side. These widths may be reduced to a minimum 12-foot paved surface and minimum 2-foot grassy buffers in constrained areas. The *Design Approach* section of this report will discuss the typical section in more detail. The completed trail will be a permitted use along the Florida Power corridor, with limited access by the trail users to other parts of that corridor. Amenities such as benches, shelters, water fountains, and informational signage may be located at intersecting roads or along the corridor.

The next section of this report presents the design approach used to complete this master plan.





Note: Existing and future trails shown are taken from the Pinellas County MPO Pinellas Trail Network Map.





Fred E. Marquis Pinellas Trail
EXTENSION ON FLORIDA POWER RIGHT-OF-WAY

Design Approach

The *Design Approach* highlights key issues that will affect the feasibility and costs of constructing the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way. A complete set of conceptual alignment plans (at a scale of 1:200) is provided in the *Master Plan* section of this report. These plans should provide detail on the trail alignment through the project area, alternative alignments, crossing plans, and adjacent right-of-way impacts. Supplemental 1:80 scale drawings are included in the *Appendix* that should provide additional detail to several key areas.

Planning and Design Objectives

The factors that influenced the design and alignment of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way are briefly discussed below and elaborated on throughout this report.

Roadway Crossings/Structures

Safety of trail users is paramount, therefore special attention was directed towards the design of all intersections. There are a total of 87 existing roadway and driveway crossings along the Extension of the Pinellas Trail. The roadway type, width, speed, traffic volume, angle of crossing, and sight distance were taken into consideration in determining the type of treatment necessary at each intersection. Details on these crossings and how they were approached is provided later in the *Design Approach* and *Master Plan* sections.

Environmental Constraints

Environmental constraints such as wetlands, retention ponds, lakes, creeks and streams, and natural habitats were considered in the design and

alignment of the Trail. For example, in locations where the corridor passes through wetlands, special treatment is required to ensure that the wetlands are not adversely affected and that the trail is not inundated by water. One common treatment is to elevate the trail through the use of a boardwalk or low profile bridge. Details on these environmental constraints are provided later in the *Design Approach* and *Master Plan* sections.

Land Use/Right-of-Way (ROW)

A trail by definition requires a relatively uninterrupted linear right-of-way. The Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way is facilitated by the availability of the Florida Power corridor for the majority of the route between John Chesnut Sr. Park and the Weedon Island Preserve.

The design and alignment of the trail is affected by existing uses within the right-of-way, as well as land uses adjacent to the right-of-way. Existing uses within the right-of-way were taken into consideration in the routing of the trail through the ROW. Existing uses include utilities such as the Florida Power overhead lines and related equipment for which the right-of-way was originally assembled, as well as easements for other uses such as parking lots or gardens previously authorized by Florida Power. In addition, there are several locations of privately owned property over which Florida Power has an easement. Coordination with these property owners has begun and must continue as the design and construction process proceeds.

Adjacent land uses must be considered for their effect upon the trail. For example, if a trail is routed entirely along a busy roadway with little buffer, it is unlikely to be attractive as a shared-use non-motorized vehicular transportation facility. By routing the trail further from the roadway, if

possible, or by adding appropriate landscaping, the trail environment can be made more pleasant for the users.

In several locations the extension of the Pinellas Trail must diverge from the Florida Power corridor. For these locations, the examination of adjacent land uses is equally important in the alignment and design of the trail. In addition, when the trail diverges from the corridor, there is the added challenge of locating a continuous route that is relatively free of obstacles for which trail right-of-way can be acquired. The Florida Power corridor right-of-way and land uses adjacent to the trail are shown in the 1:200 scale aerial plans in the *Master Plan* section of this report. Additional information on land uses and landscape design are presented in the *Land Use and Landscape Design Approach* section.

Utilities

As suggested in the previous discussion, existing utilities must be considered in the design and alignment of the trail. Overhead transmission lines and towers, underground electrical wires, and an underground hot oil pipeline exist within most of the Florida Power corridor. In addition, waterlines, gaslines, telephone poles and other assorted utility structures are located in easements along the corridor and at roadway crossings. In order to maintain unobstructed future access to these utilities and to ensure the safety of the trail users, the trail was aligned to avoid these utilities wherever possible. Detailed survey and coordination with utility owners will take place during the design phase. Known major utilities located within Florida Power right-of-way are shown on the 1:200 scale aerial photographs in the *Master Plan* section.

Drainage

Drainage on and surrounding the trail is an important consideration that determines the interaction of the trail with the existing flowways. The general drainage investigation includes identification of the adjacent basins and sub-basins, low-lying and flood-prone areas, and investigation of existing drainage structures. In some instances, a grass swale alongside the trail may be appropriate to direct flow away from the trail. Similarly, if the proposed trail route is in the path of a flow channel through a basin, it may be necessary to add a new structure to facilitate flow underneath the trail. Details of the design approach to drainage issues are provided later in the *Design Approach* section.

Connectivity

The value of a trail is directly linked to the value of the connections it provides. For this reason, the trail should be accessible to as many people as possible. Locating the trail near residential areas provides a readily available population of potential trail users. The trail should also be accessible to local attractions such as commercial areas, schools, libraries, recreational facilities, etc. A third consideration is linking the trail with other proposed and existing trails in order to create a continuous network. Providing connections to a variety of destinations makes the trail attractive for both commuting and recreational uses.

In the case of the extension of the Pinellas Trail, the general route follows the fixed Florida Power corridor. Nonetheless, the identification of adjacent residential areas, local attractions, and other trails is important so that local connections can be strengthened as the trail evolves.

Constructability/Maintenance of Traffic

Constructability and maintenance of traffic (MOT) are important factors that were taken into consideration in determining the design and alignment of the trail. To create a safe and successful trail, certain standards should be met concerning trail features such as the slope or width of the trail. Natural features of the land as well as existing uses such as the power lines required innovative planning and design to meet desirable standards.

Whenever the trail interfaces with the local roadway system, maintenance of traffic during construction becomes an issue. There are three scenarios in which maintenance of traffic compatibility should be examined. These are at: bridge/roadway overpass locations, sections where the trail is parallel and adjacent to a roadway, and at trail/roadway intersections. Maintenance of traffic for bridge construction, as well as both types of roadway interfaces, should follow the FDOT Standard Index 600 series.

The next section of this report presents the specific design criteria used to develop the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way Master Plan.

Design Criteria

The existing Pinellas Trail system services a multitude of users and the extension must be planned using the appropriate design criteria to safely accommodate these users. The facility will be designed in accordance with Florida Department of Transportation (FDOT) and American Association of State Highway and Transportation Officials (AASHTO) design standards. The following are the design manuals used during the development of the trail design criteria.

- FDOT Roadway and Traffic Design Standards for Design Construction, Maintenance of Traffic, and Utility Operations on the State Highway System, January 2000
- FDOT Bicycle Facilities Planning and Design Handbook, April 2000
- AASHTO Guide for the Development of Bicycle Facilities, 1999
- Manual of Uniform Minimum Standards for Design, Construction and Maintenance of Traffic for Street and Highways, 1989.

The *FDOT Bicycle Facilities Planning & Design Handbook* states that a two-way multi-use trail should be a minimum of 12-feet wide. A minimum of 10-feet may be used if ALL of the following are met:

- Bicycle traffic will be low, even on peak days or during peak hours;
- Pedestrian or in-line skater use of the facility will be only occasional;
- There will be good horizontal and vertical alignment providing safe and frequent passing opportunities; and
- The path will not be subjected to maintenance vehicle loading conditions that would cause pavement edge damage.

The recommended trail width, shown in Figure 5, is 15-feet with 5-foot shoulders. A minimum 12-foot width with 2-foot shoulders may be used in constrained areas. Analysis of the corridor concludes the need for less than a 12-foot wide path will be minimal. A 10-foot width will be considered on a case-by-case analysis. The preferred distance to a fixed object is 4-feet and the minimum is 2-feet. This cross-section will provide adequate capacity and flexibility to accommodate the needs of different users through the life of the trail.

The *FDOT Bicycle Facilities Planning & Design Handbook* states a 6-foot lateral separation is desirable from any embankment that would create difficulties

for bicyclists (greater than or equal to a 3:1 slope). Otherwise an appropriate safety railing should be installed. A minimum 6-foot separation from the edge of the bike path to top of slope is desirable. If this is not possible, a positive barrier such as dense shrubbery or chain link fence shall be provided. Under most conditions for this project, when slopes are encountered adjacent to the trail alignment, a waterway is located at the bottom of the slope. Based on engineering judgment, a lateral separation less than 6-feet will require fence or railing. Where waterways exist at the toe of slope, a fence or railing treatment should be provided. Individual locations will be analyzed for the appropriate treatment during design.

Where possible the trail should be located outside of the clear zone of the parallel roadway. For extreme conditions, the *FDOT Bicycle Facilities Planning & Design Handbook* states that if the distance from the edge of shoulder to edge of trail is less than 3.5-feet, then a suitable physical divider should be considered. Based on engineering judgment, a 5-foot separation is preferred, and less than 3.5-feet would require a divider. This pertains to rural roadway sections and could be overruled by the roadway's clear zone requirements. For urban roadway sections, the trail may be placed adjacent to the back of curb. In determining the appropriate divider to separate the trail from a parallel rural roadway, the posted speed limit and roadway clear zone requirements will be accounted for.

Since the trail alignment will be within Florida Power right-of-way for the majority of its length, there are utility offset criteria, which must be upheld during the design of the proposed trail. An offset of 50-feet is preferred between the trail and tower legs, but a minimum of 20-feet must be met in order to accommodate Florida Power's Condor Truck. If a 20-foot minimum cannot be maintained, and Florida Power approves this variance, then a special pavement section will be required for the trail (See

Figure 5). A minimum offset of 15-feet is preferred between the hot oil pipeline and the trail. Any variance to these distances will be reviewed on a case-by-case analysis and will require approval by appropriate Florida Power personnel.

The FDOT Bicycle Facilities Planning & Design Handbook states that for a 20 MPH design speed (30 KPH) the minimum radius is 73-feet (24 meters); AASHTO *Guide for the Development of Bicycle Facilities* states that the minimum radius is 100-feet (27 meters). Based on engineering judgment and analysis, the preferred minimum radius is 100-feet, but in constrained areas, and with proper signing, a lesser radius may be used.

Additional typical details of the cross-sectional treatments are also provided on Figure 5. The overall design criteria used to develop this Master Plan are summarized in Table 2-1. Design criteria related to structural design (bridges/overpasses) is provided later in Table 2-3 of the Design Approach Section.

Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way Segments

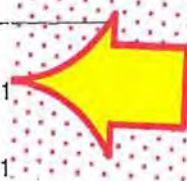
The 22.59 miles under consideration as part of this Master Plan are broken down into trail segments. Major roadway crossings, right-of-way issues, or changes in land use along the corridor define the segments. Five segments that were previously presented on Figures 2, 3 and 4 are described below:

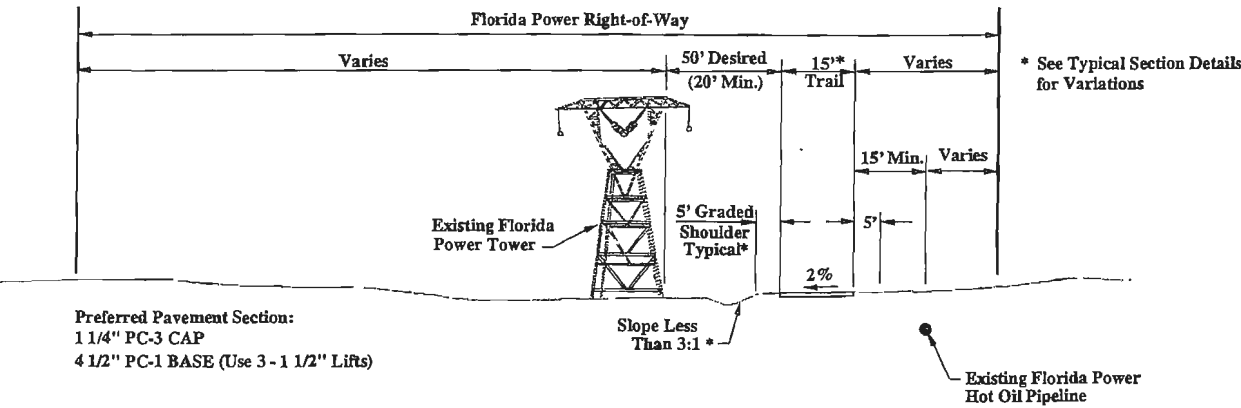
- **Segment A**—From John Chesnut Sr. Park (the existing Pinellas Trail) to U.S. 19 at Enterprise Road;
- **Segment B**—From U.S. 19 at Enterprise Road to Allen's Creek;
- **Segment C**—From Allen's Creek to State Road (SR) 688/Ulmerton Road;
- **Segment D**—From SR 688/Ulmerton Road to I-275; and,
- **Segment E**—From I-275 to the San Martin Boulevard.

Table 2-1
Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way Design Criteria

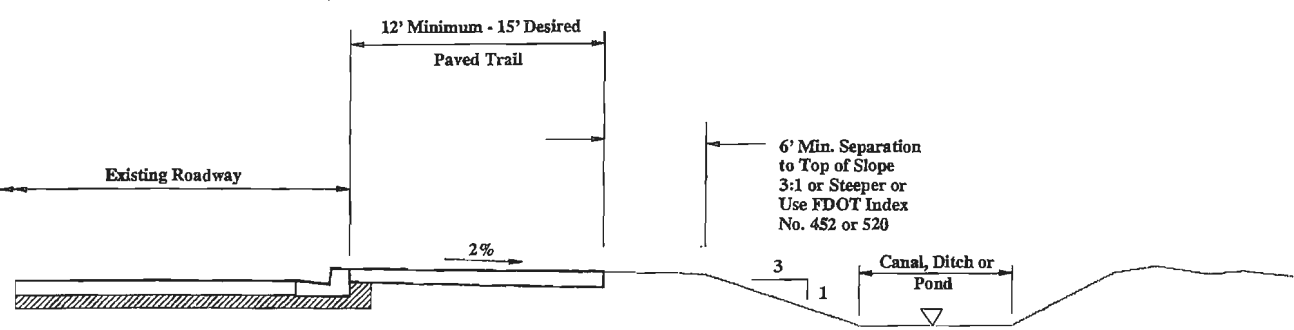
Reference: *FDOT Bicycle Facilities Planning and Design Handbook, April 2000*

Design Element	Criteria	Source
Trail Alignment		
Design Speed	20 mph (30 km/hr)	Page 5-11
Horizontal Widths	15' Desirable 12' Minimum	Page 5-8
Graded Shoulders	5' Desirable 2' Minimum	Page 5-10
Horizontal Clearance to Obstructions	4' Desirable 2' Minimum	Page 5-10
Separation from Slopes >3:1 and from Canals/Ditches	6' Desirable or Use FDOT Index No. 452 or 520.	Page 5-10
Separation from Parallel Roadways	Rural: 5' Desirable; 3.5' Minimum from roadway shoulder or use a suitable physical divider Urban: Trail may be adjacent to back of curb.	Page 5-10
Superelevation	2% Maximum	Page 5-12
Radius	100' Desirable	Table 1, Page 5-13
Stopping Sight Distance Grade = 5% Descending	130' (Varies for other grades)	Figure 1, Page 5-17
Lateral Clearance on Inside of Horizontal Curve	Based on sum of Stopping Sight Distance for trail users traveling in opposite directions	Table 3, Page 5-19
Vertical Alignment Grades	≤5% Desirable >5%	Page 5-13 Page 5-14
Length of Vertical Curve	Based on grades and required Stopping Sight Distance	Table 2, Page 5-18
Vertical Clearances		
Vertical Clearance to Obstructions	8' Minimum	Page 5-11
Tunnels/Underpasses	10'	Page 5-11
Other		
Roadway Median Width	10'	Page 5-22
Clearance to Florida Power Towers	50' Desired 20' Minimum	From Florida Power
Clearance to Florida Power Hot Oil Pipeline	15' Minimum	From Florida Power

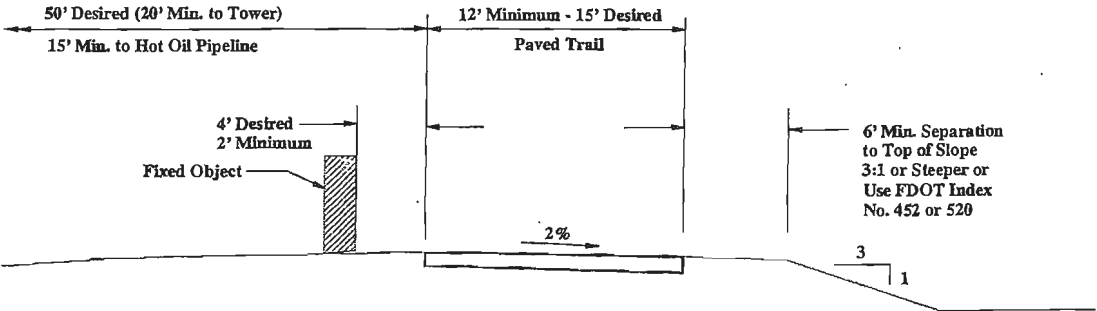




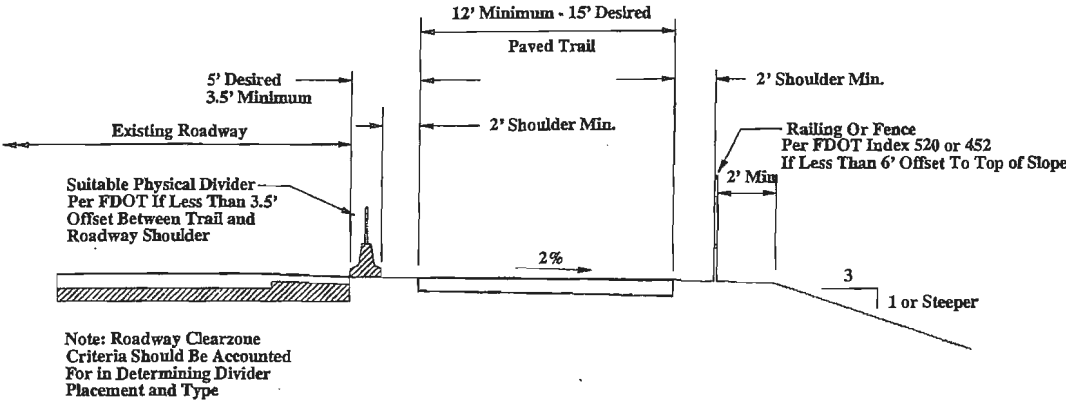
Preferred Project Typical Section



Typical Section Detail No. 2



Typical Section Detail No. 1



Typical Section Detail No. 3

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Figure 5

Fred E. Marquis Pinellas Trail
Extension on Florida Power Right-of-Way

Typical Sections

Roadway/Trail Crossings

There are approximately 87 roadway and driveway crossings along the proposed Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way. Preliminary analysis of each of these crossings to determine the appropriate trail crossing method was completed using the "Trail Intersection Design Guidelines, August 1996" prepared by the University of North Carolina Highway Safety Research Center for FDOT. This handbook presents a number of considerations that take the trail user's, as well as motor vehicle user's, expectations, abilities, and behaviors into account for trail design. The following is a summary of the Principles of "friendly" design as presented in the handbook.

Principles of "Friendly" Trail Design

- Design for the full spectrum of trail users—young and old, slow and fast, bicyclists, skaters, and walkers.
- When assigning right-of-way, provide clear right-of-way assignment.
- Provide positive guidance for trail users and motorists to ensure full awareness of the intersection.
- Minimize conflicts, to the degree possible.
- Unavoidable conflicts should occur at right angles.
- Optimize sight triangles, ensuring stopping, intersection crossing, and decision sight distances.
- Minimize trail user crossing distance with a median refuge area or by narrowing the roadway, as appropriate.
- Discourage unwanted motor vehicle intrusion onto the trail while enabling emergency and maintenance vehicle entry.
- Avoid obstacles and visibly highlight unavoidable obstacles.
- At signalized intersections, minimize trail user delay by minimizing traffic signal cycle time.

- Provide adequate signal crossing time for design pedestrians.
- Provide easily accessible tactile/audible pushbuttons.
- Treat every road as a potential trail entrance and exit point, integrated with sidewalks and on-street bicycle facilities, as appropriate.
- Consider lighting.
- Consider the ease of both construction and maintenance and the initial and lifetime costs for construction and maintenance.
- Be consistent in design.

These principles of "friendly" trail crossing design and suggested treatment methods for roadway crossings, as found in the "Trail Intersection Design Guidelines", along with a review of current practices along the existing Pinellas Trail, shaped the proposed treatment of trail crossings and user right-of-way assignment for each of the 87 roadway and driveway crossings summarized in Table 2-2. Generally, the right-of-way will be assigned to the motorists. The users of the trail have the greatest potential for harm where conflicts occur; therefore they must be put in a position of making active decisions related to crossing a roadway or driveway. Signage and control mechanisms will be designed for the trail, to ensure that the users are made aware of the conflict points in advance of, as well as at the point of conflict. At the crossing, signs and crosswalks will also warn the motorists that a crossing point exists. In addition to signs and crosswalks, the trail will be designed with passive features that will change the general character at the crossing, causing the user to become more attentive to the surroundings.

The recommendations shown in Table 2-2 will require further evaluation during the design phase to determine the most appropriate solution for each crossing. Pedestrian warrants described in the

Manual on Uniform Traffic Control Devices, as well as intersection analysis, signal warrant analysis and coordination with the appropriate traffic control jurisdiction, will also be completed and used during final design. Existing signalization at or adjacent to trail crossings and whether or not pedestrian features are currently in place, have been noted. The signalization is generally the jurisdiction of the local municipality. Pinellas County maintains unincorporated areas and some local municipalities signals. Signals affected by this project include the City of Clearwater and the City of St. Pete.

Details for typical roadway crossings are provided on Figures 6 and 7. These details include signing, pavement marking, and signal applications for both the trail and roadway for two-lane, four-lane divided and seven-lane roadways. All signing, pavement marking and signalization applications will meet Pinellas County, FDOT, and AASHTO standards where applicable. In addition to the details, 1:200 scale aerial plans in the *Master Plan* section of this report illustrate the trail alignment, and in some cases potential alternatives, envisioned at the critical crossings.

Structures

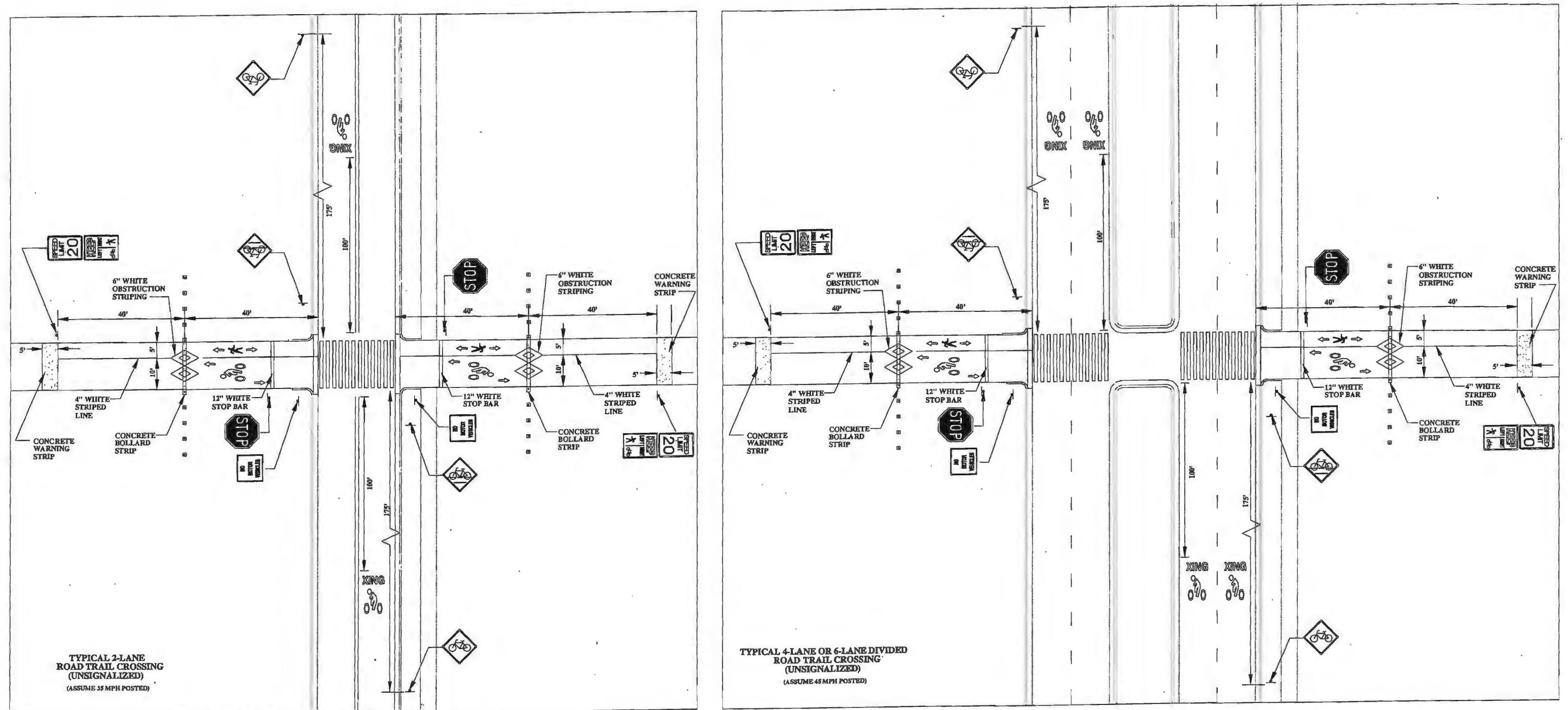
In some locations, in order to increase safety, it is desirable to provide grade separation. Several water crossings and a highway overpass are proposed as part of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way Master Plan. In addition, two widenings to existing bridge structures are proposed. This section conceptually identifies the viable structural systems for the crossings and discusses the proposed alternatives considered for each location.

Table 2-2
Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way Trail Crossings

Trail Segment	Number	Street Name	Plan Sheet	Class	Jurisdiction	Posted Speed Limit (mph)	Number of Lanes	Potential Crossing Method
A	1	Park Entrance #1	1	Minor	Pinellas County	NA	1	Sign trail and roadway as crossing where <i>trail</i> traffic stops.
	2	Park Entrance #2	1	Minor	Pinellas County	NA	1	Sign trail and roadway as crossing where <i>trail</i> traffic stops.
	3	New Residential Entrance	2	Minor	Pinellas County	NA	2 + Median	Sign trail and roadway as crossing where <i>trail</i> traffic stops.
	4	North Boot Ranch Entrance	2	Medium	Pinellas County	NA	4 + Median	Utilize existing signal and pedestrian features; modify to meet standards.
	5	South Boot Ranch Exit Ramp	3	Medium	Pinellas County	NA	1	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	6	South Boot Ranch Entrance Ramp	3	Medium	Pinellas County	NA	1	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	7	Tampa Road	4	Major*	FDOT	45	6 +	Utilize existing signal, need to add pedestrian features across Tampa Road.
	8	Catalina Drive	5	Minor*	Pinellas County	35	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	9	Lake Street George Drive	6	Minor*	Pinellas County	35	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	10	Curlew Road	7	Major*	FDOT	45	6 + Median	Add midblock pedestrian signal to stop <i>roadway</i> traffic.
	11	Meadow Wood Drive	7	Minor*	City of Clearwater	35	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	12	Fox Hill Drive	8	Minor	City of Clearwater	35	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	13	North Ridge Drive East	8	Minor*	City of Clearwater	35	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	14	Northside Drive	9	Minor*	City of Clearwater	35	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	15	Countryside Recreation Driveway	11	Minor	City of Clearwater	NA	1	Sign trail and roadway as crossing where <i>trail</i> traffic yields.
	16	SR 580/Main Street	11	Major*	FDOT	45	6 + TWLTL	Add midblock pedestrian signal to stop <i>roadway</i> traffic.
	17	Countryside Boulevard	12	Major	City of Clearwater	45	6 + Median	Signalize Mail entrance and include pedestrian features.
	18	Village Drive	12/13	Medium	City of Clearwater	40	4 +	Utilize existing signal at Enterprise Road, need to add pedestrian features.
B	19	US 19 @ Enterprise Road	13	Major w/OP*	FDOT	55 ?	6 + Median	Due to future FDOT plans, best alternative is to cross U.S. 19 at-grade at existing Enterprise Road signal.
	20	Enterprise Road	12/13	Major*	City of Clearwater	45	4 + Median	Due to future FDOT plans, best alternative is to cross Enterprise Road at-grade at existing U.S. 19 signal.
	21	Sunset Point Road	15	Major*	Pinellas County	45	4 + Median	Add midblock pedestrian signal to stop <i>roadway</i> traffic.
	22	Stag Run Boulevard cul-de-sacs	16	N/A	City of Clearwater	NA	NA	N/A - Going between cul-de-sacs.
	23	NE Coachman Road	16	Major*	FDOT	45	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	24	Seaboard Coast Line Railroad	17	Major*	CSX Corporation	?	1 Track	Add at-grade crossing; Sign trail as crossing where <i>trail</i> traffic stops.
	25	Transfer Station Driveway	17	Minor	City of Clearwater	NA	2	Sign trail and roadway as crossing where <i>trail</i> traffic stops.
	26	Baseball Complex Driveway	18	Minor	City of Clearwater	NA	2	Sign trail and roadway as crossing where <i>trail</i> traffic yields.
	27	Driveway Entrance (@Sharkey Road)	18	Minor*	City of Clearwater	NA	2	Sign trail and roadway as crossing where <i>trail</i> traffic stops.
	28	Target Driveway	18	Minor	City of Clearwater	NA	2	Sign trail and roadway as crossing where <i>trail</i> traffic stops.
	29	TWC Parking and Driveway	18	Minor	City of Clearwater	NA	NA	Sign trail and roadway as crossing where <i>trail</i> traffic stops.
	30	Draw Street	18	Major*	City of Clearwater	40	2 +	Utilize existing signal and pedestrian features at Old Coachman and Drew Street; modify to meet standards.
	31	Shopping Entrance	18	Medium	City of Clearwater	NA	2 +	Sign trail and roadway as crossing where <i>trail</i> traffic stops.
	32	Post Court Apartments Entrance	19	Medium	City of Clearwater	NA	3	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	33	Greek Orthodox Church Entrance	19	Minor	City of Clearwater	NA	2 + Median	Sign trail and roadway as crossing where <i>trail</i> traffic yields.
	34	Tampa Tribune Driveway	19	Minor	City of Clearwater	NA	2	Sign trail and roadway as crossing where <i>trail</i> traffic yields.
	35	Gulf to Bay Boulevard/SR 60	19	Major*	FDOT	45	6 + Median	Utilize existing signal at Old Coachman and Gulf to Bay; need to add pedestrian features across Gulf to Bay.
	36	Druid Road	20	Major*	City of Clearwater	35 ?	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	37	Bumice Drive	20	Minor*	City of Clearwater	35	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	38	Brentwood Drive	20	Minor*	City of Clearwater	35	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	39	Ham Boulevard	21	Minor*	City of Clearwater	40	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	40	Nursery Road	21	Minor*	City of Clearwater	45	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.
	41	Belleair Road	22	Minor*	Pinellas County	35	2	Sign trail and roadway as "midblock" crossing where <i>trail</i> traffic stops.

Table 2-2
Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way Trail Crossings
(Continued)

Trail Segment	Number	Street Name	Plan Sheet	Class	Jurisdiction	Posted Speed Limit (mph)	Number of Lanes	Potential Crossing Method
C	42	Allen's Creek	22	Major w/OP*	Pinellas County	NA	NA	Interim option: east on Bellair to US 19.
	43	US 19/SR 55	22/23	Major w/OP*	FDOT	50	6+ Median	Interim option: cross US 19 at either existing Bellair signal or existing Haines Bayshore signal, will need to add pedestrian features at either signal.
	44	Haines Bayshore Road	23	Medium	Pinellas County	45	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	45	Whitney Road	24	Minor*	Pinellas County	45	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	46	Apartment Complex Entrance	25	Minor	Pinellas County	NA	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	47	Roosevelt Boulevard	25	Major*	FDOT	45	6+ Median	Utilize existing signal with pedestrian features at 62nd Street North; modify to meet standards.
	48	Vacant Nursery Driveway	25	Minor	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	49	Driveway	25	Minor	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	50	PTEC Driveway #1	25	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	51	PTEC Driveway #2	25	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	52	PTEC Driveway #3	25	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	53	PTEC Driveway #4	26	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	54	Automobile Road (150th)	26	Minor*	Pinellas County	40	2	Utilize existing signal and pedestrian features at 62nd Street North; modify to meet standards.
	55	Nursery Driveway	26	Minor	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	56	Mears Court	26	Medium	Pinellas County	?	2	Sign trail and roadway as crossing where trail traffic stops.
	57	Auto Trader Magazine Parking and Driveway	26	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic stops.
	58	Driveway #1	26	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic stops.
	59	Driveway #2	26	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	60	Driveway #3	26	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	61	Driveway #4	26	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	62	Driveway #5	26	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	63	Driveway #6	26	Medium	Pinellas County	NA	2	Sign trail and roadway as crossing where trail traffic yields.
	64	142nd Avenue North	27	Minor*	Pinellas County	45	2	Utilize existing signal and pedestrian features at 142nd and 62nd Streets; modify to meet standards.
	65	61st Street	27	Minor*	Pinellas County	35	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	66	60th Street	27	Minor*	Pinellas County	35	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	67	Icot Boulevard	27	Minor*	Pinellas County	40	4+	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	68	58th Street North	28	Medium	Pinellas County	40	4+	Sign trail and roadway as "midblock" crossing where trail traffic stops.
D	69	Ulmerton/SR 688	28	Major w/OP*	FDOT	50	4+ Median	As part of FDOT widening to 6-lanes, trail may pass under Ulmerton Road. Interim option: utilize existing signal at 58th Street North, will need to add pedestrian features; then east on south side of Ulmerton.
	70	Rubin Icot Center Road	28	Minor	Pinellas County	NA	2+	On-road bike lanes.
	71	Ulmerton Sub Station Access Drive	29	Minor	Pinellas County	NA	NA	Sign trail and access road as "midblock" crossing where road traffic yields.
	72	126th Street	29	Minor*	Pinellas County	40	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	73	49th Street North/118th Avenue North	30	Major*	City of Pinellas Park	50/50	6+ Median/ 6+ Median	Utilize existing signal and pedestrian features to cross 118th Avenue and 49th Street; modify to meet standards.
	74	47th Street Mary Boulevard	31	Minor*	City of Pinellas Park	40	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	75	43rd Street North	32	Minor	City of Pinellas Park	40	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	76	MCI Drive	35	Minor	City of Pinellas Park	45	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	77	28th Street North	36	Minor*	City of St. Petersburg	50	4+ Flush Median	Add midblock pedestrian signal to stop roadway traffic.
E	78	I-275	37	Major w/OP*	FDOT	55	6+	Interim option: 26th Street North, south to Gandy, then east, along Gandy to 9th Street North.
	79	18th Street North	38	Minor*	City of St. Petersburg	45	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	80	18th Street North	38	Minor*	City of St. Petersburg	45	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	81	Access Road	39	Medium	City of St. Petersburg	45	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	82	US 92/Gandy Boulevard	39	Major w/OP*	FDOT	45	4+	Utilize the existing signal at 9th and Gandy, will need to add pedestrian features.
	83	9th Street North	39	Major w/OP*	Pinellas County	45	4+	Utilize the existing signal at 9th and Gandy, will need to add pedestrian features.
	84	Executive Center Drive/5th Street North	40	Minor*	City of St. Petersburg	45	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	85	4th Street North	40	Major w/OP*	FDOT	45	4+	Possible utilization of the existing signal and pedestrian features South of 99th Avenue @ Koger Boulevard; modify to meet standards.
	86	2nd Street North	40	Minor*	City of St. Petersburg	40	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.
	87	San Martin Boulevard	41	Medium	Pinellas County	45	2	Sign trail and roadway as "midblock" crossing where trail traffic stops.

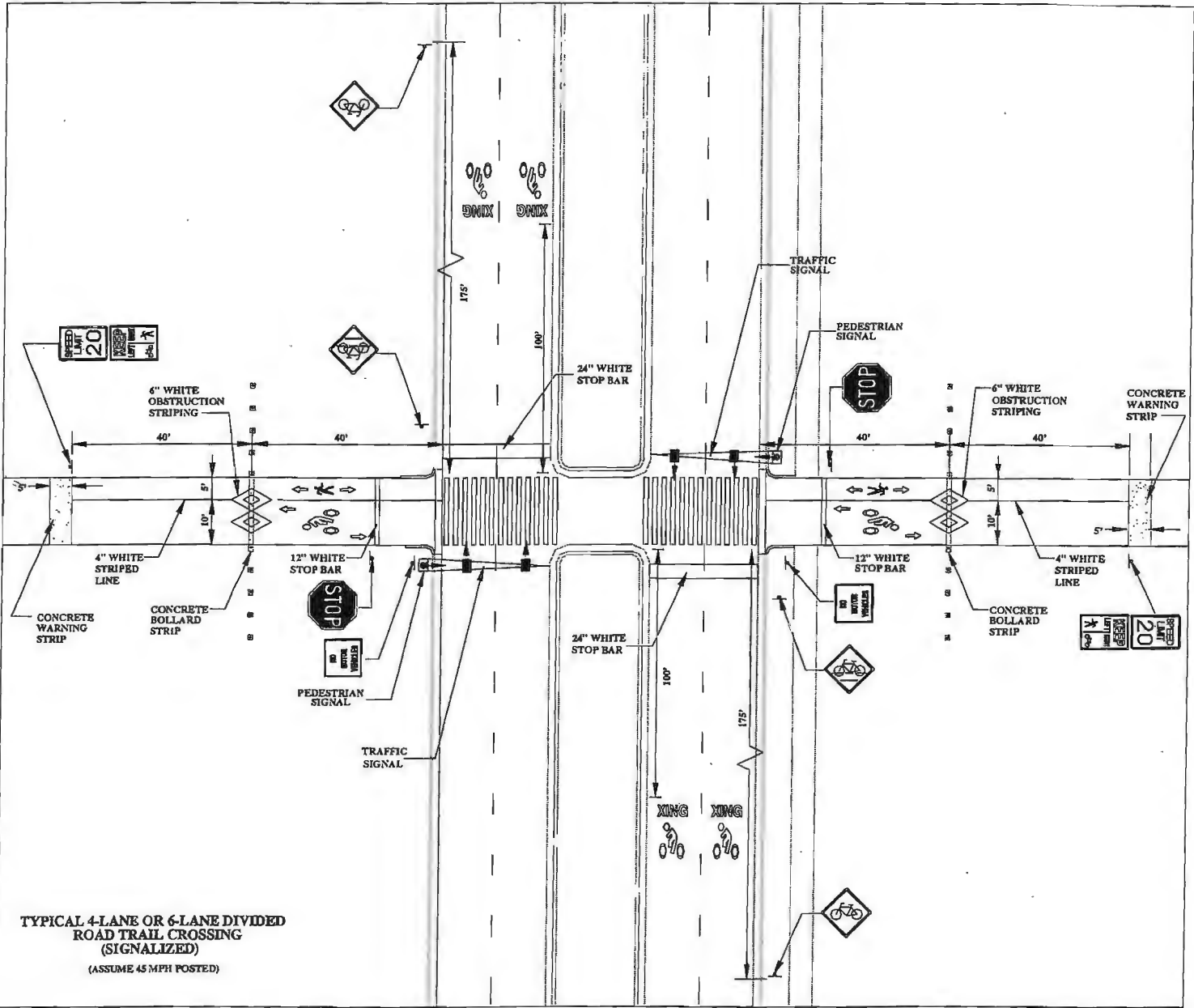
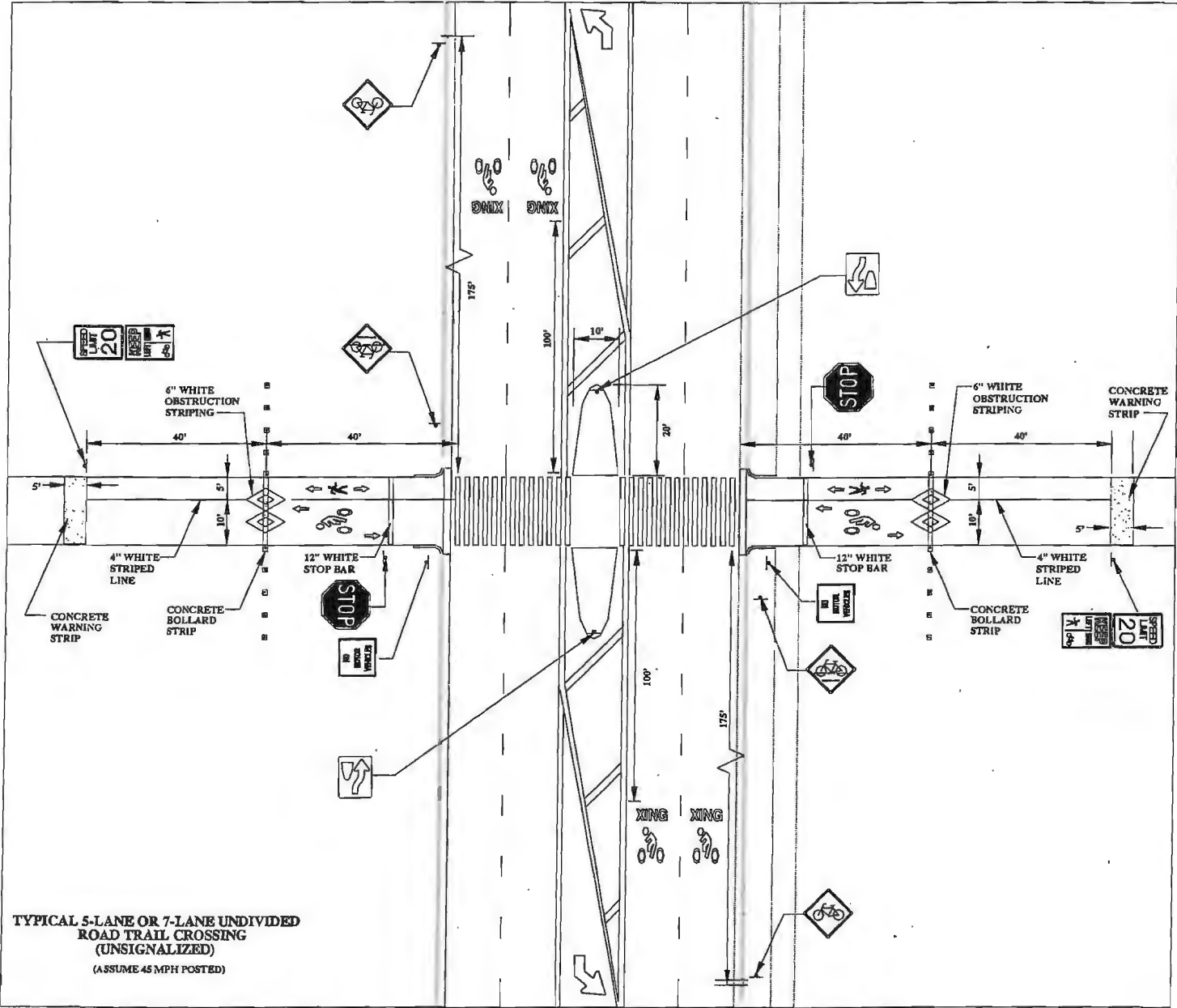


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Figure 6

Fred E. Marquis Pinellas Trail
 Extension on Florida Power Right-of-Way

Typical At-Grade Crossings



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Figure 7

Fred E. Marquis Pinellas Trail
Extension on Florida Power Right-of-Way

Typical At-Grade Crossings

The Pinellas Trail highway overpass will occur at:

- I-275

A low-level mangrove, wetland and water crossing is located at:

- Wetland Crossing near Lake St. George Drive

Canal crossing locations are located at:

- Cross Bayou Canal
- Drainage Canal

Bridges to be widened include:

- East Lake Road over Brooker Creek
- East Lake Road over Lake Tarpon Outfall Canal SB off-ramp

Two other major structures that are alternative trail alignments, which are also discussed in this report, are at:

- US 19 at Allen’s Creek
- Allen’s Creek

Analysis was also completed for major roadway crossings to determine the need for additional grade separations and their priority. The summary matrix for this analysis can be found in the *Appendix*. The analysis for each crossing included examination of the number of roadway lanes the trail must cross, the volume of the cross street traffic, the posted speed limit of the cross street, the availability of a median or refuge within the street, what type of disruption the roadway crossing presents to the trail user, the type of traffic control possible without a grade separation, and the estimated cost for providing a grade separation. The total scores for each roadway crossing were then ranked from lowest to highest. The top five priority locations for which further analysis is recommended are at U.S. 19 at Allen’s Creek, SR 580/Main Street, US 19 at Enterprise Road, Allen’s Creek, and SR 60/Gulf to Bay Boulevard. Please note that the US 19 at Enterprise grade separation is an alternative to the preferred alignment. Further analysis of this location will only

be necessary if the preferred alignment cannot be achieved.



Typical Section Evaluation

The Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way is considered a two-directional multi-use facility. Therefore, the trail typical section for structures in this study includes a 12-foot minimum clear width, a 2-foot clear area at both sides of the path, as well as 1 foot curbs on both sides of the structure for a total section width of 18 feet as shown on Figure 8. Bicycle rails are provided on both sides of the bridge. In addition, all roadway overpasses are covered with an enclosed type chain link fence for protection of traffic below. A minimum headroom clearance of 10 feet at the edge of curb is maintained for emergency vehicle access.

Approach ramps at the overpass locations are proposed. The maximum grade of ramps is 8.33%. Five feet intermediate level platforms shall be provided on all approach ramps for rest areas. Slopes between 5% and 6.5% require rest areas every 40 feet, while slopes steeper than 6.5% require the intermediate landings every 30 feet. Intermediate level platforms may be provided at intervals greater than 40 feet for slopes less than 5%.”



Structural Design Criteria

The structural design of the bridges shall be in full compliance with the latest edition of the following specifications and guidelines:

- AASHTO Standard Specifications for Highway Bridges
- AASHTO Guide Specifications for the Design of Pedestrian Bridges
- FDOT Structures Design Guidelines (SDG)
- FDOT Plans Preparation Manual, July 2001

- FDOT Bicycles and Facilities Planning and Design Manual, April 2000
- Manual of Uniform Minimum Standards for Design, Construction, and Maintenance for Streets and Highways for the State of Florida, July 2000.

The design criteria used in developing the preliminary structural solutions is summarized in Table 2-3.

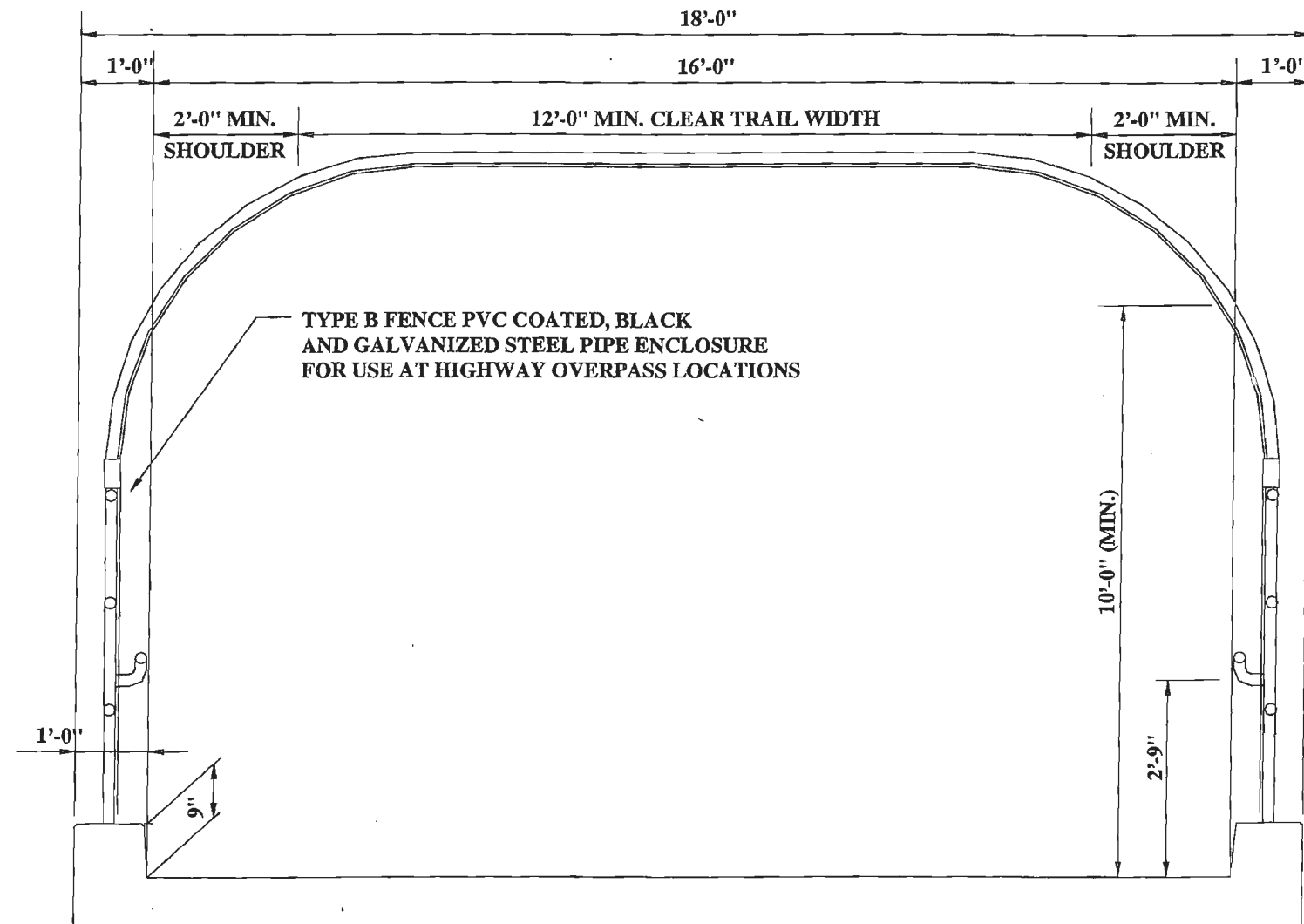
Table 2-3
Design Loads

Dead Loads	
Concrete, Structural:	150 pcf
Structural Steel:	490 pcf
Barrier and Fence:	192 plf
Future Wearing Surface:	15 psf
Stay-in-place Forms:	20 psf
Live Load (Pedestrian)	85 psf
Service Vehicle Load:	H-10
Railing Loads:	AASHTO
Wind Loads:	SDG/AASHTO



Clearance Requirements

Feasible span arrangements and overall bridge length and profile are governed by factors such as economic considerations, hydraulics and clearance requirements. For water crossings, hydraulic considerations generally require the elevation of the low member on the bridge to be at least 2 feet above the design high water elevation and 6 feet above the normal water elevation. Close coordination shall occur with the respective governing agencies for



Typical Section

Vanasse Hangen Brustlin, Inc.

Figure 8

Fred E. Marquis Pinellas Trail
Extension on Florida Power Right-of-Way

Typical Section on Structure

specific clearance criteria over a waterway at any location. The minimum vertical clearance to be maintained is 17 feet- 6 inches over roadway overpasses. Horizontal clearances shall be met as specified in the FDOT Plans Preparation Manual.

■

Span Arrangements – Preferred Alignment

The four structures proposed as a part of the preferred trail alignment are located at I-275, a wetland crossing near Lake St. George Drive, the Cross Bayou Canal, and a drainage canal located just west of the Derby Lane parking area.

I-275 Overpass

The span arrangement alternative identified for this overpass consists of two 160-foot spans for a total bridge length of 320 feet. The span configuration places the Begin and End Bridge outside of the FDOT right-of-way. However, this alternative considers that a pier is placed in the center of the I-275 median. Refer to Plan and Elevation in Figure 9.

Wetland Crossing near Lake St. George Drive

The crossing of this wetland area requires a low-level structure to carry the Trail. Three span configurations have been identified for this crossing, based on viable superstructure alternatives at this location. The three span configuration alternatives yield a total bridge length of 455 feet. The first span arrangement has 13 spans at 35 feet in length. This option involves a composite prestressed flat slab unit superstructure. The second span configuration option consists of 7 spans with a length of 65 feet. The superstructure for this option consists of prestressed inverted tee beams. The third span arrangement considered is five 91-foot long spans. The superstructure for this option consists of Type IV AASHTO beams.

Cross Bayou Canal Crossing

Two alternative span arrangements have been identified for this crossing over the Cross Bayou Canal. The first alternative is a three-span configuration with a center span of 35 feet in length and end spans 32 feet- 6 inches long for a total bridge length of 100 feet. This option is constructed with two intermediate bents within the canal. The second alternative proposes a two-span configuration with 50-foot spans and one intermediate bent in the water. The selection of the span arrangement shall be made based on hydraulic requirements, horizontal and vertical clearance requirements, and optimum structural depth.

Tributary Crossing near Executive Center Drive

This crossing will require a single-span bridge structure with a length of approximately 35 feet. The superstructure alternatives considered for the Tributary crossing include those described under the Short Span Alternatives section.

Drainage Canal Crossing

A viable span arrangement has been identified for this canal crossing. The alternative consists of a three-span configuration with a center span of 45 feet in length and end spans 43 feet- 3 inches long for a total bridge length of 130 feet. This option is constructed with two intermediate bents within the canal. The selection of the span arrangement shall be made based on hydraulic requirements, horizontal and vertical clearance requirements, and optimum structural depth.



Span Arrangements – Alternate Alignment

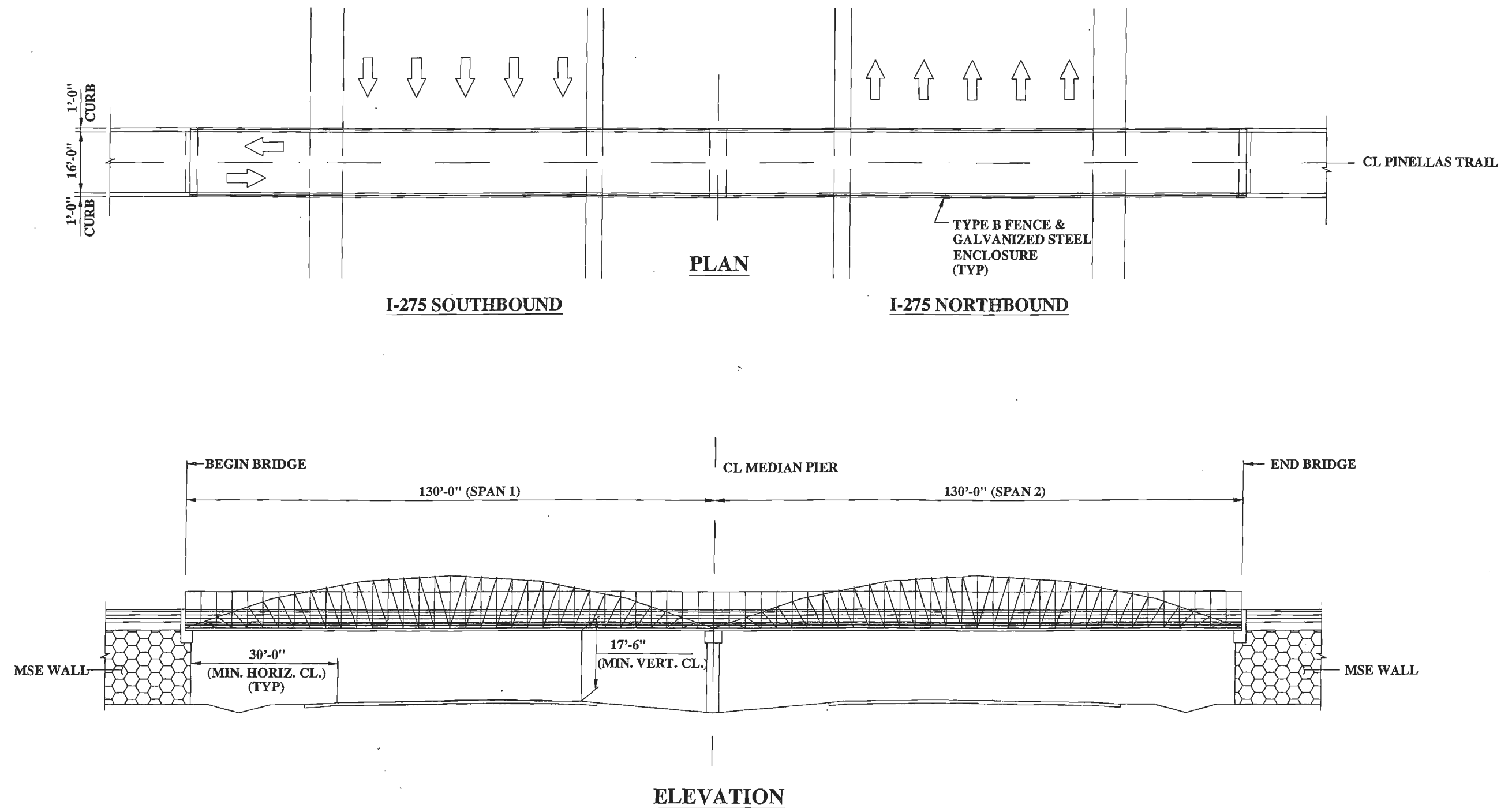
Two additional new structures were considered as part of this Master Plan, but are considered alternates to the preferred at-grade solution presented in the Master Plan section of this report. These two structures would be located over U.S. 19 at Allen’s Creek and over Allen’s Creek just west of U.S. 19.

US 19 at Allen’s Creek Overpass

An alternative to the at-grade crossing of Pinellas Trail at US 19 is an overpass structure. A span configuration for this option was identified based on the US 19 typical section, bridge skew, and minimum horizontal clearance to be maintained, as specified in FDOT criteria. The span configuration consists of two 130-foot spans for a total bridge length of 260 feet. This alternative considers a pier placed in the center of the US 19 median. Refer to Plan and Elevation in Figure 10.

Allen’s Creek Crossing

A low-level crossing, which traverses the mangrove area and Allen’s Creek, is under consideration as an alternative. Three span configurations have been identified for this crossing, based on viable superstructure alternatives at this location. The first span arrangement has 30 spans at 35 feet in length for a total bridge length of 1050 feet. This option involves a composite prestressed flat slab unit superstructure. The second span configuration option consists of 16 spans with a length of 70 feet and a total bridge length of 1050 feet. The

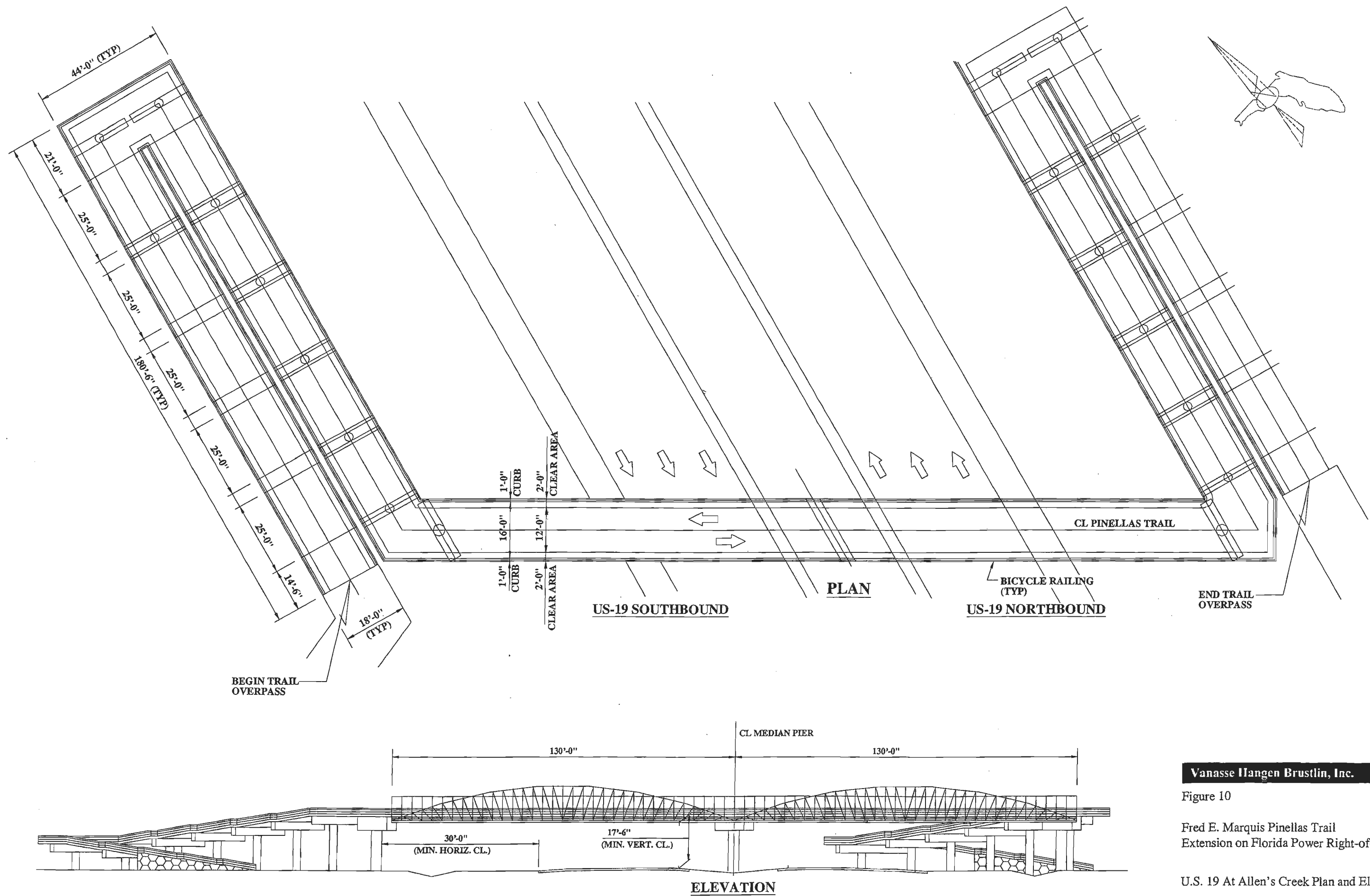


Vanasse Hangen Brustlin, Inc.

Figure 9

Fred E. Marquis Pinellas Trail
Extension on Florida Power Right-of-Way

I-275 Overpass Plan and Elevation



Vanasse Hangen Brustlin, Inc.

Figure 10

Fred E. Marquis Pinellas Trail
Extension on Florida Power Right-of-Way

U.S. 19 At Allen's Creek Plan and Elevation

superstructure for this option consists of prestressed inverted tee beams. The third span arrangement considered is 11-95 foot long spans with a total length of bridge of 1045 feet. The superstructure for this option consists of Type IV AASHTO beams. Refer to the Plan and Elevation in Figure 11.



Superstructure Alternatives

Superstructure alternatives feasible for different span ranges are presented below. Alternatives have been categorized as short span, medium span and long span alternatives. The range of spans included under each category is approximate and serves as a guide in describing the feasible superstructure types for the Trail. During the design phase, further analysis must be performed at each crossing location to determine the appropriate superstructure solution for the selected span arrangement.

Short Span Alternatives

Short span alternatives are generally applied when the span does not exceed 40 feet and the curvature of the structure is such that straight girders can be utilized without excessive cantilever slab variation. In general, short span alternatives include cast-in-place conventionally reinforced concrete slabs, precast prestressed concrete slab units, prestressed double-tees, precast prestressed concrete beams supporting a composite cast-in-place deck such as AASHTO or Florida Bulb Tee sections, inverted tee beams, and Florida U beams. Please refer to Figure 12 for short span alternatives considered.

Cast-in-Place Concrete Flat Slabs

This superstructure represents the most common short-span bridge solution currently used in Florida. The superstructure section is a conventionally reinforced constant depth slab, cast-in-place on shoring. For multiple span structures, continuity of

the slab is provided. Contractors are familiar with construction techniques and the structures have an excellent maintenance and durability record. While cast-in-place concrete flat slabs are capable of spanning up to 50 feet, it has been found that it can do so economically up to 30 feet for simply supported spans and 40 feet for continuous spans. Based on deflection criteria, the minimum depth of slab recommended for a 50 feet continuous span is 2 feet. At this depth of slab, other solutions may become more competitive based on depth and weight of superstructure. Cast-in-place concrete flat slabs readily accommodate future widening. However, field forming for placement of concrete slab over a waterway presents a construction issue, since temporary supports would likely be needed in the canal resulting in slower construction and higher labor construction costs. Therefore, the cast-in-place alternative is considered viable for canal crossings, but has not been considered for further analysis in wetland crossings.

Precast Concrete Flat Slab Units

Like the cast-in-place flat slabs, precast concrete units are ideal for low-level bridges where shallow superstructures are necessary. The precast flat slab considered is a solid, constant depth panel, pretensioned longitudinally. It can be used at any location where the cast-in-place alternative is considered. A composite topping on the precast units continuous over supports should be used to ensure a smooth riding surface on the bridge.

Advantages in the use of this system include eliminating the need of erecting forms over the waterway. Due to the continuity of the topping over the supports, the open joints over supports are eliminated. This composite structure offers the same appearance of the cast-in-place slab, with the advantages prestressing offers. The depth of structure for this system utilizing a 12-inch precast unit and a 6-inch concrete topping would be 18 inches. Therefore, this alternative is competitive for bridges with vertical clearance restrictions requiring

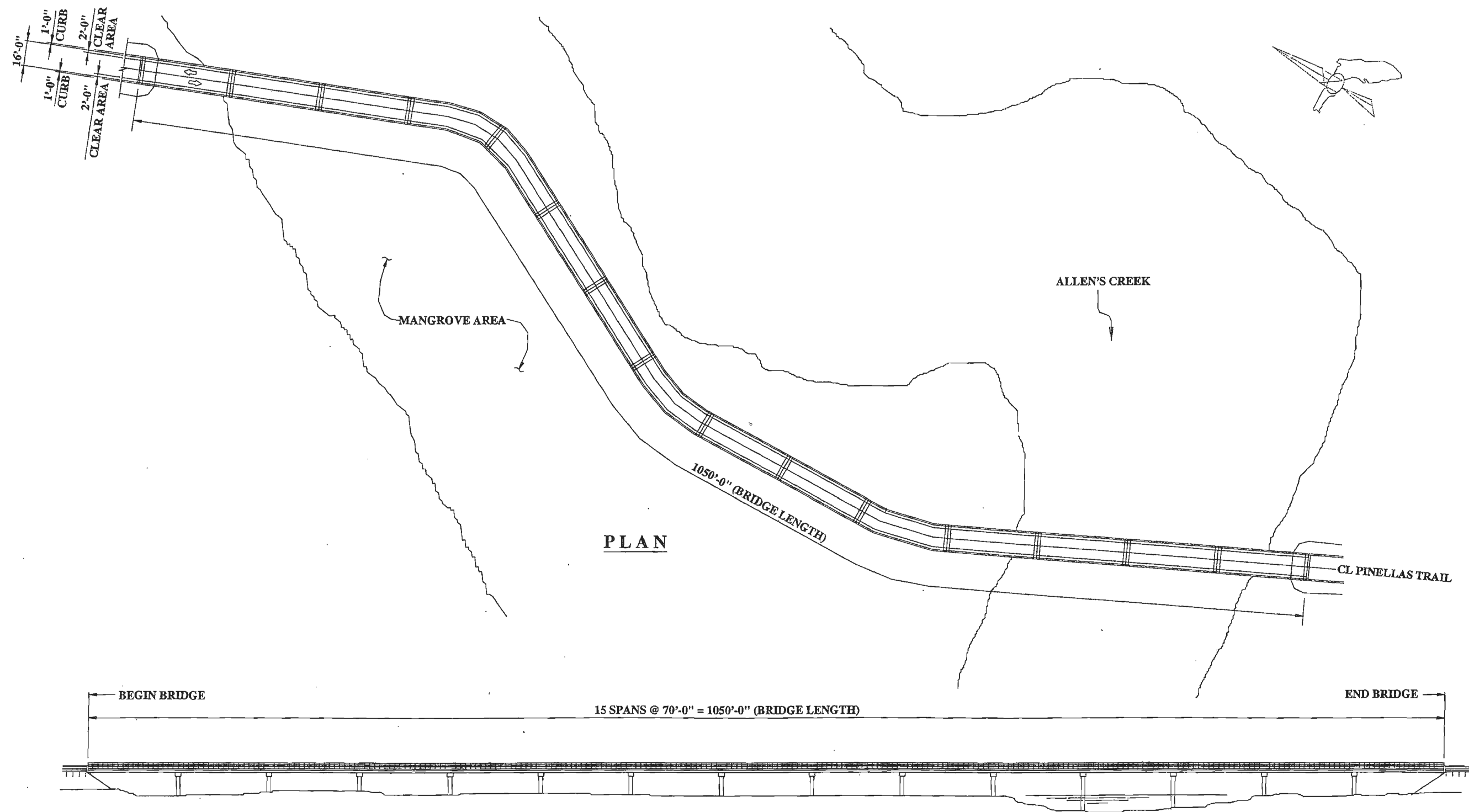
a shallow structural depth. The composite prestressed flat slab units should be considered as a viable alternative along Pinellas Trail for crossings with spans less than 40 feet long.

Prestressed Double-Tees

The Florida Double-Tees are available in 18-, 24-, and 30-inch stem sections with 7-inch flange widths. These sections have maximum practical span limits of 39-40 feet, 48-50 feet and 59-61 feet, respectively. The features of design are much like the precast flat slab since the double-tee panels are precast and longitudinally pretensioned. The use of a concrete topping is recommended to ensure a smooth riding surface for bicycles.

Inverted Tee Beams

The Inverted Tee System (IT) is composed of a pretensioned, prestressed concrete inverted tee beam with a 6-inch cast-in-place deck. The IT system has been designed for short to medium span bridges, however, its use is new in Florida. These girders are simple to produce and light to handle. This system may be used as an alternative to cast-in-place concrete deck slabs because of their excellent span-to-depth ratios (as low as 1/36) and the fact that the system requires no forming or falsework as compared to slab bridges. The system offers the advantages of a precast section, while maintaining the same look as a cast-in-place slab bridge. The ideal applications for inverted tee beams include locations with vertical clearance restrictions where the superstructure depth needs to be kept to a minimum, where the erection of shoring needs to be minimized due to site restrictions, and in locations where continuity over the supports is desired using precast construction. Continuity prior to casting of deck slab is recommended in cases of multiple spans. The IT-Girder system has the added advantage that it can be easily accommodated to a staged construction. The IT system minimizes construction



SPAN ARRANGEMENT ALTERNATIVES:

- OPTION 1 :
30 SPANS @ 35' WITH COMPOSITE PRESTRESSED FLAT SLAB SUPERSTRUCTURE.
- OPTION 2 :
15 SPANS @ 70' WITH PRESTRESSED INVERTED TEE BEAMS.
- OPTION 3 :
11 SPANS @ 95' WITH TYPE IV AASHTO BEAMS.

ELEVATION ALONG CL BRIDGE

Vanasse Hangen Brustlin, Inc.

Figure 11
Fred E. Marquis Pinellas Trail
Extension on Florida Power Right-of-Way
Allen's Creek Plan and Elevation

configuration option consists of 16 spans with a length of 70 feet with a superstructure involving the use of 20-inch prestressed inverted tee beams and a composite 6-inch concrete slab for a total structural depth of 26 inches.

This bridge is mostly crossing a mangrove area. Generally, this type of site is considered a corrosive environment. Therefore, the structural alternative selected for this location must be carefully chosen to take into account the site conditions. Concrete elements are usually more resistant to this type of environment and require less maintenance than steel solutions.

During the construction of the bridge, special consideration should be implemented to ensure this area is not adversely affected. Disturbance to the mangroves must be kept to a minimum. Construction staging may prove difficult on this site, particularly considering the length of bridge necessary for this crossing. Therefore, it is recommended that the bridge designer consider the advantages of top-down construction for this application, where the bridge is built progressively from the structure's deck, and select a structural alternative and a design solution that may easily accommodate this type of construction process.

Although this bridge is considered a low-level structure, the bridge crosses underneath transmission power lines. Therefore, the power lines will impose constraints on the type of structure selected and on the construction of the bridge. Close utility coordination during construction is of utmost importance. If possible, overhead transmission power lines should be de-activated during lifting operations. The following are restrictions to when the overhead transmission lines related to the NC circuit, known as the Ulmerton - East Clearwater Substation 230kv circuit, can be de-energized:

- Avoid time period of June - September any year

- Avoid time period of June - September and February - March during year 2004.

Use of Retaining Walls

An alternative to constructing the approach ramps at some crossing locations is the use of permanent retaining walls. The use of retaining walls reduces the total bridge length, and therefore, its cost. The most common retaining wall alternatives considered are cast-in-place concrete cantilevered walls and mechanically stabilized embankment (MSE) walls. Site and geotechnical considerations, economics, constructability, and aesthetics should be considerations in determining the viable alternative of a permanent retaining wall system.

The Structures Design Guidelines indicates that MSE walls are generally the most economical walls when the exposed surface area of the walls exceeds 1076 square feet, the total wall height is greater than 10 feet, and sufficient room for the earth reinforcement system is available. In addition, MSE walls tolerate greater differential settlements than cast-in-place walls. However, during the design phase of a bridge, a Wall Justification Report should be completed. A wall evaluation and a cost analysis should be performed at this stage, taking into account all considerations described above as well as the recommendations included in the geotechnical engineering report.

Drainage

Pinellas County Drainage Basin Studies that include the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way corridor are illustrated on Figure 14. While development has occurred around the Florida Power R/W since the completion of these studies, the pre- and post-development runoff and drainage patterns are basically intact as

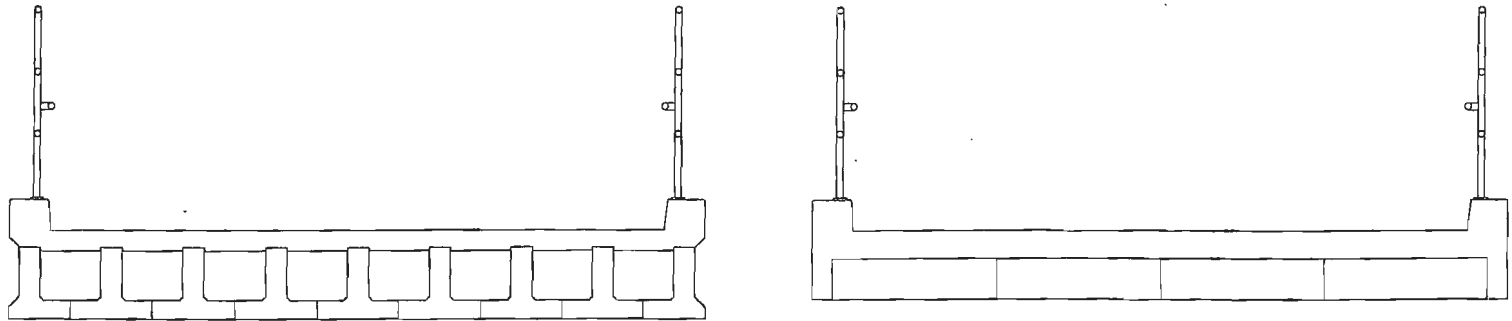
required by regulations. The progression of the trail from north to south through the various drainage basins and their associated sub basins is detailed on Table 2-4, Flood Study Data. This table details the pertinent basin name, size, Flood Study map number, sub-basin number, sub-basin size, channel crossings, and other features of note in the remarks column. Each study (report) has been compiled into a two-volume set and provides detailed hydrologic and hydraulic characteristics of each basin (both "existing" and projected future discharges).

The trail alignment has been plotted on FEMA Flood Insurance Rate Maps (FIRM) for the entire corridor to determine any flood zone or floodway encroachments that might occur. The trail is predominantly an at-grade facility and will have no significant impact on the natural drainage patterns of the basins it traverses. Therefore, no significant flood zone or floodway encroachment is anticipated. These working documents are part of the project file.

Design Criteria

Design standards of water crossings where flood plain encroachment is a consideration as follows:

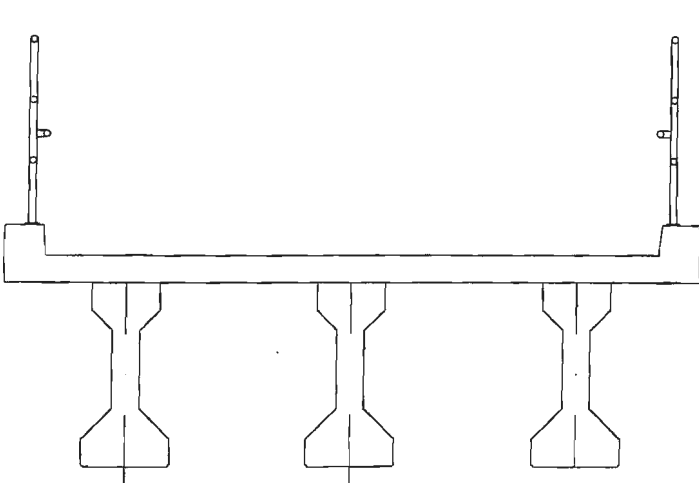
- A 25-year design flood frequency will be used except where the trail structure parallels a roadway bridge crossing a highway (such as at US 19 over Allen's Creek), where a 50-year



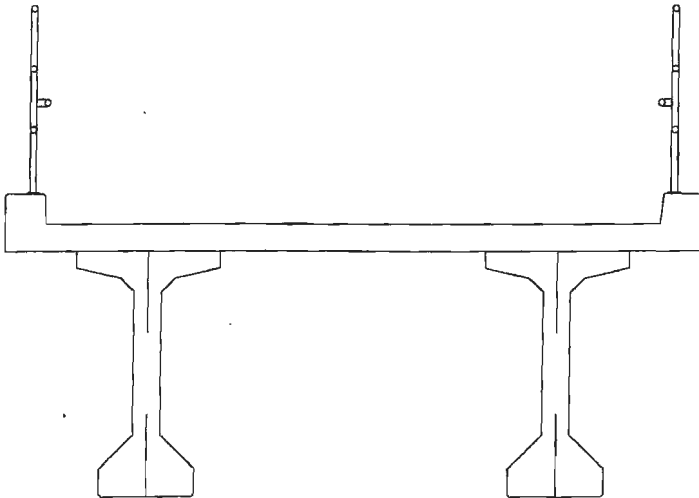
INVERTED TEE BEAMS

COMPOSITE PRESTRESSED FLAT SLAB UNITS

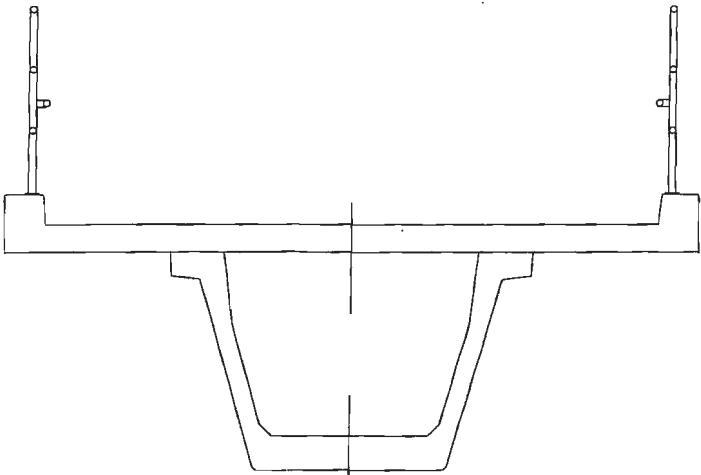
SHORT SPAN ALTERNATIVES



AASHTO TYPE IV PRESTRESSED BEAMS



AASHTO TYPE IV PRESTRESSED BEAMS



FLORIDA U BEAM

MEDIUM SPAN ALTERNATIVES

Vanasse Hangen Brustlin, Inc.

Figure 12

Fred E. Marquis Pinellas Trail
Extension on Florida Power Right-of-Way

Short and Medium Span Alternatives

time and is more economical than cast-in-place solutions due to the overall lighter superstructure. The weight of the IT beams with a width of 24 inches and depths ranging from 12 inches to 24 inches is between 178 to 256 lb./ft, respectively, which may be compared to the weight of a 14-inch x 14-inch precast concrete pile. This innovative alternative is a particular advantage for the crossings along the Trail since contractors do not require heavy lifting equipment. However, a disadvantage to the inverted tee-beams at this time is their limited availability in Florida. Currently, only the 20-inch section has been produced in the state. This section was used successfully for the SR 804 Bridge Replacement over the Intracoastal Waterway in Boynton Beach. The FDOT Bridge Development Report Cost Estimating Guidelines indicate that due to limited availability of these beams, the provided cost per linear foot should be escalated to account for new formwork. Therefore, proposed span configurations presented in this study using this system have only considered the 20-inch section with a 6-inch cast-in-place concrete slab on top.

Medium Span Alternatives

Medium span alternatives are generally applied when the span does not exceed 150 feet and the curvature of the structure is such that straight girders can be utilized without excessive cantilever slab variation. In general, medium span alternatives include precast prestressed concrete beams supporting a composite cast-in-place deck such as AASHTO or Florida Bulb Tee sections and Florida U beams. Other solutions include steel alternatives such as steel plate girders or rolled wide flange beams. Steel rolled wide flange beams may be considered a viable solution for medium spans where the spans do not exceed about 115 feet. While steel superstructures are generally lighter than concrete solutions, higher maintenance costs for steel structures as well as environmental considerations should be taken into account when selecting the

most feasible alternative. Further analysis should be performed during the early design stage to determine the most appropriate solution at each site. Please refer to Figure 12 for medium span alternatives considered in this master plan.

Precast Prestressed Concrete AASHTO Beams

Precast Prestressed Concrete AASHTO beams are one of the most commonly used superstructure types in Florida. The relative economy, ease of erection, low maintenance and acceptable aesthetics make these beams a viable alternative for this project. The Type II AASHTO beam section accommodates spans from 40 to 50 feet with a depth of 36 inches. The Type IV AASHTO beam section with a depth of 54 inches is utilized for spans ranging from 70 feet to 100 feet in length. The Type V AASHTO beam section accommodates spans from 90 feet to 120 feet and has a depth of 63 inches. In addition, a simply supported Type VI beam, with a depth of 72 inches, is economically and structurally viable for spans up to 140 feet in length. While using higher strength concrete allows for the possibility of longer spans, especially in pedestrian bridges where the loads are considerably lower than in highway bridges, its use is not typical. In addition, precast members longer than about 150 feet present limitations in transportation. The longer precast members could be transported in segments, but post-tensioning of precast members would then be required, thus complicating the bridge construction and increasing its cost. The general description of span alternatives in this study attempts to provide a range of superstructure alternatives typically used for different span lengths. During the design phase, each structure must be analyzed independently to determine the most appropriate structural system and construction method for each site.

Florida U Beams

The Florida U beams are available in four sections from 48 inches to 64 inches in depth. These prestressed concrete sections span up to 157 feet, thus being competitive with I-girder solutions. This

superstructure type is a viable solution for the highway overpasses along the Trail since their efficiency may reduce the superstructure depth as compared to an AASHTO beam. In addition, this alternative is considered a cost-effective solution for the favorable level of aesthetics it provides.

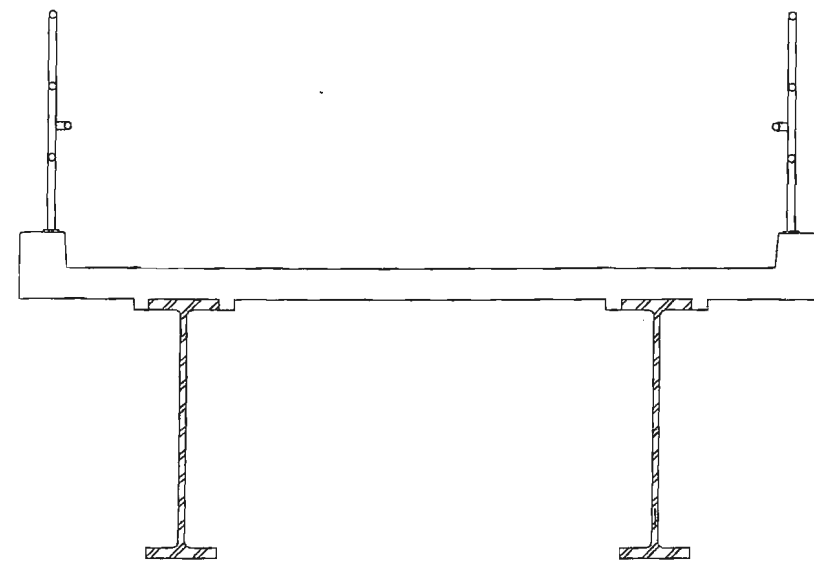
Long Span Alternatives

Long span steel alternatives are generally applied where medium span alternatives are not feasible. Long span steel alternatives include plate and box girders supporting a composite cast-in-place concrete slab, as well as open truss superstructures. Other long span solutions involve post-tensioned concrete sections, including haunch beams and modified Florida Bulb Tees. However, this superstructure type has not been considered for further analysis due to the higher maintenance involved in post-tensioned elements, the typically higher construction costs and the added challenges during installation. Please refer to Figure 13 for long span alternatives considered.

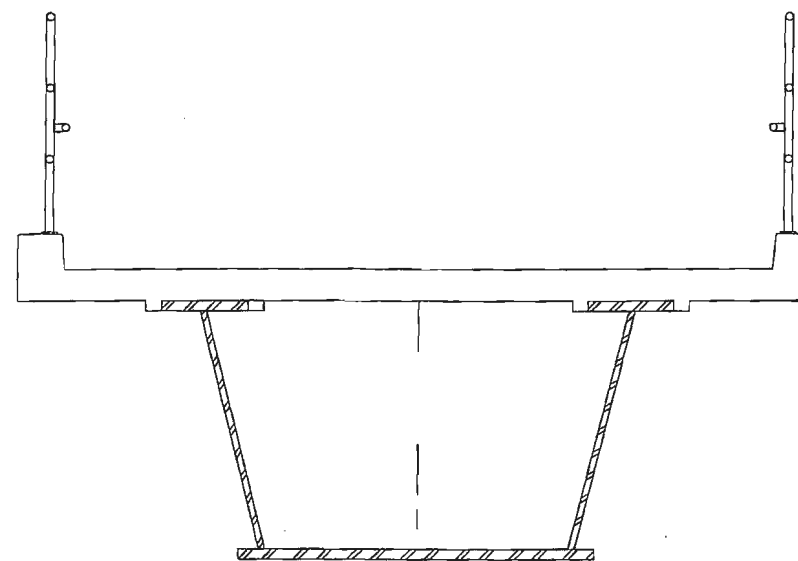
Steel Plate Girders

Steel plate girders are lighter than concrete alternatives. For the span configurations considered, either a simply supported or a continuous steel girder superstructure may be used. The use of continuous plate girders yields less steel quantities than the use of simply supported girders. However, construction of the continuous plate girder alternative would require the use of falsework for the field splicing of the members.

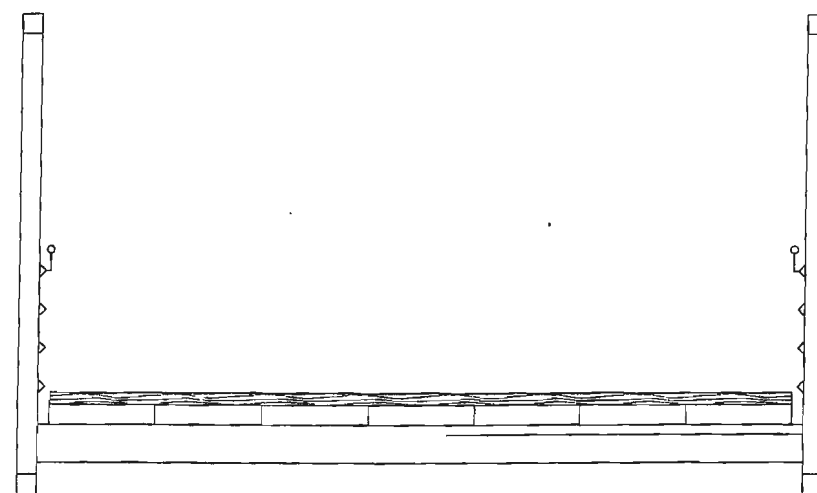
The advantage associated with the simply supported steel girder alternative is that it is simpler to construct since it does not require the use of temporary falsework, additional maintenance of traffic to accommodate the falsework, or field splicing of the plate girders. However, structural depth for the simply supported steel girder structure



CONTINUOUS STEEL PLATE GIRDER



CONTINUOUS STEEL BOX GIRDER



STEEL TRUSS

Vanasse Hangen Brustlin, Inc.

Figure 13

Fred E. Marquis Pinellas Trail
Extension on Florida Power Right-of-Way

Long Span Alternatives

is greater than that of the continuous steel alternative, thus requiring a higher profile to meet the bridge minimum vertical clearance over the roadway. In addition, the steel quantities for the simply supported alternative are higher than those of the continuous steel girder alternative. It is recommended that both the simply supported and continuous steel plate girder options be further evaluated during the Bridge Development Report phase.

A disadvantage associated with any configuration of steel superstructure types is the higher maintenance required for steel over concrete. The higher cost to maintain steel structures must be included in the evaluation of superstructure alternatives when analyzing a steel versus a concrete structure during the design phase.

Steel Box Girders

The steel box girder alternative is similar to the steel plate girder option. However, the box girders offer an aesthetic solution to situations where plate girders are applicable. For the bridge typical section being considered, only one continuous box girder would be required.

Prefabricated Steel Open Truss

The steel open truss offers an alternative to the other steel options described above. Pedestrian and bicycle bridges are an excellent application for the steel open truss system. An advantage to this system over the plate girder and box girder options, especially in areas with vertical clearance constraints, is that a shallower profile may be developed. These prefabricated bridges are delivered to the site in sections and are then field spliced. The favorable aesthetics, the possibility it offers to lower the vertical profile and the lightweight quality of the structure makes this system a viable alternative for this project.



Structural Site Evaluation – Preferred Alignment

Advantages and disadvantages of each superstructure system have been previously discussed. The selection of a superstructure for each bridge located along the Extension of the Pinellas Trail shall be conducted at the Bridge Development Report (BDR) stage during the design phase, once the final alignment and profile have been established. The selection shall be based on bridge cost, aesthetics, site conditions and limitations, and constructability. Limitations on span lengths due to site constraints and constructability include wetland crossings. The span configurations proposed in this study for all wetland crossings fall under the short or medium span alternatives since constructability will likely dictate the selection of the superstructure alternative at these locations.

I-275 Overpass

The feasible superstructure alternatives for the span configuration identified for this location (two 160-foot spans) include those described in the Long Span Alternatives section. Continuous steel plate girders or a steel box girder with an approximate structural depth of 5 feet-3 inches, or a prefabricated open truss bridge may be used at this location.

Overhead transmission lines run parallel to the bridge alignment on both sides of the structure. These lines impose constraints on the vertical clearance and height of the structure, as well as on construction activities. Therefore, the depth of structure should be a major consideration in the selection of a superstructure type, and the chosen span configuration should minimize conflicts with existing utilities. In addition, constructability of the bridge must be addressed during the design phase. It is recommended that drilled shafts be considered as a foundation alternative at this location to avoid conflicts near the overhead transmission lines. In

any case, the overhead transmission lines should be de-energized during required lifting operations if possible. The following are restrictions to when the overhead transmission lines related to the NC circuit, known as the Ulmerton-Northeast Substation 230kv circuit, can be de-energized:

- Avoid time period of June to September any year
- Avoid time period of June to November during year 2003
- Avoid time period of June to September and February to March during year 2004

Due to site obstructions near the approach ramps to the bridge, permanent retaining walls are recommended to construct the approach ramps leading to the overpass structure.

Wetland Crossing near Lake St. George Drive

The span arrangements discussed above for this location have been established based on the possible use of several superstructure systems. The option for the first span arrangement, 13 spans at 35 feet in length, involves a composite prestressed flat slab unit superstructure. The total superstructure depth for this option is 18 inches. The second option, which considers 7-65 foot long spans, involves the use of 20-inch prestressed inverted tee beams and a composite 6-inch concrete slab for a total structural depth of 26 inches. The third span configuration option consists of 5 spans with a length of 91 feet with a superstructure involving the use of Type IV AASHTO beams with an approximate structural depth of 5 feet-9 inches.

This bridge is crossing a wetland area. Generally, this type of site is considered a corrosive environment. Therefore, the structural alternative selected for this location must be carefully chosen to take into account the site conditions. Concrete elements are usually more resistant to this type of

environment and require less maintenance than steel solutions.

As with the Allen’s Creek Bridge, it is recommended that during the design phase a more detailed analysis is conducted to determine whether top-down construction of this bridge is a competitive solution. The structure and span configuration selected should reflect the constructability of the bridge.

Cross Bayou Canal Crossing

The span arrangements proposed for this crossing have been identified based on the possible use of several superstructure systems. The first option considering a three-span structure may be constructed using a continuous cast-in-place concrete slab with a depth of 18 inches, precast prestressed concrete composite slab units also with a structural depth of 18 inches, or 20-inch inverted tee beams for a total superstructure depth of 26 inches. Prestressed double-tees are also a viable alternative for this crossing. The two-span alternative with 50-foot spans may use Type II AASHTO beams as a superstructure option.

Construction of this crossing will occur directly underneath overhead power lines, so constraints on the type of structure selected and on the construction of the bridge will be imposed. Close utility coordination during construction is of utmost importance. If possible, overhead transmission power lines should be de-activated during lifting operations. The selection of the superstructure and substructure types must take constructability of the bridge into consideration. For this reason, driving of piles for the foundation is not recommended at this location. Drilled shafts as a foundation system are recommended at this location.

Tributary Crossing near Executive Center Drive

This crossing will require a single-span bridge structure with a length of approximately 35 feet. The superstructure alternatives considered for the Tributary crossing include those described under the Short Span Alternatives section.

Drainage Canal Crossing

The three-span arrangement proposed for this crossing has been identified based on the possible use of several superstructure systems and the total length of bridge required. As discussed previously, three spans are proposed with a center span of 45 feet and two end spans of 43 feet- 3 inches. This structure may be constructed using precast prestressed concrete composite slab units with a structural depth of 18 inches, 20-inch inverted tee beams for a total superstructure depth of 26 inches, prestressed double-tees, or Type II AASHTO beams with a total superstructure depth of approximately 50 inches. Determination of structural type shall be made based on minimum horizontal and vertical clearances, trail profile, and hydraulic requirements.



Structural Site Evaluation – Alternate Alignment

US 19 at Allen’s Creek Overpass

The superstructure alternatives feasible for the span configuration identified for this location (two 130-foot spans) include all those described in the Medium Span or Long Span Alternatives section. Prestressed AASHTO Type VI beams with an approximate structural depth of 7 feet-3 inches, Florida U 54 beams with an approximate depth of structure of 5 feet-6 inches, continuous steel plate girders or a steel box girder with an approximate

structural depth of 4 feet-6 inches, or a prefabricated open truss bridge may be proposed at this location.

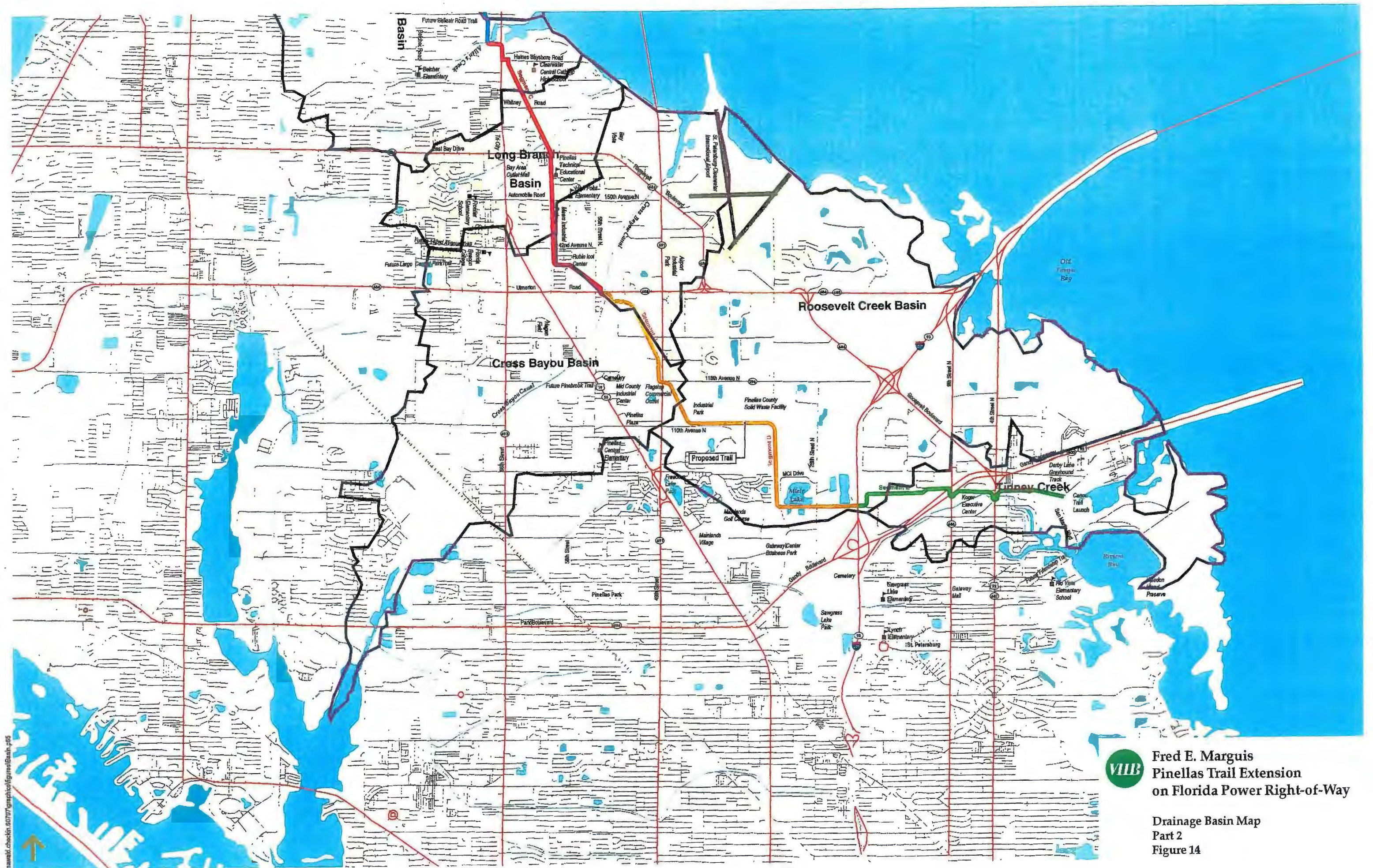
Two transmission towers are located on the west side of the proposed bridge crossing. One trail alignment alternative has approach ramps to the bridge crossing under the overhead power lines. Minimum vertical clearance to the power lines must be maintained at this location. Therefore, the use of structural access span switches may be necessary to clear the overhead transmission lines.

Similar to the I-275 overpass, the chosen span configuration should minimize conflicts with existing utilities. Also, constructability of the bridge must be addressed during the design phase. It is recommended that drilled shafts also be considered as a foundation alternative at this location to avoid conflicts in the vicinity of the overhead transmission lines. It is recommended that overhead transmission lines near the structure be de-energized during required lifting operations. The following are restrictions to when the overhead transmission lines related to the NC circuit, known as the Ulmerton - East Clearwater Substation 230kv circuit, can be de-energized:

- Avoid time period of June - September any year
- Avoid time period of June - September and February - March during year 2004.

Allen’s Creek Crossing

The span arrangements discussed above for this location have been selected based on the possible use of several superstructure systems. The option for the first span arrangement, 30 spans at 35 feet in length, involves a composite prestressed flat slab unit superstructure. The total superstructure depth for this option is 18 inches. The second option, considers 11 95-foot long spans, involves the use of Type IV AASHTO beams with an approximate structural depth of 5 feet-9 inches. The third span



VIIB Fred E. Marguis
Pinellas Trail Extension
on Florida Power Right-of-Way

Drainage Basin Map
Part 2
Figure 14

**PINELLAS TRAIL - FLORIDA POWER EXTENSION
FLOOD STUDY DATA**

Table 2-4
Reference: Pinellas County Storm Drainage Basin Studies and Flood Study Maps

DATE: 10/4/01

Flood Study Map No.	Basin Name (s)	Total Basin Area (acres)	Sub Basin No.(s)	Sub Basin Area (acres)	Flood Study Report	Major Rdwy	Drainage Crossings	Corresponding Plan Sht No.	Remarks / Surrounding Development
SEGMENT A - JOHN CHESNUT PARK (BEGIN PROJECT) TO US 19 AT ENTERPRISE ROAD									
S1/2 33-27-16	Lake Tarpon	14,325	03-27-01	421.24	Lake Tarpon (Basin No. 3)	East Lake Road		1	Lake Tarpon - DHW (25yr) El. =4.5
N1/2 4-28-16	Lake Tarpon	14,325	03-27-01	421.24	Lake Tarpon (Basin No. 3)	East Lake Road		1	Lake Tarpon - DHW (25yr) El. =4.5
N1/2 4-28-16	Brooker Creek	22,455	04-01-01	107.00	Brooker Creek (Basin No. 4)	East Lake Road	Brooker Creek	1 & 2	Limited Development ; Conc. Bridge Structure @ East Lake Rd.; Main outfall - Channel A
S1/2 4-28-16	Brooker Creek	22,455	04-01-01	107.00	Brooker Creek (Basin No. 4)	East Lake Road		2	Suburban Housing Development (Boot Ranch)(6000 acres) ; Stormwater Treatment Pond
S1/2 4-28-16	Brooker Creek	22,455	04-02-02	279.00	Brooker Creek (Basin No. 4)	East Lake Road		2 & 3	Suburban Housing Development (Boot Ranch)(6000 acres) ; Stormwater Treatment Ponds
S1/2 4-28-16	South Creek	2,892	06-01-03	235.30	South Creek (Basin No. 6)	East Lake Road		2 & 3	Main Outfall Sub-basin; Lake Tarpon Canal(100yr.discharge=725 cfs); Suburban Housing Development
N1/2 9-28-16	Brooker Creek	22,455	04-02-02	52.00	Brooker Creek (Basin No. 4)	East Lake Road		3	Main Outfall Sub-basin; Lake Tarpon Canal(100yr.discharge=725 cfs); Suburban Housing Development
N1/2 9-28-16	South Creek	2,892	06-01-03	235.30	South Creek (Basin No. 6)	East Lake Road		3	Main Outfall Sub-basin; Lake Tarpon Canal(100yr.discharge=725 cfs); Suburban Housing Development
N1/2 9-28-16	South Creek	2,892	06-03-01	125.11	South Creek (Basin No. 6)	East Lake Road		3 & 4	Main Outfall Sub-basin; Lake Tarpon Canal(100yr.discharge=725 cfs); Mixed Residential/Commercial Development
S 1/2 9-28-16	South Creek	2,892	06-03-01	125.11	South Creek (Basin No. 6)	S.R. 584		4	Main Outfall Sub-basin; Lake Tarpon Canal(100yr.discharge=725 cfs); Mixed Residential/Commercial Development
S 1/2 9-28-16	South Creek	2,892	06-01-02	105.60	South Creek (Basin No. 6)	S.R. 584	Lake Tarpon Canal	4	Main Outfall Sub-basin; Lake Tarpon Canal(100yr.discharge=725 cfs); Commercial Development
S 1/2 9-28-16	South Creek	2,892	06-04-02	254.81	South Creek (Basin No. 6)	S.R. 584		4 & 5	Main Outfall Sub-basin; Lake Tarpon Canal(100yr.discharge=725 cfs); Mixed Residential/Commercial Development; (2) Wetlands Crossings
S 1/2 8-28-16	South Creek	2,892	06-01-02	105.60	South Creek (Basin No. 6)	Lake St. George Drive		5 & 6	Main Outfall Sub-basin; Lake Tarpon Canal(100yr.discharge=725 cfs); Mixed Residential/Commercial Development; (2) Wetlands Crossings
S 1/2 8-28-16	South Creek	2,892	06-04-02	254.81	South Creek (Basin No. 6)	Lake St. George Drive		5 & 6	Main Outfall Sub-basin; Lake Tarpon Canal(100yr.discharge=725 cfs); Mixed Residential/Commercial Development; (2) Wetlands Crossings
S 1/2 8-28-16	South Creek	2,892	06-08-01	106.51	South Creek (Basin No. 6)	Lake St. George Drive		6	Possible Outfall - Lake St. George (W.E. = 12.2) Residential Development
N1/2 17-28-16	South Creek	2,892	06-08-01	106.51	South Creek (Basin No. 6)	S.R. 586		6 & 7	Substantial Residential Development
N1/2 17-28-16	South Creek	2,892	06-02-01	182.50	South Creek (Basin No. 6)	S.R. 586		6 & 7	Substantial Residential Development
N1/2 17-28-16	South Creek	2,892	06-02-02	121.67	South Creek (Basin No. 6)	S.R. 586		6 & 7	Substantial Residential Development
S1/2 17-28-16	South Creek	2,892	06-02-02	121.67	South Creek (Basin No. 6)	Curlew Road		7	Substantial Residential Development - Stormwater Treatment Pond
S1/2 17-28-16	Possum Branch	1,974	11-06-01	140.00	Possum Creek (Basin No. 11)	Curlew Road		7 & 8	Steeply sloped terrain; Substantial Residential Development; most of basin does not have defined outfall
N1/2 20-28-16	Possum Branch	1,974	11-06-01	140.00	Possum Creek (Basin No. 11)	Countryside Blvd.		8	Steeply sloped terrain; Substantial Residential Development; most of basin does not have defined outfall
N1/2 20-28-16	Curlew Creek	6,800	10-05-02	237.59	Curlew Creek (Basin No. 10)	Countryside Blvd.		8 & 9	Substantial Residential Development
S1/2 20-28-16	Curlew Creek	6,800	10-05-02	237.59	Curlew Creek (Basin No. 10)	Countryside Blvd.	Existing Cross Drain	9	Substantial Residential Development
S1/2 20-28-16	Curlew Creek	6,800	10-04-03	154.95	Curlew Creek (Basin No. 10)	Countryside Blvd.	Existing Cross Drain	9 & 10	Substantial Residential Development
N1/2 30-28-16	Curlew Creek	6,800	10-04-01	274.32	Curlew Creek (Basin No. 10)	Summerdale Drive		11	Substantial Residential Development
N1/2 30-28-16	Curlew Creek	6,800	10-04-02	195.13	Curlew Creek (Basin No. 10)	S.R. 580		11 & 12	Substantial Residential Development
N1/2 30-28-16	Curlew Creek	6,800	10-04-02	195.13	Curlew Creek (Basin No. 10)	Countryside Blvd.		12	Substantial Residential / Commercial Development; Wetland Crossing
N1/2 30-28-16	Curlew Creek	6,800	10-04-01	274.32	Curlew Creek (Basin No. 10)	Countryside Blvd.		12	Substantial Residential / Commercial Development; Wetland Crossing
S1/2 30-28-16	Curlew Creek	6,800	10-02-11	81.49	Curlew Creek (Basin No. 10)	Countryside Blvd.		12 & 13	Substantial Residential / Commercial Development; Wetland Crossing
SEGMENT B - US 19 AT ENTERPRISE ROAD TO ALLEN'S CREEK @ US 19									
N1/2 31-28-16	Curlew Creek	6,800	10-02-11	81.49	Curlew Creek (Basin No. 10)	U.S 19		13	Substantial Residential / Commercial Development
N1/2 31-28-16	Curlew Creek	6,800	10-02-10	283.51	Curlew Creek (Basin No. 10)	U.S 19	Channel B	13 & 14	Substantial Residential / Commercial Development; Channel B; has experienced substantial flooding.
N1/2 31-28-16	Alligator Creek	5,686	14-02-04	367.30	Alligator Creek (Basin No. 14)	U.S 19		14	Varying terrain with residential development
S1/2 31-28-16	Alligator Creek	5,686	14-03-02	207.75	Alligator Creek (Basin No. 14)	U.S 19		14 & 15	Varying terrain with residential development
S1/2 31-28-16	Curlew Creek	6,800	10-02-10	283.51	Curlew Creek (Basin No. 10)	U.S 19		14 & 15	Varying terrain with residential development
N1/2 6-29-16	Alligator Creek	5,686	14-03-02	207.75	Alligator Creek (Basin No. 14)	U.S 19		15	Substantial Residential / Commercial Development / (2) Stormwater treatment ponds adjacent to alignment; Wetland Crossing
N1/2 6-29-16	Alligator Creek	5,686	14-03-01	166.43	Alligator Creek (Basin No. 14)	U.S 19		15	Substantial Residential / Commercial Development / (2) Stormwater treatment ponds adjacent to alignment; Wetland Crossing
N1/2 6-29-16	Alligator Creek	5,686	14-09-01	187.09	Alligator Creek (Basin No. 14)	U.S 19		15 & 16	Substantial Residential / Commercial Development / (2) Stormwater treatment ponds adjacent to alignment; Wetland Crossing
S1/2 6-29-16	Alligator Creek	5,686	14-09-01	187.09	Alligator Creek (Basin No. 14)	U.S 19		15 & 16	Substantial Residential / Commercial Development / (2) Stormwater treatment ponds adjacent to alignment; Wetland Crossing
S1/2 6-29-16	Alligator Creek	5,686	14-10-01	121.67	Alligator Creek (Basin No. 14)	U.S 19		16	Mixed Residential / Commercial Development; Wetland Crossing
S1/2 6-29-16	Alligator Creek	5,686	14-03-01	229.56	Alligator Creek (Basin No. 14)	U.S 19		16	Mixed Residential / Commercial Development; Wetland Crossing
N1/2 7-29-16	Alligator Creek	5,686	14-10-01	121.67	Alligator Creek (Basin No. 14)	U.S 19		16 & 17	Mixed Residential / Commercial Development ; Stormwater pond on west side / pond or lake on eastside
N1/2 7-29-16	Alligator Creek	5,686	14-01-05	59.69	Alligator Creek (Basin No. 14)	U.S 19	Alligator Creek	17	Mixed Development ; Phillis Baseball Complex ; Channel A downstream under US 19 (3) 8' X 8' Box Culverts
S1/2 7-29-16	Alligator Creek	5,686	14-01-05	59.69	Alligator Creek (Basin No. 14)	U.S 19		17 & 18	Mixed Development ; Phillis Baseball Complex ; Channel A downstream under US 19 (3) 8' X 8' Box Culverts
S1/2 7-29-16	Alligator Creek	5,686	14-06-02	137.74	Alligator Creek (Basin No. 14)	U.S 19		18	Mainly Commercial Development and Park/ Recreational Development
N1/2 18-29-16	Coastal Zone 2	896	20-00-02	95.00	Coastal Zone II	U.S 19		18 & 19	Mixed Residential / Commercial Development; St. Petersburg JC Campus
N1/2 18-29-16	Coastal Zone 2	896	20-00-01	135.00	Coastal Zone II	U.S 19		19	Mixed Residential / Commercial Development; St. Petersburg JC Campus
S1/2 18-29-16	Coastal Zone 2	896	20-00-01	135.00	Coastal Zone II	U.S 19		19 & 20	Mainly Commercial Development
S1/2 18-29-16	Coastal Zone 2	896	20-00-02	95.00	Coastal Zone II	U.S 19		20	Mixed Residential / Commercial / Recreational Development; McMullen Park
N1/2 18-29-16	Coastal Zone 2	896	20-00-01	135.00	Coastal Zone II	U.S 19		20 & 21	Mixed Residential / Commercial / Recreational Development; McMullen Park
N1/2 19-29-16	Allen's Creek	4,890	19-02-48	192.00	Allen's Creek	U.S 19		21	Mixed Residential / Commercial Development; (2) Stormwater Treatment Ponds; Natural Storage Area - DHW (100) = 46.16 Ac-Ft.; Wetland Crossing
S 1/2 19-29-16	Allen's Creek	4,890	19-02-48	192.00	Allen's Creek	U.S 19		21 & 22	Mixed Residential / Commercial Development; (2) Stormwater Treatment Ponds on each side
S 1/2 19-29-16	Coastal Zone 2	896	20-00-04	108.00	Coastal Zone II	U.S 19		21 & 22	Mixed Residential / Commercial Development; (2) Stormwater Treatment Ponds on each side
S 1/2 19-29-16	Allen's Creek	4,890	19-01-67	92.00	Allen's Creek	U.S 19	Allen's Creek	22	Mixed Residential / Commercial Development; (2) Stormwater Treatment Ponds on each side
SEGMENT C - ALLEN'S CREEK @ US 19 TO ULMERTON ROAD (SR 688)									
N1/2 30-29-16	Allen's Creek	4,890	19-01-67	92.00	Allen's Creek	U.S 19		22	Orange Blossom Groves in vicinity and Allen's Creek crossing on boardwalk;Wide flat flood plain
N1/2 30-29-16	Coastal Zone 2	896	20-00-04	108.00	Coastal Zone II	U.S 19		22 & 23	Orange Blossom Groves in vicinity and Allen's Creek crossing on boardwalk;Wide flat flood plain
N1/2 30-29-16	Allen's Creek	4,890	19-01-67	92.00	Allen's Creek	U.S 19		22 & 23	Orange Blossom Groves in vicinity and Allen's Creek crossing on boardwalk;Wide flat flood plain
N1/2 30-29-16	Allen's Creek	4,890	19-01-72	229.00	Allen's Creek	U.S 19		22 & 23	Orange Blossom Groves in vicinity and Allen's Creek crossing on boardwalk;Wide flat flood plain
N1/2 29-29-16	Allen's Creek	4,890	19-01-72	229.00	Allen's Creek	U.S 19		23	Cove Cay Golf Course / US 19 Roadway Corridor
N1/2 29-29-16	Allen's Creek	4,890	19-01-67	92.00	Allen's Creek	U.S 19		23	Orange Blossom Groves in vicinity and Allen's Creek crossing on boardwalk;Wide flat flood plain
S1/2 29-29-16	Allen's Creek	4,890	19-01-72	229.00	Allen's Creek	U.S 19		23 & 24	Mixed Residential / Commercial / Office Development
S1/2 29-29-16	Long Branch	1,769	22-01-08	159.00	Long Branch	U.S 19	Long Branch Creek	24	Mixed Residential / Commercial / Office Development
S1/2 29-29-16	Long Branch	1,769	22-01-07	314.00	Long Branch	U.S 19		24 & 25	Mixed Residential / Commercial / Office Development
N1/2 32-29-16	Long Branch	1,769	22-01-07	314.00	Long Branch	Roosevelt Blvd.		24 & 25	Mixed Residential / Commercial / Office Development; 25-Yr Event Main Stream Elevation varies 7.4 to 11.7
N1/2 32-29-16	Long Branch	1,769	22-01-08	159.00	Long Branch	Roosevelt Blvd.		24 & 25	Mixed Residential / Commercial / Office Development; 25-Yr Event Main Stream Elevation varies 7.4 to 11.7
S1/2 32-29-16	Long Branch	1,769	22-01-07	314.00	Long Branch	Roosevelt Blvd.		25	Mixed Residential / Commercial / Office Development
S1/2 32-29-16	Cross Bayou	7,916	24-01-39	224.00	Cross Bayou	Roosevelt Blvd.		25 & 26	Mixed Residential / Commercial / Office Development
N1/2 5-30-16	Cross Bayou	7,916	24-01-39	224.00	Cross Bayou	150th Ave.		26 & 27	Mixed Residential / Commercial / Office Development

**PINELLAS TRAIL - FLORIDA POWER EXTENSION
FLOOD STUDY DATA**

Table 2-4

Reference: Pinellas County Storm Drainage Basin Studies and Flood Study Maps

DATE: 10/4/01

Flood Study Map No.	Basin Name (s)	Total Basin Area (acres)	Sub Basin No.(s)	Sub Basin Area (acres)	Flood Study Report	Major Rdwy	Drainage Crossings	Corresponding Plan Sht No.	Remarks / Surrounding Development
N1/2 5-30-16	Cross Bayou	7,916	24-01-42	210.00	Cross Bayou	150th Ave.		26 & 27	Mixed Residential / Commercial / Office Development
N1/2 5-30-16	Long Branch	1,769	22-01-07	314.00	Long Branch	150th Ave.		26 & 27	Mixed Residential / Commercial / Office Development
S1/2 5-30-16	Cross Bayou	7,916	24-01-39	224.00	Cross Bayou	U.S 19		26 & 27	Mixed Residential / Commercial / Office Development
S1/2 5-30-16	Cross Bayou	7,916	24-01-37	105.00	Cross Bayou	U.S 19		26 & 27	Mixed Residential / Commercial / Office Development
SEGMENT D - ULMERTON ROAD (SR 688) TO I-75									
S1/2 4-30-16	Cross Bayou	7,916	24-01-37	105.00	Cross Bayou	Ulmerton Road	Cross Bayou	28	Mixed Commercial / Industrial Development ; Triple 12' x 12' box culvert at 86th Street Crossing
S1/2 4-30-16	Cross Bayou	7,916	24-01-36	164.00	Cross Bayou	Ulmerton Road		28	Mixed Commercial / Industrial Development
S1/2 4-30-16	Cross Bayou	7,916	24-01-35	65.00	Cross Bayou	Ulmerton Road		28	Mixed Residential / Commercial / Industrial Development
N1/2 9-30-16	Cross Bayou	7,916	24-01-35	65.00	Cross Bayou	Ulmerton Road		28 & 29	Mixed Residential / Commercial / Industrial Development
N1/2 9-30-16	Cross Bayou	7,916	24-01-34	72.00	Cross Bayou	Ulmerton Road		28 & 29	Mixed Commercial / Industrial Development
N1/2 9-30-16	Cross Bayou	7,916	24-01-37	105.00	Cross Bayou	Ulmerton Road		29	Mixed Commercial / Industrial Development
N1/2 9-30-16	Cross Bayou	7,916	24-01-36	164.00	Cross Bayou	Ulmerton Road		29	Mixed Commercial / Industrial Development
N1/2 9-30-16	Cross Bayou	7,916	24-01-33	156.00	Cross Bayou	Ulmerton Road		29	Mixed Commercial / Industrial Development
S1/2 9-30-16	Cross Bayou	7,916	24-01-33	156.00	Cross Bayou	49th St.		29 & 30	Mixed Commercial / Industrial Development
S1/2 9-30-16	Cross Bayou	7,916	24-01-31	298.00	Cross Bayou	49th St.		30	Mixed Commercial / Industrial Development
N1/2 15-30-16	Roosevelt Creek	7,916	23-05-25	N/A	Roosevelt Creek	118th Avenue	Channel Crossing	30, 31, 32 & 33	Mixed Commercial / Industrial Development; Channel Crossing and Lake (99-05-2) outfall (100yr el.=11.6)
N1/2 15-30-16	Roosevelt Creek	8,320	23-05-30	N/A	Roosevelt Creek	118th Avenue	Channel Crossing	30, 31, 32 & 33	Mixed Commercial / Industrial Development; Channel Crossing and Lake (99-05-2) outfall (100yr el.=11.6)
N1/2 15-30-16	Roosevelt Creek	8,320	23-01-60	N/A	Roosevelt Creek	118th Avenue		30, 31, 32 & 33	Mixed Commercial / Industrial Development; Natural Pond Impacts; Lake (99-05-1) Outfall (100yr el.=11.6)
S1/2 15-30-16	Roosevelt Creek	8,320	23-05-25	N/A	Roosevelt Creek	Lake Blvd.	Channel Crossing	30, 31, 32 & 33	Mixed Commercial / Industrial Development; Channel Crossing and Lake (99-05-2) outfall (100yr el.=11.6)
S1/2 15-30-16	Roosevelt Creek	8,320	23-05-30	N/A	Roosevelt Creek	Lake Blvd.	Channel Crossing	30, 31, 32 & 33	Mixed Commercial / Industrial Development; Channel Crossing and Lake (99-05-2) outfall (100yr el.=11.6)
S1/2 15-30-16	Roosevelt Creek	8,320	23-01-60	N/A	Roosevelt Creek	Lake Blvd.		30, 31, 32 & 33	Mixed Development; Natural Pond Impacts; Lake (99-05-3) Outfall (100yr el.=10.6); Wetland Crossing
S1/2 15-30-16	Roosevelt Creek	8,320	23-02-45	N/A	Roosevelt Creek	Lake Blvd.	Channel Crossing	30, 31, 32 & 33	Mixed Development; Channel Crossing and Lake (99-05-3) Outfall (100yr el.=10.6); Wetland Crossing
N1/2 22-30-16	Roosevelt Creek	8,320	23-02-45	N/A	Roosevelt Creek	Mainland Blvd.		34 & 35	Residential Development; Stormwater Treatment Ponds and Lake (99-05-3) Outfall (100yr el.=10.6)
N1/2 22-30-16	Roosevelt Creek	8,320	23-02-50	N/A	Roosevelt Creek	Mainland Blvd.		35 & 36	Limited Development; Stormwater Treatment Ponds and Miele Lake (99-05-4) Outfall (100yr el.=10.3)
N1/2 22-30-16	Roosevelt Creek	8,320	23-02-55	N/A	Roosevelt Creek	Mainland Blvd.		35 & 36	Limited Development; Stormwater Treatment Ponds and Lake (99-05-5) Outfall (100yr el.=9.3)
N1/2 23-30-16	Roosevelt Creek	8,320	23-02-45	N/A	Roosevelt Creek	I-75		35 & 36	Limited Development; Stormwater Treatment Ponds and Lake (99-05-3) Outfall (100yr el.=10.6)
N1/2 23-30-16	Roosevelt Creek	8,320	23-02-50	N/A	Roosevelt Creek	I-75		35 & 36	Limited Development; Stormwater Treatment Ponds and Miele Lake (99-05-4) Outfall (100yr el.=10.3)
N1/2 23-30-16	Roosevelt Creek	8,320	23-02-35	N/A	Roosevelt Creek	I-75		36	Limited Development; Stormwater Treatment Ponds and Miele Lake (99-05-2) Outfall (100yr el.=8.6); Wetland Crossing
N1/2 23-30-16	Roosevelt Creek	8,320	23-02-30	N/A	Roosevelt Creek	I-75		36 & 37	Metropole Commerce Park - relatively undeveloped
SEGMENT E - I-75 TO SAN MARTIN BLVD (END PROJECT)									
N1/2 24-30-16	Roosevelt Creek	8,320	23-02-30	N/A	Roosevelt Creek	Gandy Blvd.		37	Metropole Commerce Park - relatively undeveloped; Wetland Crossing
N1/2 24-30-16	Roosevelt Creek	8,320	23-02-25	N/A	Roosevelt Creek	Gandy Blvd.		37 & 38	Relatively flat undeveloped terrain - limited commerce; Wetland Crossing
N1/2 24-30-16	Tinney Creek	3,940	31-02-15	N/A	Tinney Creek	Gandy Blvd.		38 & 39	Mixed Residential / Commercial Development - Gandy Blvd. / 9th St. Interchange area
N1/2 24-30-16	Tinney Creek	3,940	31-02-20	N/A	Tinney Creek	Gandy Blvd.		38 & 39	Mixed Residential / Commercial Development - Gandy Blvd. / 9th St. Interchange area
N1/2 24-30-16	Tinney Creek	3,940	31-01-15	N/A	Tinney Creek	Gandy Blvd.	Tidal Creek Crossing	39	Mixed Residential / Commercial Development - Gandy Blvd. / 9th St. Interchange area
N1/2 19-30-17	Tinney Creek	3,940	31-01-15	N/A	Tinney Creek	Gandy Blvd.		39 & 40	Mixed Residential / Commercial Development - Gandy Blvd. / 9th St. Interchange area
N1/2 19-30-17	Tinney Creek	3,940	31-02-05	N/A	Tinney Creek	Gandy Blvd.	Channel Crossing	39 & 40	Residential Development : Lake (99-01-1) Outfall (100 yr. el. = 4.1)
N1/2 19-30-17	Tinney Creek	3,940	31-01-10	N/A	Tinney Creek	Gandy Blvd.		40 & 41	Mixed Residential / Commercial Development
N1/2 19-30-17	Tinney Creek	3,940	31-01-05	N/A	Tinney Creek	Gandy Blvd.	Channel Crossing	40 & 41	Residential / Commercial Development : Lake (99-01-1) Outfall (100 yr. el. = 4.1)
N1/2 20-30-17	Tinney Creek	3,940	31-01-05	N/A	Tinney Creek	San Martin Blvd.		40 & 41	Residential / Commercial Development : Lake (99-01-1) Outfall (100 yr. el. = 4.1)
N1/2 20-30-17	Tinney Creek	3,940	31-99-05	N/A	Tinney Creek	San Martin Blvd.		41	Derby Lane Race track; Limited other Development

design flood frequency will be satisfied. The designs must satisfy pertinent clearance criteria over the predicted water surface elevation of the design storm event.

- Vertical Drift Clearance will be a minimum of 2 feet between the design flood stage and the low bridge member.
- Vertical Navigation Clearance will be a minimum of 6 feet between the design flood stage and the low bridge member.
- Horizontal Drift Clearance requires a minimum of 10 feet for the passage of debris.

The minimum length bridge (hydraulic) structure is one that satisfies minimum clearance criteria and results in a one foot rise in the water surface profile elevation over the existing condition (no bridge structure) for the 100-year flood event immediately upstream of the structure. (SWFWMD section 1.4.7 of "Storage and Conveyance for Floodplains, Floodways, Streams, Lakes, and Other Water Courses and Water Bodies")



Drainage Design Issues

The following paragraphs overview the key drainage issues associated with the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way. Additional details are shown on the 1:200 scale plan sheets that appear in the *Master Plan* section.

Segment A

The north end of the corridor is located in the southern end of the Lake Tarpon Basin. East Lake Road is the basin divide between the Brooker Creek and Lake Tarpon basins, the trail predominantly located on the Lake Tarpon side. Brooker Creek is the first channel crossing (labeled as "Channel A" in

the Brooker Creek Basin Study). The existing bridge, which has a low chord elevation of 10.2, will be widened to accommodate the trail. The revised structure will not result in any decreased hydraulic or navigational clearances (vertical or horizontal). The trail is within FEMA flood zone A (within the 100-year flood plain) from the north end of the project until just south of the Brooker Creek crossing (see Sheet 1 in the *Master Plan* section of this report). The trail then remains in zone B until entering the Florida Power R/W south of Tampa Road (see Sheets 2 – 4 in the *Master Plan* section of this report).

The alignment crosses both the Brooker Creek Basin and the South Creek Basin as it approaches Tampa Road, where it will cross the navigable Lake Tarpon Outfall Canal. The preferred alignment will widen southbound East Lake Road off ramp bridge to accommodate the trail, with no decrease in hydraulic or navigational clearances (vertical or horizontal). The alignment will then bridge a wetland area (see Plan Sheet 5 in the *Master Plan* section of this report) just south of Lake St. George within the South Creek Basin. The area between Catalina Drive and Lake St. George Drive is predominantly Flood Zone A, which then becomes Flood Zone C (minimal flooding) as the trail reaches Lake St. George drive and continues westerly.

South of Curlew Road, the alignment briefly enters the Possum Branch Basin and then crosses into the Curlew Creek basin just northeast of Fox Hill Drive (see Plan Sheet 8 in the *Master Plan* section of this report) within the Florida Power R/W. The trail passes over an existing cross drain between transmission towers NC 78 and NC 77 on Plan Sheet 9, which drains westerly and outfalls into Channel "M" within the Curlew Creek Basin. The same situation occurs between towers NC 76 and NC 75 , as well as the trail's crossing of cross drain structure 48 immediately north of tower NC 74 (both crossings are on Plan Sheet 10 in the *Master Plan* section of this report). No impacts are anticipated to these existing structures by the trail. This

portion of the trail is within Flood Zone X (outside the 500-year flood plain).

Segment B

The alignment briefly crosses into the Alligator Creek Basin between Enterprise Road and US 19, and immediately enters back into the Curlew Creek Basin, as it turns westerly across US 19. The trail crosses Channel "B" just south of tower NC 67 on Plan Sheet 13 in the *Master Plan* section of this report, which will require a pipe under the trail. While still within Flood Zone X, this segment of the Florida Power R/W is adjacent to the Top of the World golf course. The existing drainage patterns and poor soil conditions in this area should be considered during the design phase.

The trail enters the Alligator Creek Basin once again as it heads southward at Florida Power's East Clearwater Substation. The alignment remains in Flood Zone X to a point just north of the CSX railroad crossing as shown on Plan Sheet 17 in the *Master Plan* section of this report, where it enters zone AE (an area within the base (100-year) flood plain where flood elevations have been determined). South of the railroad, the trail crosses Alligator Creek, where the base flood elevation of this floodway is approximately 26.2 (NGVD 1929). This crossing will be bridged, with a profile providing a vertical hydraulic clearance that results in "no-rise" of the base flood over the existing conditions, as Alligator Creek is a regulated floodway.

The alignment re-enters Flood Zone X immediately south of the Alligator Creek crossing and remains in this flood zone until it crosses Belleair Road within the Allen's Creek Basin.

The trail continues south within the Alligator Creek Basin to a point just north of Gulf to Bay Boulevard, where it enters the Coastal Zone 2 Basin (see Plan

Sheet 19 in the *Master Plan* section of this report). The trail crosses a ditch just south of Brentwood Drive (Plan Sheet 20), which will likely be accomplished utilizing a pipe culvert. The trail enters the Allen's Creek Basin just south of this ditch crossing, returns to the Coastal Zone 2 Basin Harn Road and Nursery Road (see Plan Sheet 21 in the *Master Plan* section of this report), and then re-enters Allen's Creek Basin just north of Belleair Road. Segment B terminates at the Allen's Creek crossing, which is located within Flood Zone AE (elevation 10).

Segment C

The north end of Segment C begins in the Allen's Creek Basin. Allen's Creek, which is tidally influenced, will be spanned with a concrete bridge structure that will provide appropriate hydraulic, navigation, and drift clearances (horizontal and vertical). After crossing U.S. 19 with a pedestrian bridge, the trail crosses into the Long Branch Basin about 1,000 feet north of Whitney Road. The alignment is predominantly within Flood Zone C heading southerly to Long Branch Creek, where it crosses into Zone A9, which has a base flood elevation of 10. It is anticipated that an open-bottom box culvert will span this channel. The trail re-enters flood zone C just south of Long Branch Creek, and crosses into the Cross Bayou Basin at 150th Avenue (Plan Sheet 26).

The 62nd Street R/W abuts the Florida Power R/W and contains a swale system that runs parallel to the trail from 150th Avenue to south of 142nd Avenue. Two swales cross the Florida Power R/W transversely and tie into the 62nd Avenue swale (they are located on the north and south sides of 142nd Avenue). Due to the proximity of the transverse ditches to 142nd Avenue, it is likely these crossings will be accomplished by means of piping the ditch under the trail.

The trail heads easterly south of 142nd Avenue at the southern terminus of Segment C, where another ditch crossing occurs, accomplished utilizing a pipe culvert (Plan Sheet 27). The alignment remains in flood zone C until it reaches the Cross Bayou Canal, where it crosses into flood zone AE (base flood elevation of 11) just east of 58th Street North.

Segment D

Segment D begins within the Cross Bayou Basin and Flood Zone X. The trail will cross under Ulmerton Road by way of a proposed FDOT Ulmerton Road bridge over the Cross Bayou Canal. Once past the roadway, the trail will cross the canal with a concrete bridge. The trail remains in Flood Zone X and enters into the Roosevelt Creek Basin south of 118th Avenue and soon thereafter turns in an easterly direction at 110th Avenue. The alignment temporarily exits the Florida Power R/W and follows the 110th Avenue R/W, where it exits Flood Zone X outside the 500-year flood plain and enters Zone X within the 500-year flood plain. As the alignment within the 110th Avenue R/W approaches the 34th Street R/W (Plan Sheet 33), the trail crosses within Flood Zone A9 (base flood elevation 10). The alignment then traverses southerly along the 34th Street R/W and crosses a ditch just prior to re-entering the Florida Power R/W (Plan Sheet 35 of *Master Plan* section of this report). This crossing will likely be accomplished by means of piping the ditch under the trail.

The alignment traverses a small upland strip between existing ponds (lake 99-02-4 to the north and lake 99-02-5 to the south, Roosevelt Creek Basin) located just west of tower NC-5. This area is prone to flooding during the rainy season. A minimum 2-year event and a desired 5-year event will be evaluated during design. However, a series of 18-inch pipe culverts should adequately provide for

existing drainage along this approximately 200-foot area.

Segment E

The west end of Segment E is within the Roosevelt Creek Basin in Flood Zone C, and crosses into the Tinney Creek Basin just west of the Gandy Boulevard - 9th Street North intersection (Plan Sheets 38 and 39). The alignment crosses tributary 31-02 between 9th Street North and 4th Street North (Plan Sheet 39). This crossing will be spanned with a boardwalk structure. The final crossing occurs between 4th Street North and the southern terminus of the project at San Martin Boulevard (Plan Sheet 41). This canal will be spanned with a boardwalk structure.

Environmental Considerations

The alignment of the proposed trail was planned to avoid and minimize impacts to wetlands and protected wildlife species. Environmental resource areas potentially affecting trail alignment and design of the proposed trail corridor include upland habitats supporting protected wildlife species and wetlands subject to the regulatory purview of various local, state, and federal resource protection agencies.

Preliminary review of the proposed Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way corridor and initial site investigations identified nineteen (19) potential areas subject to the wetland regulatory authorities. Subsequent to the initial review, preliminary inventories were performed by Florida Power and VHB Environmental Scientists to identify and characterize the wetlands and other environmental considerations within the trail corridor.


Wetland jurisdictional areas throughout the 22.59-mile length of the trail are identified on the Conceptual Alignment Plans (Sheets 1 through 41 of the *Master Plan*). The wetland delineations were developed through review of aerial photography correlated with the *Soil Survey of Pinellas County* and field investigations to verify and characterize the wetland areas. The Southwest Florida Water Management District (SWFWMD), Florida Department of Environmental Protection (FDEP) and U.S. Army Corps of Engineers (ACOE) jurisdictional limits are believed to coincide. Therefore the wetland limits as depicted in the *Master Plan* are expected to fall within the regulatory purview of these agencies, as well as Pinellas County Environmental Management.

With few exceptions, wetland impacts have been avoided, minimized, and/or limited to previously impacted or highly degraded systems. Communications have been initiated with the SWFWMD, FDEP, and ACOE relative to the proposed wetland impacts and associated mitigation requirements. Further communications with FDEP and SWFWMD to confirm the lead agency in the processing of the required Environmental Resource Permit will be required. Formal pre-application meetings with the various wetland regulatory agencies as well as the development of the wetland assessments (formal delineations, functional value determinations, wetland characterizations, etc.) will take place as a component of permit preparation and processing.

The locations of mitigation sites are continuing to be discussed with Florida Power. It is anticipated that compensation for any wetland impacts can be provided within the corridor. Mitigation may include enhancement of existing wetlands as well as wetland creation. However, due to the primary function of the right-of-way as a transmission line corridor, conservation easements within the ROW are prohibited. These and other related issues will

be fully explored with the various wetland resource protection agencies during the pre-application process. No ponds are anticipated at this time.

Preliminary and follow-up field investigations have identified the location of gopher tortoise burrows in the vicinity of the Curlew Substation and approximately 100 feet from the Trail (see Sheet 6 of 14 of the *Master Plan*). Any protected species observed throughout the ongoing investigation of the corridor will be recorded and located. Any protected species issues that potentially affect the design or construction of the project will be identified and closely coordinated with the Florida Fish and Wildlife Conservation Commission. With the appropriate construction barrier and avoidance of any construction within 50 feet of a gopher tortoise burrow, no impacts to the species is expected.



Fred E. Marquis Pinellas Trail
EXTENSION ON FLORIDA POWER RIGHT-OF-WAY

Master Plan

This chapter summarizes the master plan for the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way given the design criteria and approach previously articulated. The preferred alignment presented here is the culmination of a detailed alternatives analysis, and input from Florida Power, Pinellas County Staff, and the general public.

Conceptual Alignment

Segment A—Preferred Alignment - From John Chesnut, Sr. Park to U.S. 19 at Enterprise Road

Segment A will begin at the John Chesnut, Sr. Park Entrance on East Lake Road where the existing trail currently terminates, and will extend to Enterprise Road. This segment of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way is approximately 6.55 miles in length and will connect numerous residential developments to shopping and business centers, health facilities, schools, parks, and libraries. Segment A is anchored to the north by John Chesnut, Sr. Park and to the south by Countryside Mall. Approximately 85 percent of the remainder of the corridor is surrounded by residential developments that should supply ample users to this segment of the trail. The following is a detailed description of the Segment A trail alignment as it passes from north to south. Following this description are Conceptual Alignment Plans (Sheets 1 to 12 of 41) that graphically depict the trail alignment.

Beginning at John Chesnut, Sr. Park, a 12-foot wide paved trail with variable 2 to 5-foot shoulders will replace an existing sidewalk that extends from the park to just south of the Boot



Ranch Residential Development along East Lake Road. This roadway is owned by Pinellas County. The trail will begin at the park entrance with an at-grade treatment (Sheet 1 of 41). Approximately 100 feet south of the trail beginning is a second park entrance that will also be provided with an at-grade treatment. Existing wooden utility poles and a chain



link fence that separates the trail alignment from the East Lake Road drainage swale system will border the trail from the park to Brooker Creek.

The chain link fence may require relocation in order for the trail to traverse through the corridor given the limited availability of right-of-way. The existing fence appears to be in place to provide a safety barrier between the existing sidewalk and the roadway swale back slope, which appears to be greater than 3:1. The intent is to continue to provide the same safe conditions for the trail. The existing Brooker Creek Bridge will require modification. Per the *AASHTO Guide for the Development of Bicycle Facilities* and the *FDOT Bicycle Facilities Planning & Design Handbook*, the minimum clear width on a structure should be the same as the approach width of the paved trail, plus the minimum 2-foot wide clear areas. The paved trail will be 12-feet wide in this area. Including the clear area and bridge railings, the total trail facility width will be 18-feet wide (a 10-foot widening of the existing structure). No impacts to the creek or adjacent wetlands is expected. There are several existing utilities that will need to be considered during design. These include, but are not limited to, gas, sewer, and reclaimed water. Further investigation and coordination will be completed during final design to identify the location of all utilities along this portion of Segment A.



Once past Brooker Creek, the trail alignment will front residential developments. Access to three residential development entrances will be crossed at-grade. The trail crossing at the main Boot Ranch Entrance will utilize the pedestrian signal and striping features that are currently provided (Sheet 2 of 41). Once past Boot Ranch, the trail alignment will skirt an existing retention pond, and then run parallel to Seagull Drive (Sheet 3 of 41). The Boot Ranch on and off access ramps will be provided with at-grade treatments. In order to provide safe clearance of the trail from East Lake Road and its drainage swale system, the trail alignment will be placed adjacent to Seagull Drive, with appropriate separation from the curb provided.

Once the trail reaches the Seagull Drive cul-de-sac, the alignment will shift back towards East Lake Road. In order to minimize right-of-way impacts, it



is recommended that the trail be placed as close to the East Lake Road Southbound off-ramp as both roadway and trail design criteria will allow.

A barrier with railing should separate the trail from the roadway travel lanes. As the trail continues along the East Lake Road Southbound off-ramp, the amount of available right-of-way becomes limited. A preliminary assessment shows that the extent of right of way impacts will involve a partial taking on a row of the Apartment complex parking stalls. Included in the Appendix are enlarged 80 scale schematic design plans that further detail the extent of impacts. Without a topographic and right of way control survey, the specific extent cannot be confirmed until the final design stage.

The East Lake Road Southbound off-ramp bridge will require modification. Per the *AASHTO Guide for the Development of Bicycle Facilities* and the *FDOT Bicycle Facilities Planning & Design Handbook*, the minimum clear width on a structure should be the

same as the approach width of the paved trail, plus the minimum 2-foot wide clear areas. The paved trail will be 12-feet wide in this area. Including the clear area and bridge railings, the total trail facility width will be 18-feet wide (Sheet 4 of 41). No impacts to the canal are expected. The trail will cross Tampa Road at-grade at the off-ramp terminus. Pedestrian signal and striping features will need to be added to the existing traffic signal. The existing signal and the impacts to its timing should be analyzed during design to determine the appropriate modifications needed to accommodate the trail. Coordination with the appropriate traffic signal jurisdiction will also be completed during design.



Once past Tampa Road, the trail alignment will follow an existing sidewalk parallel to McMullen Booth Road until the trail turns onto the Florida Power right-of-way. A clear distance of 4-feet is desired between the trail and the McMullen Booth Road retaining wall. Due to horizontal constraints, such as trees and fill slopes, the trail width may be reduced to 12-feet along this area. Once the trail enters Florida Power right-of-way, the alignment turns to the west, returns to the preferred 15-foot width, and crosses Catalina Drive at-grade (Sheet 5 of 41). Just west of Catalina Drive is a small isolated jurisdictional area which supports a dense stand of exotic and invasive species. Every effort should be made to minimize or eliminate impacts to this area. Due to current development in the area, additional surveys will be needed to determine the exact location of the jurisdictional area and the new development. In order to minimize the amount of easement required for the trail, the preferred trail alignment swings to the north at tower CC-298 and continues along the northern right-of-way line. The southern "half" of the power corridor is privately owned between Catalina Drive and Lake St. George

Drive. Based on preliminary investigations, another more extensive jurisdictional area located just east of Lake St. George Drive will have unavoidable impacts. This area, too, is heavily infected with invasive species. This area, like the small isolated wetland is periodically mowed to maintain vegetation within the transmission corridor. Due to flooding history of this area, as well as to minimize the impacts to the jurisdictional area, a low level concrete "boardwalk" is proposed.



Once back at-grade, the trail will cross Lake St. George Drive (Sheet 5 of 41). This intersection will be given an at-grade treatment. There are possible profile grade problems with this street crossing. Once detailed survey is obtained, the trail alignment should be carefully analyzed in order to ensure compliance with ADA criteria. The trail alignment will then split towers LTC-21 and CC-300 (Sheet 6 of 41). The desired offset of 50 feet from the tower legs should be obtained here. After the trail alignment enters the Curlew Substation property, the trail alignment shifts to the east to run parallel between the right-of-way and buried water line. Arrangements will need to be made with Florida Power for access to be opened in the chain link fence boundary of the substation property. Care must be taken to avoid impacts to the gopher tortoise (*Gopherus polyphemus*) burrows that have been found in this area. Any tortoises within the construction area of the trail will be relocated in accordance with the *Gopher Tortoise Relocation Guidelines* recently approved and implemented by the Florida Fish and Wildlife Conservation Commission. After passing through the Curlew Substation property, the trail leaves Florida Power right-of-way and extends across private property (owned by



Blue Jay Mobile Home Estates). Although no negotiations have been made, information gathering meetings and correspondence in association with the February 22, 2001 Public Information Workshop have occurred between Florida Power, Pinellas County and the residents of Blue Jay Mobile Home Estates. There is a mobile home and recreational vehicle storage area that will require special measures. The proposed alignment sets the trail 5 feet from the easterly right-of-way line. There is another small community of possible gopher tortoise burrows located within this portion of the trail corridor that will have to be taken into consideration during design.



An at-grade treatment will be provided as the trail alignment crosses Curlew Road (Sheet 7 of 41). Based on the *FDOT Trail Intersection Design Guidelines Handbook*, the typical section, traffic volume, and speed of the roadway currently warrant that a mid-block pedestrian signal be installed at this crossing location. Intersection analysis, signal warrant analysis, and coordination with the appropriate traffic control jurisdiction will be completed during design. Pedestrian warrants described in the *Manual on Uniform Traffic Control Devices* will also be used to analyze these conditions during final design. Considerations must be given to the existing turn lane configurations on Curlew Road. The crossing should be placed in such a manner that impacts to these are minimized. Once past Curlew Road, the trail enters property owned by the City of Clearwater and continues along the easterly property line. A retention pond, and possible jurisdictional area, occupies a good portion of this property. The trail should be placed as far as possible from the slopes of this



pond. If sufficient clearance between the trail and the top of pond bank is not obtainable, then the appropriate safety railing should be installed between the trail and pond. A narrower typical section may also be required in this area. Once the trail is past the pond, the typical section will be returned to the desired dimensions.

The trail alignment will then cross Meadow Wood Drive at-grade and enter back into Florida Power right-of-way. The trail will continue along the easterly and southerly right-of-way, crossing Fox Hill Drive (Sheet 8 of 41), North Ridge Drive East, and Northside Drive (Sheet 9 of 41) at-grade. As the trail turns south to cross Northside Drive, the Florida Power Hot Oil Pipeline enters the right-of-way and continues along the easterly right-of-way line. The edge of trail should be offset 15 feet from the center of the pipeline. Existing cross drains are located approximately 1,200 feet and 2,850 feet south of Northside Drive (Sheets 9 and 10 of 41). Preliminary analysis concludes that there will be no impact to these 30-inch reinforced concrete pipes (RCPs).

The City of Clearwater Countryside Recreational Complex has a utilization agreement with Florida Power allowing for an overflow parking lot within the right-of-way (Sheet 11 of 41). The preferred trail alignment continues straight along the easterly right-of-way line and crosses the parking lot access drive at-grade. One of the primary functions of the trail is to provide connections, therefore the preferred alignment brings the trail users and the recreational complex users together. The existing parking configuration currently separates the grass parking stalls from the Florida Power Hot Oil Pipeline with wooden bollards. The preferred alignment would relocate these bollards adjacent to the paved access drive, therefore eliminating the eastern row of parking spaces. This alignment would bring the trail users



closer to the recreational center, a desirable destination, but would also subject the users to an at-grade crossing of the parking access drive. Since this is an overflow parking area, the interactions between trail users and parking lot users should be minimal, therefore this is the preferred alignment. Due to the effect the trail will have on the eastern row of parking, the utilization agreement may need to be modified in order to allow the City to have the same number of parking spaces.

Once past Countryside Recreational Center, the trail alignment will cross SR 580/Main Street at-grade. At this crossing, Main Street is a 6-lane facility with a 15-foot bi-directional center turn lane. Based on the *FDOT Trail Intersection Design Guidelines Handbook*, the typical section, traffic volume, and speed of the roadway currently warrant that a mid-block pedestrian signal be installed at this crossing location. Intersection analysis, signal warrant analysis, and coordination with the appropriate traffic control jurisdiction will be completed during design. Pedestrian warrants described in the *Manual on Uniform traffic Control Devices* will also be used to analyze these conditions during final design. In order to provide a safer trail crossing, it is recommended that a raised median be constructed for a distance of 20 feet to each side of the crossing. With the addition of a mid-block pedestrian signal, this median would provide a refuge area for trail users who are unable to cross the entire roadway at once. Attention should be given to the placement of this median and the effect it will have on the left turn movements into the residential area located in the northeast corner of the trail and Main Street intersection, as well as into the Countryside Mall entrance.

Once the trail crosses Main Street, the alignment will follow the Countryside Mall parking lot. Records indicate that this property is owned by Florida Power, however an agreement does exist allowing the Mall to utilize this area for paved parking. In

order for the parking area and the proposed trail to coexist within this area, modifications to this existing agreement will need to be arranged between Florida Power and the Mall.



The trail alignment will cross Countryside Boulevard and Winding Wood Drive at their intersection (Sheet 12 of 41). This unsignalized intersection is being considered for signalization by the City of Clearwater. Countryside Boulevard is under the jurisdiction of the City of Clearwater and Winding Wood Drive is under the jurisdiction of Pinellas County. If the intersection is not signalized at the time of trail design, then a signal should be proposed at this location with pedestrian signal and striping features included. Based on the *FDOT Trail Intersection Design Guidelines Handbook*, the typical section, traffic volume, and speed of the roadway currently warrant that a signal be installed at this crossing location. The sight distance along Countryside Boulevard for westbound vehicles is somewhat limited by the landscaping in the median of the roadway. Although proper signing and pavement marking will provide warning to vehicles approaching the proposed signalized intersection, a detailed analysis of the intersection operation and coordination with the appropriate traffic control jurisdictions should be completed prior to design completion. Pedestrian warrants described in the *Manual on Uniform traffic Control Devices* will also be used to analyze these conditions during final design. Without detailed right-of-way and topographic survey, the amount of impact the trail will have on the right-of-way of the southeast corner of the intersection can only be approximated at this time. A possible jurisdictional area was thought to be located approximately 1500 feet south of the Countryside Boulevard intersection. Subsequent field inspections have confirmed that there is not a wetland in this area.

Just north of Enterprise Road, the trail alignment turns west and parallels the roadway. An existing sidewalk system will be replaced by the 15-foot trail. The trail will continue at-grade across Village Drive, where a pedestrian signal and striping features will need to be added to the existing traffic signal. It is recommended that Segment A end in front of the businesses at the northwestern corner of the intersection of U.S. 19 and Enterprise Road. If built as the first link, appropriate striping and signage should be provided to inform trail users that the bicycle access will be ending and that further advancement is not permitted.



Segment A – Alternate Routes

There are three locations within Segment A where alternatives to the preferred alignment are shown on the Conceptual Plan Sheets. These occur at the East Lake Road and Tampa Road interchange, at the City of Clearwater Recreational Complex, and at the intersection of Countryside Boulevard, Winding Wood Drive and the Countryside Mall entrance. The following are descriptions of these alternatives.

The interchange at East Lake Road and Tampa Road is further complicated by the location of the Lake Tarpon Outfall Canal immediately below the interchange. The Oldsmar Canal Trail is proposed to run parallel to the north side of the canal and under the East Lake Road overpass. In investigating alternatives for the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way through this interchange, thought was given to connecting the Trail to the Oldsmar Canal Trail. In order to accomplish this, the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way would be



taken down and under East Lake Road on the north side of the Canal and tied into the Canal Trail. To continue south and through the interchange, the Trail would ramp up to Tampa Road and turn west to cross the Outfall Canal via the Tampa Road Bridge. For this to be successful, the 4-foot Tampa Road pedestrian way would have to be widened to accommodate the 15-foot trail. The trail would then cross the East Lake Road southbound off-ramp and Tampa Road intersection at-grade. As with the preferred alignment, a pedestrian signal and striping features would need to be added to the existing signal. Due to possible profile grade issues and the age of the Tampa Road Bridge, this option was not considered cost effective and therefore is not the preferred alignment. However, if the Oldsmar Canal Trail is completed as proposed, a connection to the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way would be optimal and part of the alternative may be useful.



Due to the proximity of the parking utilization area and the right-of-way lines, the trail will have an effect on the ability of the City of Clearwater Recreational Complex visitors to continue to utilize at least one row of existing parking area. Once final right-of-way and topographic surveys are received for this area, the effect will be known and discussions can begin with the City to modify their utilization agreement with Florida Power. The preferred alignment and the alternatives presented below could provide the same amount of opportunity for parking area expansion. One alternative transitions the trail alignment to the westerly right-of-way line just north of the parking area and transitions back to the easterly right-of-way line just south of the parking area. This alternative



would present the same amount of impact to the parking utilization area, but would eliminate the access drive crossing and provide separation of the trail users and automobiles. As mentioned in the preferred alignment discussion, the utilization agreement may be modified to provide parking area to compensate for the loss of spaces due to the trail location. The second alternative shown on the Conceptual Plan Sheets transitions the trail alignment off of Florida Power right-of-way and onto the City of Clearwater's property. As with the preferred alignment, this alternative would provide direct connectivity between the Trail and the recreational center and would require an at-grade crossing of the parking area access drive; however, there would be no loss of existing parking area. This alternative would, of course, require an agreement between Pinellas County and the City of Clearwater. Discussion thus far with the City has shown that they are interested in connections to the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way and they may be receptive to this alternative.

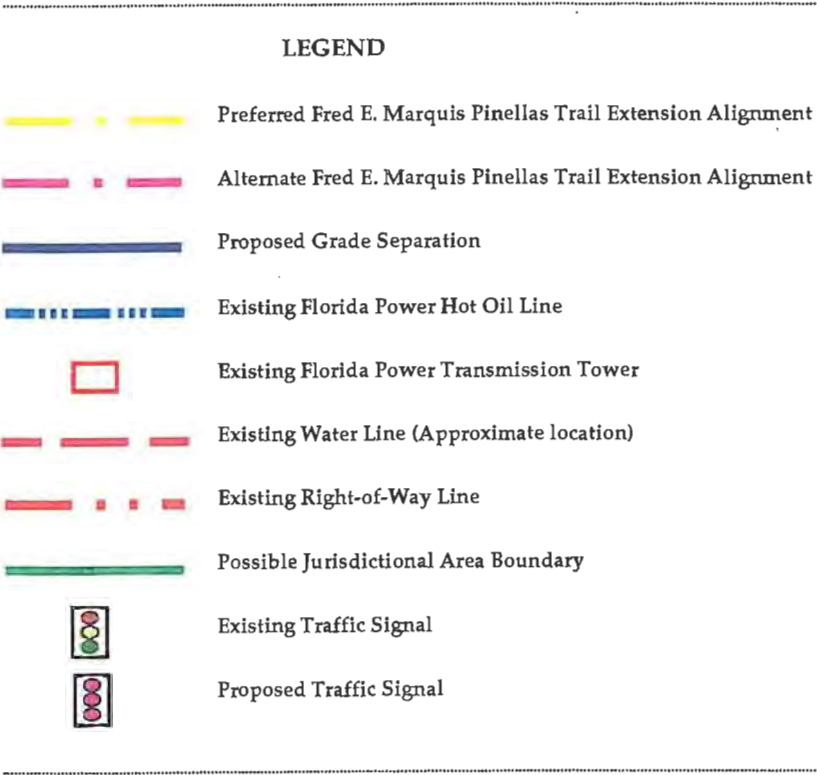
The preferred alignment at the intersection of Countryside Boulevard, Winding Wood Drive, and the Countryside Mall Entrance poses several



concerns. First, is the unknown amount of impact to the right-of-way on the southeast corner of the intersection. Second, is the sight distance concerns along Countryside Boulevard. The alternative alignment at this location only addresses the right-of-way impacts to the southeastern corner of the intersection. This alternate would cross the Mall entrance prior to crossing Countryside Boulevard, thereby eliminating the potential need for right-of-way. The Mall entrance is configured to include a free right turn lane for vehicles exiting the Mall and heading west on Countryside Boulevard. The alternate trail alignment would cross this turn lane,

creating a conflict point within the intersection. The preferred alignment seems to be best suited for the intersection's current configuration. Again, a detailed analysis of the intersection operation should be completed during design in order to define the best possible alignment.

The conceptual alignment for Segment A is provided on the following plan sheets (Sheets 1 to 12):





Segment A



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 1 of 41
3.6 Master Plan



Segment A



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 2 of 41
3.7 Master Plan



Segment A



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 3 of 41
3.8 Master Plan



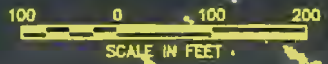
Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 4 of 41
3.9 Master Plan



Segment A



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
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3.10 Master Plan





Segment A



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
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3.11 Master Plan



Segment A



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 8 of 41
3.13 Master Plan



Segment A



**Fred E. Marquis
Pinellas Trail Extension**
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 9 of 41
3.14 Master Plan





Segment A

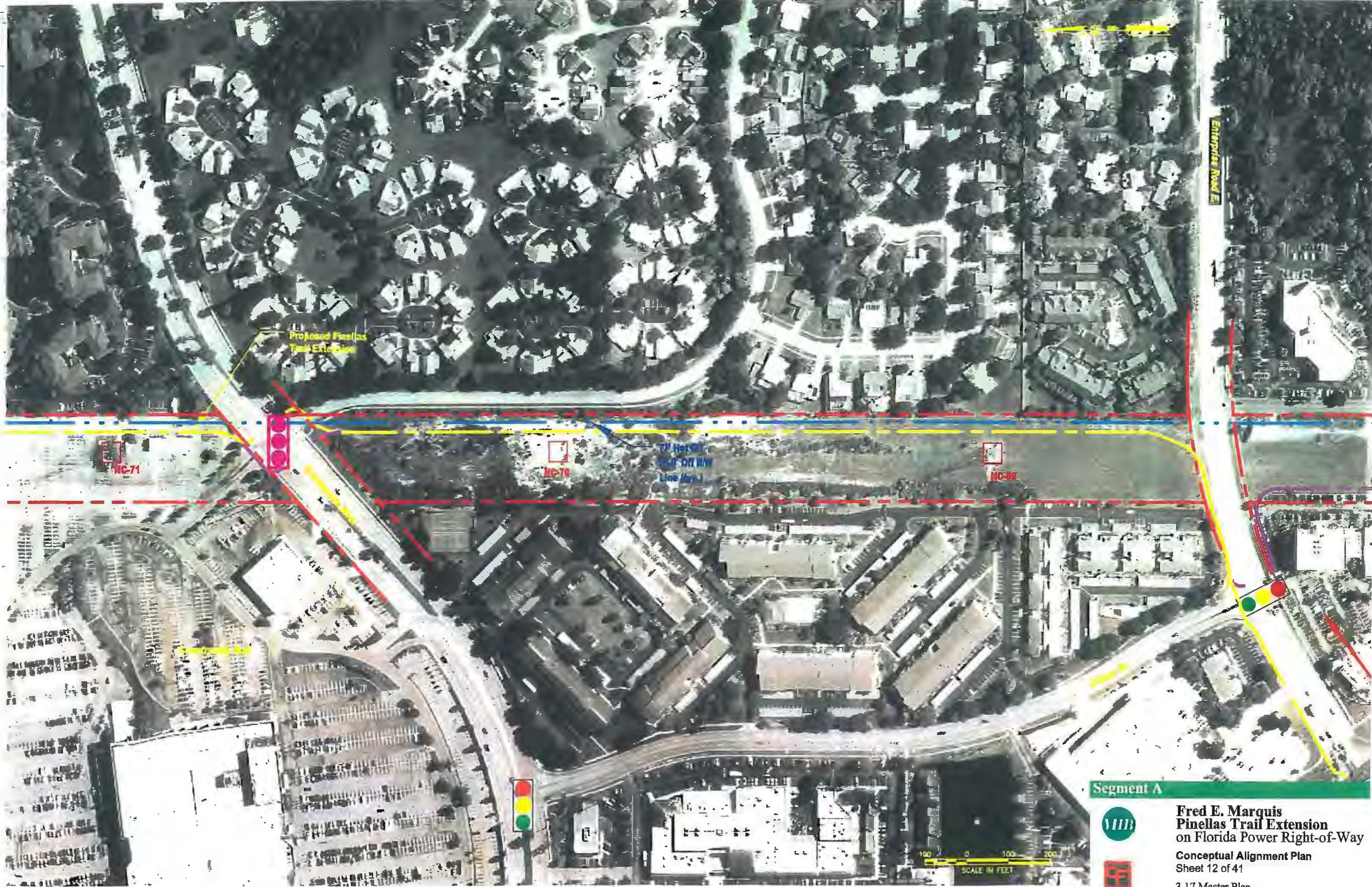


Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 10 of 41
3.15 Master Plan





Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 11 of 41
3.16 Master Plan



Segment A



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 12 of 41
3.17 Master Plan

Segment B – Preferred Alignment - From U.S. 19 at Enterprise Road to Belleair Road

Segment B will begin at the intersection of U.S. 19 and Enterprise Road and will extend south to Belleair Road just north of Allen's Creek. This segment of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way is approximately 5.46 miles in length and will connect numerous residential developments to shopping and business centers, health facilities, schools, parks, and libraries. Approximately 80 percent of the remainder of the corridor is surrounded by residential developments that should, again, supply ample users to this segment of the trail. The following is a detailed description of the Segment B trail alignment as it passes from north to south. Following this description are Conceptual Alignment Plans (Sheets 13 to 22 of 41) that graphically depict the trail alignment.

Beginning in front of the businesses at the northwest corner of the intersection of U.S. 19 and Enterprise Road, the 15-foot wide Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way will traverse the signalized U.S. 19 and Enterprise Road intersection at-grade. FDOT District 7 currently is investigating the possibility of constructing an overpass along U.S. 19 over Enterprise Road. If this project continues and is completed, then the preferred alignment will be ideal. Under existing conditions, trail users will be required to cross a 6-lane facility with a large grass median, and then to cross a 3-lane facility with ample striped gore area. Although the intersection is currently signalized, no pedestrian signals or crossing features exist, and modifications will be required to accommodate the trail. Intersection analysis, signal warrant analysis, and coordination with the appropriate traffic control jurisdiction will be completed during design. Pedestrian warrants described in the *Manual on Uniform traffic Control Devices* will also be used to

analyze these conditions during final design. Once the proposed FDOT overpass project is complete, the trail users will only be required to cross the U.S. 19 access ramps at-grade and pass under the U.S. 19 overpass. Although the at-grade option does not provide the level of safety that a pedestrian overpass would provide, it does fit best with FDOT's planned project.

Once through the intersection, the trail will extend south parallel to U.S. 19 within the roadway right-of-way for approximately 400 feet before turning back on to Florida Power right-of-way (Sheet 13 of 41). As the trail continues south, the alignment will be set 15-feet west of the hot oil pipeline. For approximately 2300 feet, the trail will be adjacent to the Top of the World Golf Course. The existing condition provides a fence around the perimeter of the golf course to



prevent trespassing on a private facility. Since the trails alignment will cross this perimeter, requiring a break in the fence, it is recommended that the fence be relocated as a part of the trail project. It is recommended that the fence be relocated such that it is between the trail and the golf course to protect trail users from errant golf balls. Although there will be approximately 100-feet between the trail and the golf course, there is still a concern of safety with regards to errant golf balls landing on or near the trail. A drainage pipe will need to be placed where the trail crosses a drainage ditch approximately 300 feet south of tower NC-67. Coordination with Top of the World or the appropriate utilization entity will be completed during the design phase.

After passing the golf course, the trail alignment will extend behind the East Clearwater Substation (Sheet 14 of 41). As the trail traverses through this area, sufficient room should be allowed to accommodate the staging area that is utilized by Florida Power. An existing distribution circuit along the western right-



of-way line and the hot oil pipeline alignment between the two towers just west of the substation, prevent the trail alignment from shifting to the western right-of-way line until the existing substation property. As the trail exits the substation area, the alignment shifts to the westerly right-of-way line, where it will be aligned between the right-of-way line and the hot oil pipeline. There is an existing 230KV overhead power line along the right-of-way line that will also have to be accounted for during the design of this segment of trail. The trail alignment continues along the westerly right-of-way line, passing, but not impacting, a possible jurisdictional area just north of Sunset Point Road (Sheet 15 of 41).

As the trail alignment approaches Sunset Point Road, it will shift to the east in order to accommodate an at-grade crossing of the roadway. This shift must



occur so that the crossing is placed where the raised median of the roadway is at full width. This will eliminate impacts to existing turn lane configurations. Based on the *FDOT Trail Intersection Design Guidelines Handbook* a mid-block pedestrian signal is warranted across Sunset Point Road. Intersection analysis, signal warrant analysis, and coordination with the appropriate traffic control jurisdiction will be completed during design to determine the appropriate treatment for this crossing. Pedestrian warrants described in the *Manual on Uniform traffic Control Devices* will also be used to analyze these conditions during final design.

As soon as the trail is past Sunset Point Road, the alignment shifts back to the westerly right-of-way line. This alignment is continued for approximately 1600 feet until the trail reaches the Stag Run Boulevard cul-de-sacs. The alignment will shift



towards the center of the right-of-way, skirting the possible jurisdictional area and passing between the two cul-de-sacs (Sheet 16 of 41). These cul-de-sacs were constructed in order to prevent traffic from using the roadway as a cut through to the Wal-Mart. An existing guardrail between the cul-de-sacs will need to be replaced with a similar barrier to prevent vehicular access on or across the trail. Although the existing Miami curb could be replaced with a vertical curb, it is not believed that this would be a sufficient barrier. The existing landscaping in this area will not require relocation as a part of the trail project; the neighborhood association will be handling this. However, there should be coordination with the neighborhood association's plans to re-landscape this area.

After passing the cul-de-sacs, the trail alignment transitions towards the easterly right-of-way line where it will be set 15 feet west of the hot oil pipeline and cross N.E. Coachman Road at-grade. Approximately 800 feet south of N.E. Coachman Road, the hot oil pipeline shifts west in order to bypass a retention pond. The trail alignment will shift to the west as well and will extend between towers HD-30 and NC-55 (Sheet 17 of 41). The



alignment will continue south, crossing Seaboard Coastline Railroad, a single-track rail line, at-grade. A meeting was held between Florida Power and the Seaboard Coastline Railroad and an at-grade crossing was approved by the railroad. Continued coordination will be required between the designer, Florida Power and the Railroad as the design process continues.

After crossing the railroad, the trail alignment continues south and crosses Alligator Creek. An



open bottom box culvert will be required for the creek crossing. Once past the creek, the trail will extend between towers HD-31 and NC-54. The alignment will then transition to the east where it will be set 15 feet east of the hot oil pipeline. The City of Clearwater Solid Waste Transfer Station driveway and existing City of Clearwater East-West Trail will be provided with an at-grade treatment. Appropriate signing indicating the trail crossing will be applied. As the trail passes through the Carpenter Complex grass parking area, it is recommended that bollards or a similar barrier be installed that will prevent cars from parking on or adjacent to the trail. There are a few paved handicap accessible parking places that should be avoided or may require relocation to best suit the Carpenter Complex visitors, as well as the trail users. The existing utilization between Florida Power and the City of Clearwater Carpenter Complex will be considered when determining the appropriate design solution.



The trail alignment will continue south, crossing the Carpenter Complex driveway, the commercial driveway at Sharkey Road, and the Target delivery driveway at-grade (Sheet 18 of 41). As the hot oil pipeline shifts back to the east just south of NC-63, the trail alignment will shift to the west to cross the hot oil pipeline and lie 15 feet west of the pipeline. After passing the Target driveway, the trail alignment enters the Time Warner Communications (TWC) utilization area. TWC uses half of their utilization area for storage of trucks and other equipment at night, while the other half is used as employee parking during the day. There



are a number of issues that arise when the trail alignment is brought into this area. The problematic mixture of vehicles and trail users is further complicated by the location of the Florida Power towers, equipment parking stalls, and landscaping around the daily parking area. It is recommended that a narrower typical section be used in this area and that special attention be given to the design of the trail at this location. As seen on the Conceptual Alignment Plan Sheet, the trail will turn west just south of NC-52, and extend to the edge of Old Coachman Road. The trail will then turn south, parallel Old Coachman Road and cross Drew Street at the signalized intersection of Old Coachman Road and Drew Street. Pedestrian signal and striping features do exist that will be utilized for the trail crossing.

The trail alignment turns east as soon as the at-grade crossing of Drew Street is complete and then turns south at the easterly Florida Power right-of-way line. As the alignment extends south, the trail should be set 15 feet west of the hot oil pipeline. St. Petersburg Junior College utilizes this portion of the Florida



Power right-of-way for an overflow parking lot (Sheet 19 of 41). There are six light poles that will require relocation and the parking aisle along the eastern side

of the parking lot will require modification to allow room for the trail. There is also a utility/pump station located adjacent to the parking lot that will have to be accounted for in the trail design. At the end of the parking lot, the trail will turn west towards Old Coachman Road, and then south to cross the Post Court Apartment Entrance at-grade.

The trail alignment will transition back to the western right-of-way, maintaining the 15-foot offset from the hot oil pipeline, until it crosses back to the west to cross at-grade at the signalized



intersection of SR 60/Gulf to Bay Boulevard and Old Coachman Road. Pedestrian signals and crosswalks exist on three of the four legs of the intersection of SR 60/Gulf to Bay Boulevard and Old Coachman Road. The trail alignment will be crossing Gulf to Bay Boulevard on the fourth leg. It is recommended that the existing signal be modified by adding two pedestrian "walk/don't walk" signals and appropriate crosswalk striping. Intersection analysis, signal warrant analysis, and coordination with the appropriate traffic control jurisdiction will be completed during design. The trail alignment will then continue along the westerly right-of-way line, and turn towards the easterly right-of-way line just south of tower HD-36.

Once back on the east side of the right-of-way, the alignment will continue with a 15-foot offset from the hot oil pipeline, crossing Druid Road at-grade (Sheet 20 of 41). The trail will extend south maintaining this alignment while crossing Burnice Drive and Brentwood Drive at-grade. Attention will be given to the location of the trail with respect to the retention ponds located between Burnice Drive and Brentwood Drive. Just south of Brentwood Drive, a drainage pipe will be placed where the trail crosses a drainage ditch within Florida Power right-of-way. Appropriate coordination with the utilization entity for this ditch will be completed during the design phase.



An alternate alignment was considered between Druid Road and Burnice Drive. This alignment would have turned west at Druid Road, traversed around the western most edge of the Bayview Substation property, passed around the City of Clearwater recreational property and rejoined the preferred alignment at Burnice Drive. Upon review, this alternative was deemed not feasible due to

planned expansion of the Bayview Substation by Florida Power and was removed from consideration.

Just north of Harn Boulevard (Sheet 21 of 41), efforts should be made to minimize or eliminate impacts to a possible jurisdictional area. Harn Boulevard will be provided with an at-grade treatment as the trail continues south. Approximately 800 feet south of Harn Boulevard, the trail alignment transitions to the west, turning south at HD-41 and skirting the retention pond just north of Nursery Road. An at-grade crossing will be provided at Nursery Road as the trail alignment continues south and east to pass just east of NC-43. An existing drainage pipe just south of Nursery Road will need to be replaced as a part of the trail project. Pinellas County has a current project (PID 922241) which involves widening Nursery Road. Coordination with the County's design team has begun and will continue as the trail design proceeds.

Once past NC-43, the trail maintains a 15-foot offset from the hot oil pipeline. It is recommended that the trail segment end at Belleair Road (Sheet 22 of 41). Depending upon the construction sequencing, appropriate striping and signage should be provided to inform trail users that the bicycle access will be ending and that further advancement is not permitted.

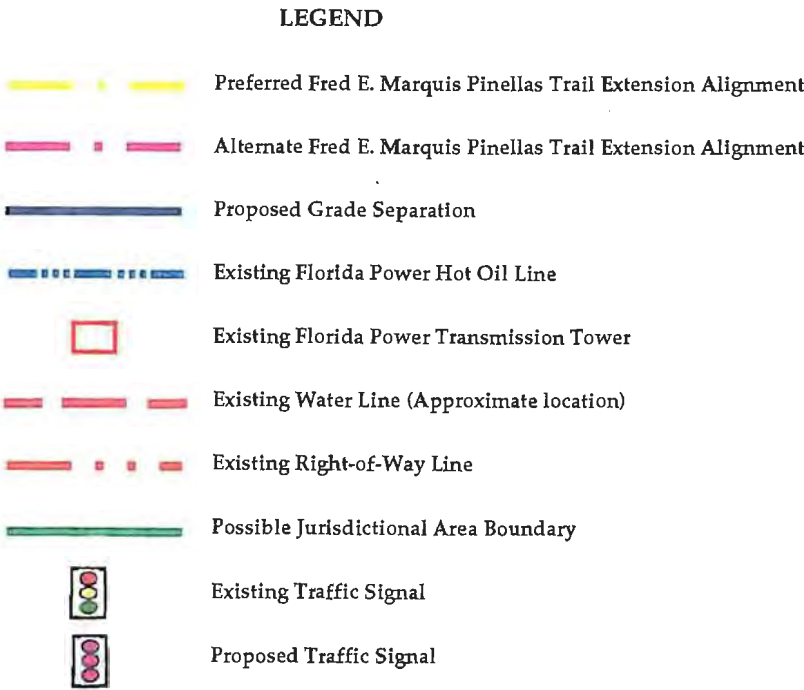
Segment B – Alternate Routes

The preferred trail alignment extension at U.S. 19 and Enterprise Road is to cross U.S. 19 and then Enterprise Road at-grade. The intersection is currently signalized; adding a pedestrian signal and striping features will be required. The alternate alignment for this portion of the trail is to continue along the Florida Power right-of-way and cross U.S. 19 via a pedestrian overpass. The alternate would begin at the intersection of Enterprise Road and Village Drive. The trail alignment would cross



Enterprise Road just east of Village Drive, where a pedestrian signal and striping features will need to be added to the existing traffic signal, and then turn east along Enterprise Road to enter back into Florida Power right-of-way. The trail would continue along the Florida Power right-of-way, crossing U.S. 19 via a pedestrian overpass. There are challenging constructability issues with this alternative due to the close proximity of the various overhead electric lines on the proposed overpass location. Additional costs for relocating utility lines would be incurred for this alternate. If the FDOT District Seven project is not completed, then Pinellas County should proceed with investigations into the design and construction of a pedestrian overpass over U.S. 19 within the Florida Power corridor.

The conceptual alignment for Segment B is provided on the following plan sheets (Sheets 13 to 22):





Segment B

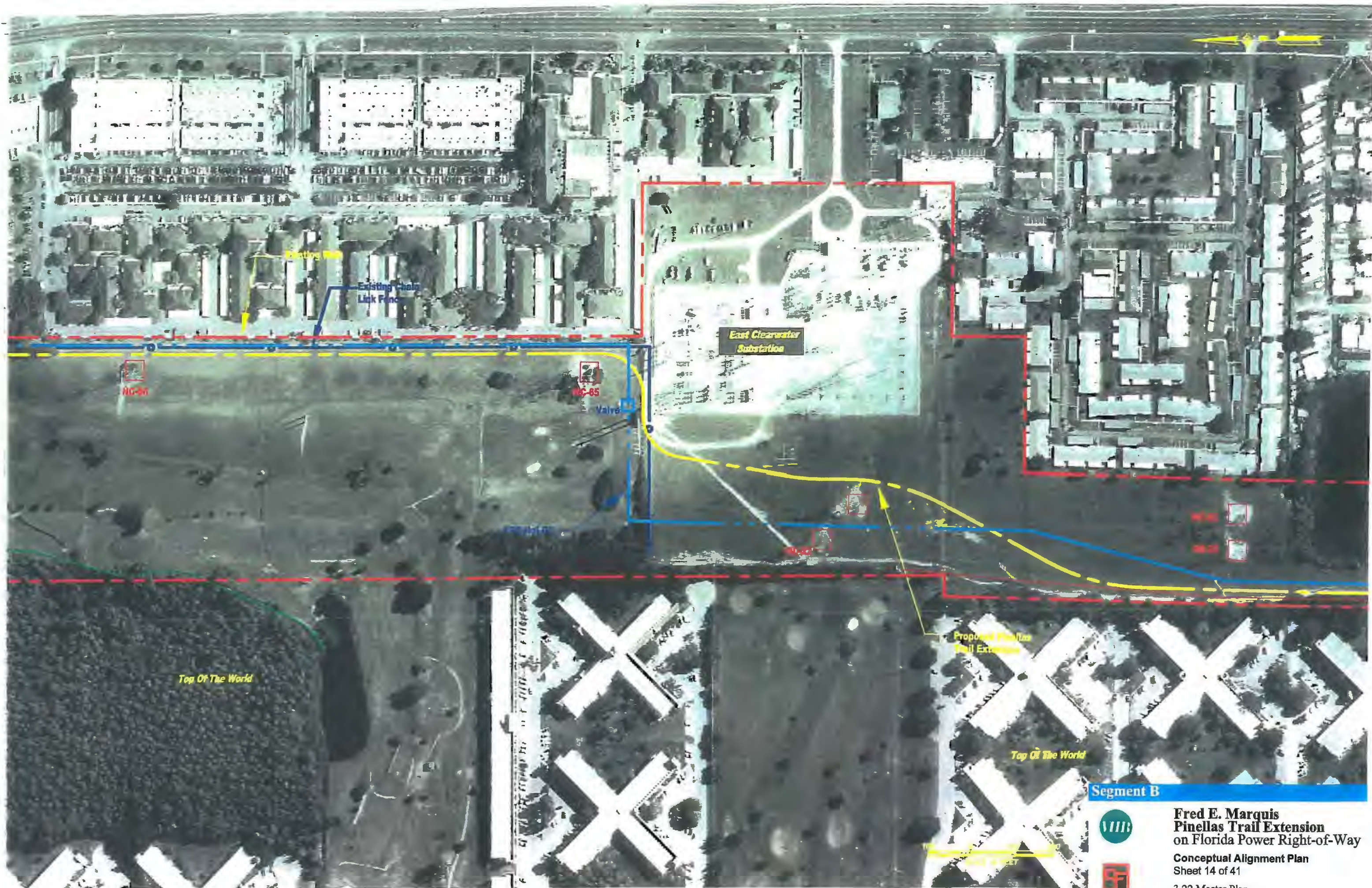


**Fred E. Marquis
Pinellas Trail Extension**
on Florida Power Right-of-Way

Conceptual Alignment Plan
Sheet 13 of 41



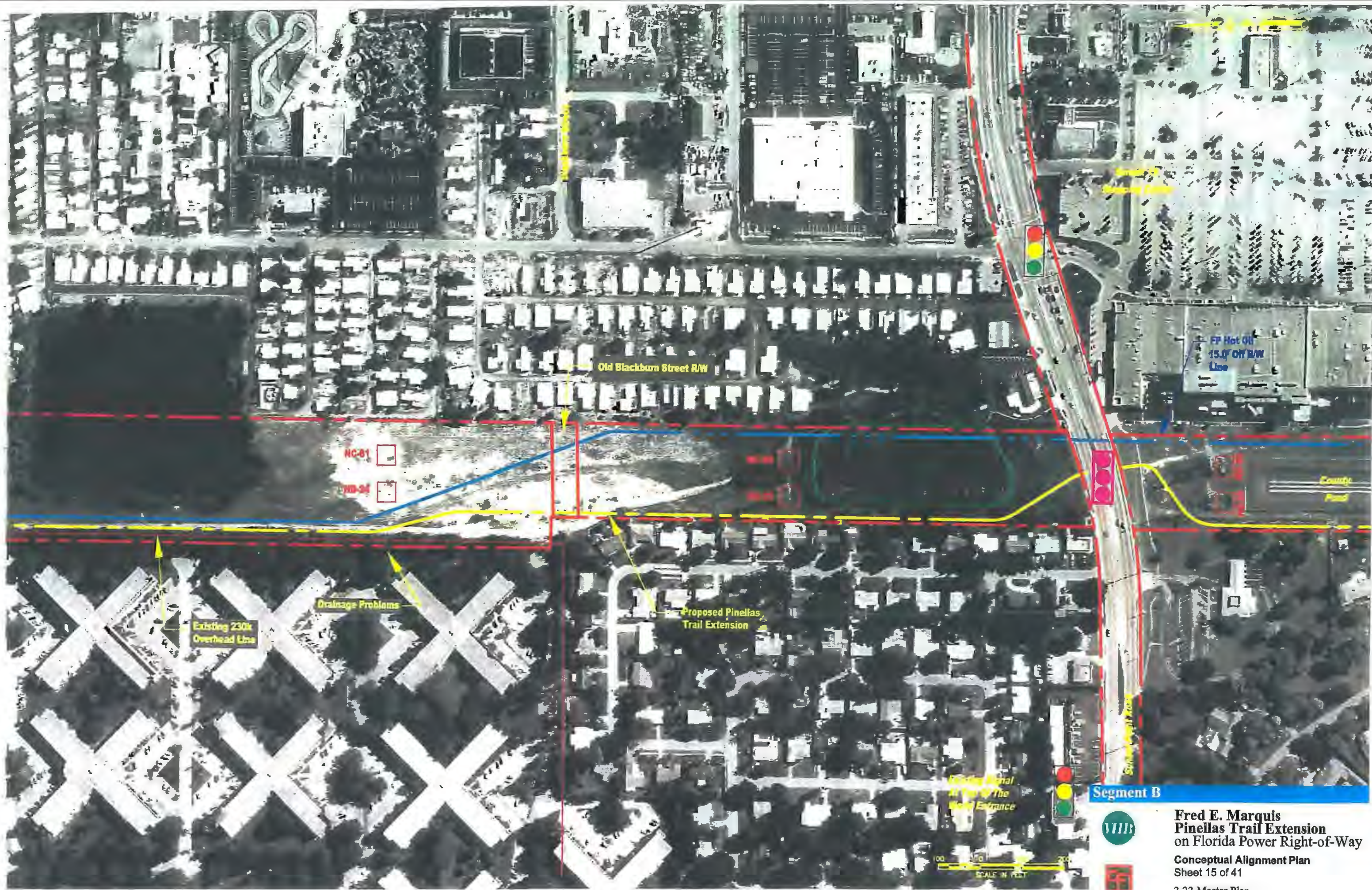
3.21 Master Plan



Segment B



**Fred E. Marquis
Pinellas Trail Extension**
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 14 of 41
3.22 Master Plan



FP Hot Oil
15.0' ON R/W
Line

County
Road

Existing 230k
Overhead Line

Drainage Problems

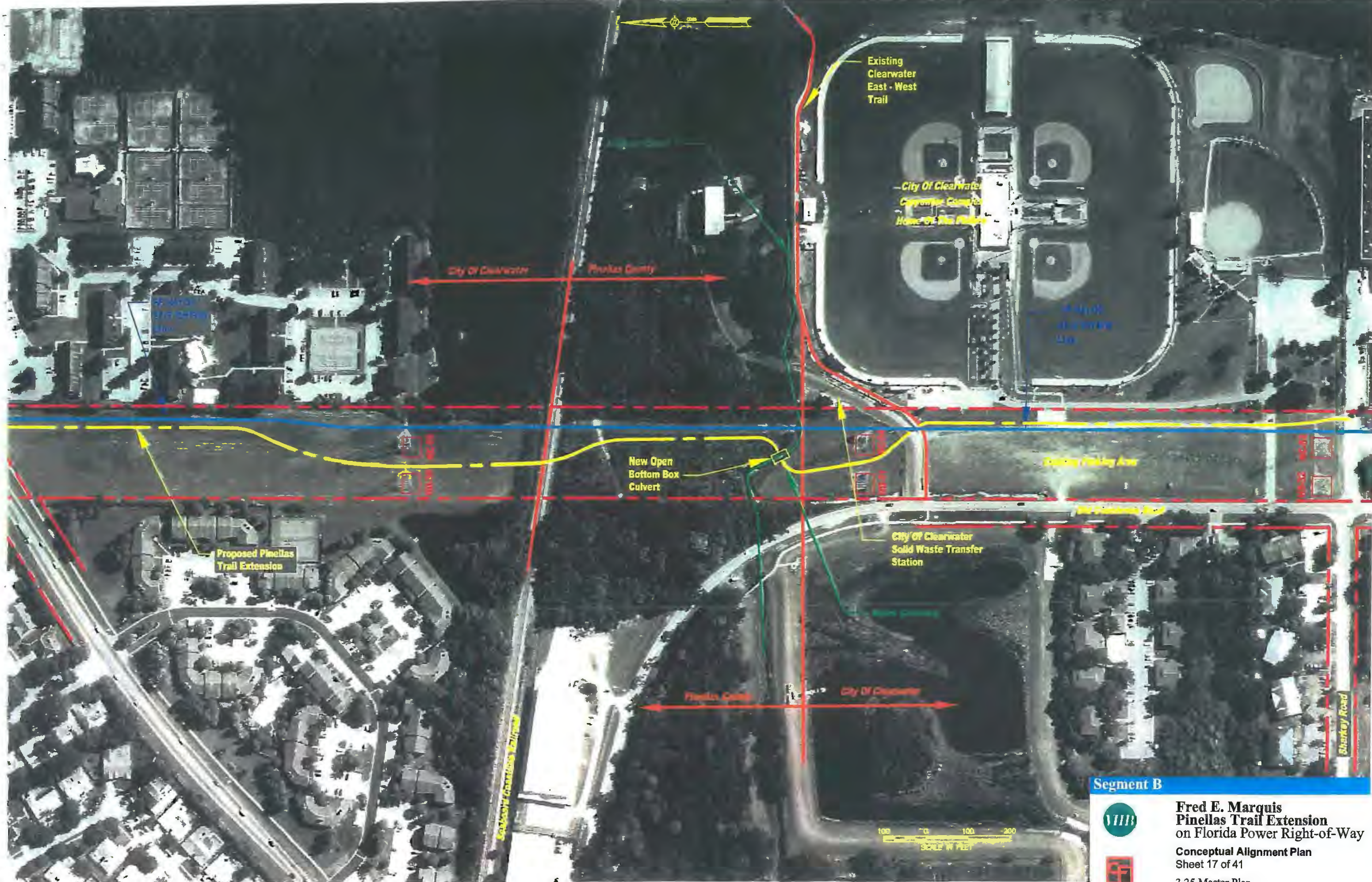
Proposed Pinellas
Trail Extension

Signal at Top of The
World Entrance

Segment B



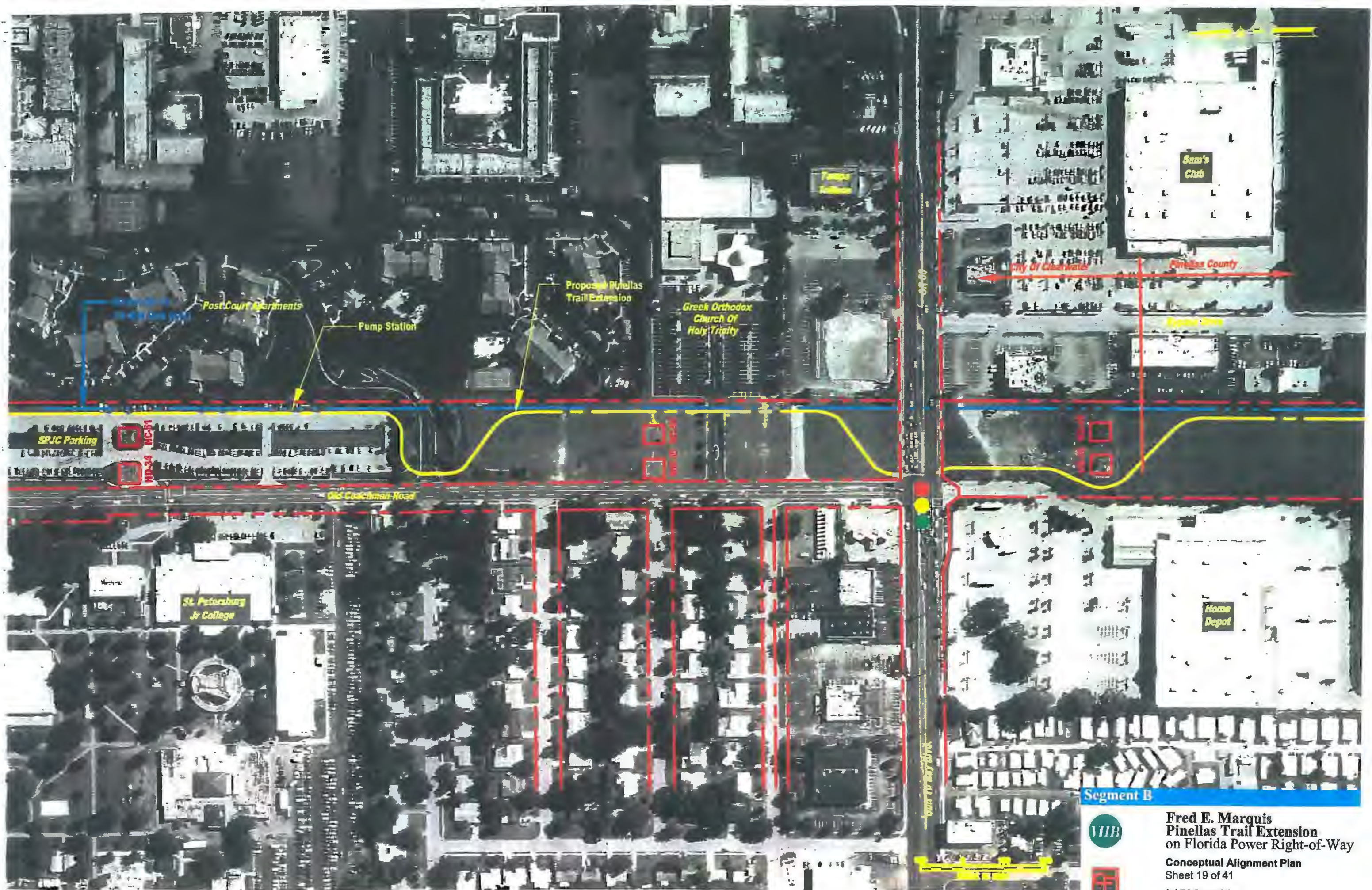
Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 15 of 41
3.23 Master Plan



Segment B



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
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3.25 Master Plan



Segment B



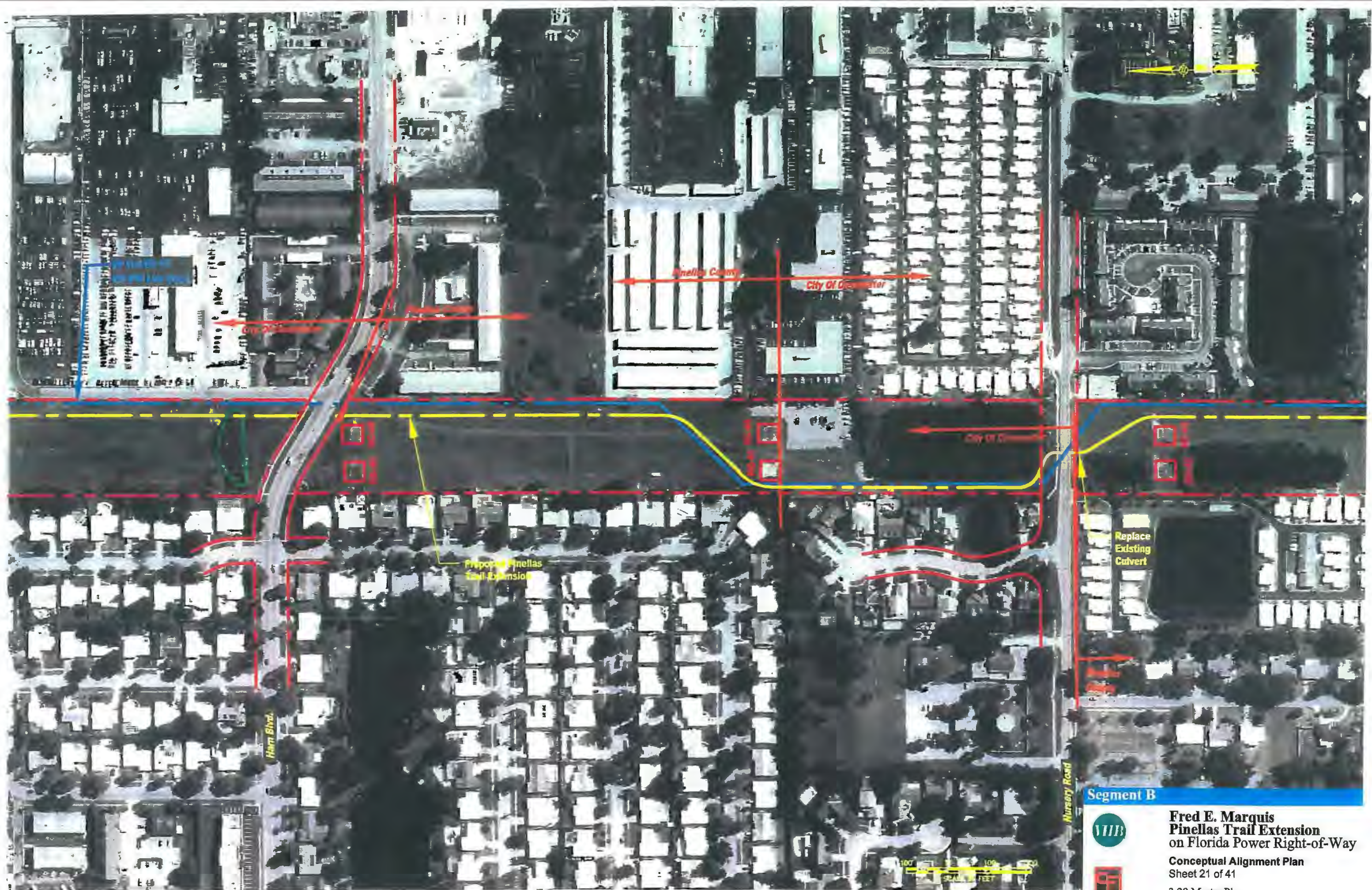
Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 19 of 41
3.27 Master Plan



Segment B



**Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way**
Conceptual Alignment Plan
Sheet 20 of 41
3.28 Master Plan



Segment B



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
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3.29 Master Plan



Segment B



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 22 of 41
3.30 Master Plan

Segment C – Preferred Alignment – From Belleair Road to SR 688/Ulmerton Road

Segment C will begin at Belleair Road just north of Allen's Creek and will extend south and east to SR 688/Ulmerton Road. This segment of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way is approximately 3.25 miles in length and will connect numerous residential developments to shopping and business centers, and schools. Approximately 60 percent of the corridor is surrounded by residential developments supply readily available users to this segment. The following is a description of the Segment C trail alignment as it passes from north to south. Following this description are Conceptual Alignment Plans Sheets (23 to 27 of 41) that graphically depict the trail alignment.

This segment of the trail begins at Belleair Road. Just south of the trail beginning is Allen's Creek, which could be a wonderful destination for trail users. The original trail alignment proposed crossing the creek via a low level concrete structure and then turning to cross U.S. 19 with a trail overpass. Due to design constraints such as length of crossing, existing utilities and limited available right-of-way, the costs for these two structures would be very high. Therefore, the preferred alignment currently turns east, parallels Belleair Road, and heads for U.S. 19. The intersection of Belleair Road and U.S. 19 is signalized and pedestrian crosswalk features do currently exist. The preferred alignment will cross U.S. 19 at this signal with minor modifications to signing along the roadways. FDOT is planning improvements to U.S. 19 that may have an effect on this alignment. FDOT has indicated this overpass project is not funded, and no conceptual plans have been developed as yet, therefore, not enough information is available at this time to make any additional engineering decisions with respect to the trail alignment.

After crossing U.S. 19 via the signalized intersection at Belleair Road, the trail will turn south and extend parallel to the east side of U.S. 19 within the roadway right-of-way for approximately 2500-feet before reaching the intersection of U.S. 19 and Haines Bayshore Road (Sheet 23 of 41). The existing U.S. 19 bridge over Allen's Creek, approximately 500-feet south of Belleair Road will need to be widened 18-feet to accommodate the trail.



In order to avoid a crossing at the busy intersection of U.S. 19 and Haynes Bayshore Road, the trail will turn east, extending parallel to the north side of Haines Bayshore Road within the roadway right-of-way for approximately 600 feet before making an at-grade mid-block crossing of the roadway. The Cove Cay Golf Course abuts upon U.S. Highway 19 and Haines Bayshore Road through this area. The existing condition provides a fence around the perimeter of the golf course to prevent trespassing on a private facility. Since the trail alignment will cross this perimeter, requiring a break in the fence, it is recommended that the fence be relocated/replaced as a part of the trail project.

After crossing Haines Bayshore Road, the trail will resume the desired 15-foot typical section and extend along a 25-foot trail easement that is being provided along the Quality Boats property. Once past the Quality Boats property, the trail will rejoin the power corridor. Approximately 700 feet into the corridor, the trail alignment will shift west, running between the corner of a residential neighborhood that interrupts the eastern edge of the Florida Power corridor, and a pond to the west. The trail will then shift back to the eastern edge of the power corridor



just north of NC-37 and HD-48. Approximately 1,300 feet further along the corridor, the trail will cross Whitney Road at-grade (Sheet 24 of 41). Florida Power owns the property to the north of Whitney Road. An agreement is in place to allow access from Whitney Road to an isolated parcel by way of an drive. The Florida Power right-of-way is currently fenced and access is granted via a locked gate. Since the access drive is currently used to move a large truck from the isolated parcel to Whitney Road, concrete aprons will be placed on each side of the trail at the drive crossing. Additional measures may be installed to prevent non-permissible vehicular access along this drive.

Beyond Whitney Road, the Florida Power corridor and trail pass through a largely residential area. Just past Whitney Road, the Tri-City Substation is occupying much of the power corridor right-of-way. There is also a permitted parking area and an access drive to the substation. As the trail passes the Substation access drive, the desired minimum of 15-foot offset between the hot oil pipeline and the trail will not be maintained for approximately 800 feet. Although the trail will not be directly above the pipeline, there will only be approximately 3 feet of horizontal clearance between the trail and the pipeline. A reduced trail width and modified access road will be required because of the limited amount of room between tower NC-36 and the hot oil pipeline. The modifications to the access road would include a slight shift in alignment and the addition of Type F Curb and Gutter along the east edge of the road. The existing access road is not gated since both Florida Power and Abilities utilize it, therefore vehicular access will not be prohibited without some treatment. Preliminary investigations suggest that Type F Curb and Gutter (or just a vertical curb) would provide the most economic separation between the trail and access road.



Bollards will be utilized at the trails intersection with Whitney Road to prevent vehicular access onto the trail at that location. This information, however, is based on the information shown on the 200 scale conceptual plan sheets and field observation. Once detailed survey is obtained, a better solution may be found to provide the desired offset. Continued coordination with and review by Florida Power staff will be required during design.

Approximately 800 feet beyond the Substation, the trail will shift south through the corridor as it crosses the 62nd Street dedicated right-of-way, Long Branch Creek and skirts to the south of tower NC-34. An easement may be needed for crossing the dedicated 62nd Street right-of-way, but the remainder of the trail will be in Florida Power right-of-way. The creek crossing will be accomplished with a new open bottom box culvert.



Once past the Long Branch Creek, the corridor continues south and the trail alignment shifts to the west side of the corridor before crossing Long Branch Drive and SR 686/ Roosevelt Boulevard at-grade (Sheet 25 of 41). The crossing of Long Branch Drive will be signed accordingly to warn traffic on the driveway and trail of the approaching intersection. There is an existing traffic signal at the intersection of SR 686/Roosevelt Boulevard and 62nd Street North. Pedestrian signal and striping features are present at this crossing.

From this intersection, the trail will run parallel to 62nd Street North for approximately half a mile (2,700 feet) before crossing 150th Ave./Automobile Road (Sheet 26 of 41). Through this section, the trail crosses six minor driveways at-grade. The



trail will cross 150th Avenue/Automobile Road and 62nd Street North at-grade through the signalized intersection. There are pedestrian signal and striping features present at this crossing (Sheet 26 of 41).

From this intersection, the trail continues parallel to 62nd Street North for another 3,400 feet before the power corridor and the trail turn east. Although the Florida Power right-of-way is approximately 175-feet wide, the location of parking areas within the corridor and numerous driveways crossing the corridor suggest that the alignment would be best if located adjacent to 62nd Street N. The current two-lane roadway has a rural section with an adjacent 10-foot swale system. There is approximately 11 feet between the swale top of bank and the right-of-way line, where there exists wooden power poles. This is not sufficient room to accommodate a two-way multi-purpose trail, so modifications to the 62nd Street N. typical section will be required in this section in order to safely accommodate the trail alignment. It does not appear that there are any plans in the Pinellas County MPO 2020 Long Range Transportation Plan to make improvements to 62nd Street N. If this is indeed the case, then it is recommended that the swale be piped and curb and gutter added along this stretch of the 62nd Street N. northbound lane. By making these changes, the 15-foot paved trail could be accommodated between the back of curb and the existing right-of-way line.



As the trail alignment continues through this section, it will cross nine minor driveways at-grade before it will cross 142nd Avenue North at-grade (Sheet 27 of 41). As part of this crossing, the trail will need to pass over two existing drainage ditches running on either side of 142nd Avenue North. Drainage pipes will be placed at these crossings. There are pedestrian signal and striping features present at the

signalized intersection of 142nd Avenue North and 62nd Street North.

As the power corridor heads east, the trail will pass to the north of tower NC-27 and then transition to the southern right-of-way line. The trail will then cross 61st Street N. and 60th Street N. at-grade. Approximately 180 feet east of 60th Street N., a drainage pipe will be required for the trail to cross a drainage ditch. After the ditch crossing, the trail will pass between towers NC-26 and UL-5 be reduced to a narrow typical section.

The grades around the ponds between towers NC-26 and UL-5 and 58th Street North, as well as horizontal constraints dictate that a reduced section with handrail would be most appropriate here. The alignment will extend past the north side of the retention pond, turn south and parallel to Icot Boulevard briefly in order to cross Icot Boulevard at-grade, and stay within the Florida Power right-of-way. Once across Icot Boulevard, the trail will continue along the north side of a series of retention ponds, meandering around existing parking lots, until it reaches 58th Street N. The trail alignment, maintains the narrow section and will turn south, cross an existing bridge structure, and run parallel to 58th Street N. for approximately 200 feet before crossing the roadway at-grade. The existing sidewalk along 58th Street North is 10 to 12 feet wide and modifications to the existing structure are not recommended. The SR 688/Ulmerton Road crossing is located just beyond 58th Street North and marks the end of Segment C and the beginning of Segment D. The Ulmerton Road crossing is a major crossing that may require a significant overpass. It therefore represents a logical division between segments. Depending on the construction sequencing, appropriate striping



and signage should be provided to inform trail users that the bicycle access will be ending and that further advancement is not advised.

Segment C – Alternate Routes

There are several locations within Segment C where alternatives to the preferred alignment are shown on the Conceptual Plan Sheets located immediately following this section of the report. These occur at the U.S. 19 and Allen's Creek crossing, at the at-grade crossing of Haines Bayshore Road, and at towers UL-5 and NC-25 before Icot Boulevard. The following are descriptions of these alternatives.

As previously mentioned, the original preferred trail alignment proposed crossing Allen's Creek via a low level concrete structure and then turning to cross U.S. 19 with a trail overpass. Allen's Creek presents the most difficult environmental challenge along the trail corridor. The Creek is a tidally influenced waterway interspersed with a mangrove shoreline and vegetated intertidal communities, bends through the right-of-way necessitating a long water



crossing if the trail follows the power corridor. Several other physical constraints resulted in the alternate alignment over Allen's Creek. Due to the

long span between towers HD-44/NC-41 and HD-45/NC-40, the overhead power lines have very low sag as they cross the creek. The more direct "open water" alternate route would result in potential conflict with the power line "sag" between the towers on either side of the creek and would require a more extensive and costly structure to cross the wider portion of the creek.

In order to eliminate these concerns, and to cross the creek at a more narrow section, the trail alternate would turn southwest upon approaching the creek, leaving the right of way for a short stretch. A 1,050-foot long section of low level concrete structure

would carry the trail along the outer edge of the mangrove fringe as the trail is directed to the narrower open-water location west of the power line right-of-way. As with any linear project, unavoidable wetlands impacts would be encountered. The impacts at the creek would be minimized to the greatest extent. It is anticipated that piling installation within mangrove areas will be limited to approximately 200' on the north side of Allen's Creek. Landfall of the Trail structure on the south side of the creek is proposed at a point where mangrove vegetation is sparse or absent. A top-down construction technique has been recommended in the structures section of the Design Approach to minimize impacts to the mangroves and creek bottom. Ambient water quality will be thoroughly investigated during the project's design phase.

An additional alternate that was considered for crossing Allen's Creek was the construction of the bridge adjacent to the hot oil pipeline. This could minimize mangrove impacts and cross the creek at a narrow location, however, any installation of pilings in the vicinity of the pipeline has been avoided.

Immediately after crossing the creek, this alternate trail alignment would turn east, running parallel to the creek and back into the power corridor. An overpass structure would be necessary to carry the trail over US 19. An oblique angled crossing is proposed in order to minimize the impacts to the mangroves located on both sides of the highway. The trail approach to this structure may be accomplished either by a straight grade-ramp, which would require mechanically stabilized earth (MSE) retaining walls, or by a "Z" type ramp on structure. Detailed survey is required to determine the available clearance between the overhead power lines of towers HD-45 and NC-40 and existing



ground. Once this information is known, the appropriate approach method could be chosen.

The preferred alignment for crossing Haines Bayshore Road is for the trail to turn east and extend approximately 600-feet along the north side of the roadway before turning south to cross the roadway at-grade. The alternate trail alignment for this crossing would utilize the existing signalized intersection of Haines Bayshore Road and US 19. Although the 1999 AADT for Haines

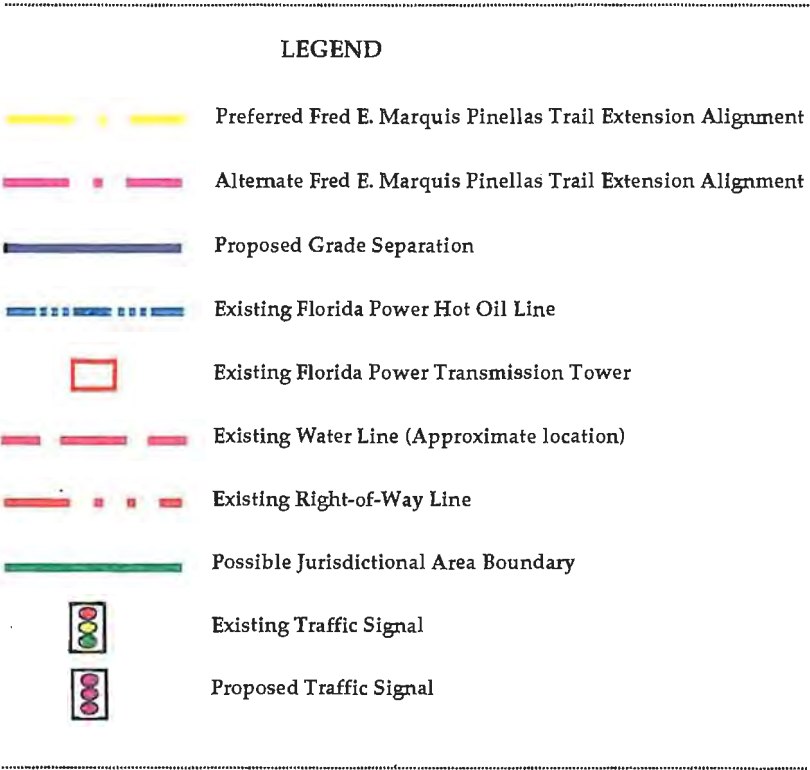


Bayshore Road do not indicate high volumes of traffic, it would seem logical to utilize the existing signal rather than introduce the mid-block crossing. There are several factors involved with crossing Haines Bayshore Road at its intersection with US 19 and proceeding along the south side of Haines Bayshore Road that cause concern. The first is the heavy amount of right turn movements from Haines Bayshore to northbound US 19. Although signage could be installed to warn both trail users and roadway users of the trail/roadway intersection, there is a danger of a right turning vehicle performing a rolling stop, rather than a complete stop, and not seeing or expecting to see a trail user. If the trail user would cross Haines Bayshore at the signalized intersection, they would then turn east and travel along a 10-foot corridor bordered by a chain line fence and Type F Curb & Gutter, both immediately adjacent to the trail. According to the design criteria being applied to this project, the trail users would essentially have a 6-foot bi-directional trail width with a minimum 2-foot clearance to the chain link fence and to the back of curb. This 6-foot width fails to meet the AASHTO minimum bi-directional trail width requirement of 8-feet. If the trail users were to continue along the south side of Haines Bayshore Road, they would be required to make a sharp turn (90°) south to travel along the Quality Boat trail easement. Due to the proximity of

a wall recently constructed along the Quality Boats eastern property/western easement line, a maximum radius of 10-feet could be designed for the trail. This configuration presents a significant sight distance problem between eastbound travelers and northbound travelers as they approach the turn. Northbound users would also be faced with the possibility of missing the sharp turn and running into the roadway. Although signage could be placed on the trail to warn of the turn, human error could still occur.

The preferred alternative for the trail at towers UL-5 and NC-26 is for the trail to veer to the center of the corridor before the towers in order to pass between the towers (Sheet 27 of 41). The trail then continues straight to pass by the northwest side of the first of a series of ponds. An alternative is for the trail to pass by the southeast side of the towers. Then, after the towers the trail would veer sharply across the corridor in order to pass by the northwest side of the first pond as in the preferred alternative. Again, depending upon results of more detailed field surveys, this alternative alignment may allow more space for the trail than a route between the towers.

The conceptual alignment for Segment C is provided on the following plan sheets (Sheets 22A to 28A):





Segment C



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 22A of 41
3.35 Master Plan



Segment C



**Fred E. Marquis
Pinellas Trail Extension**
on Florida Power Right-of-Way

Conceptual Alignment Plan
Sheet 23 of 41

3.36 Master Plan





Segment C



**Fred E. Marquis
Pinellas Trail Extension**
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 24 of 41
3.37 Master Plan



Segment C



**Fred E. Marquis
Pinellas Trail Extension**
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 25 of 41
3.38 Master Plan



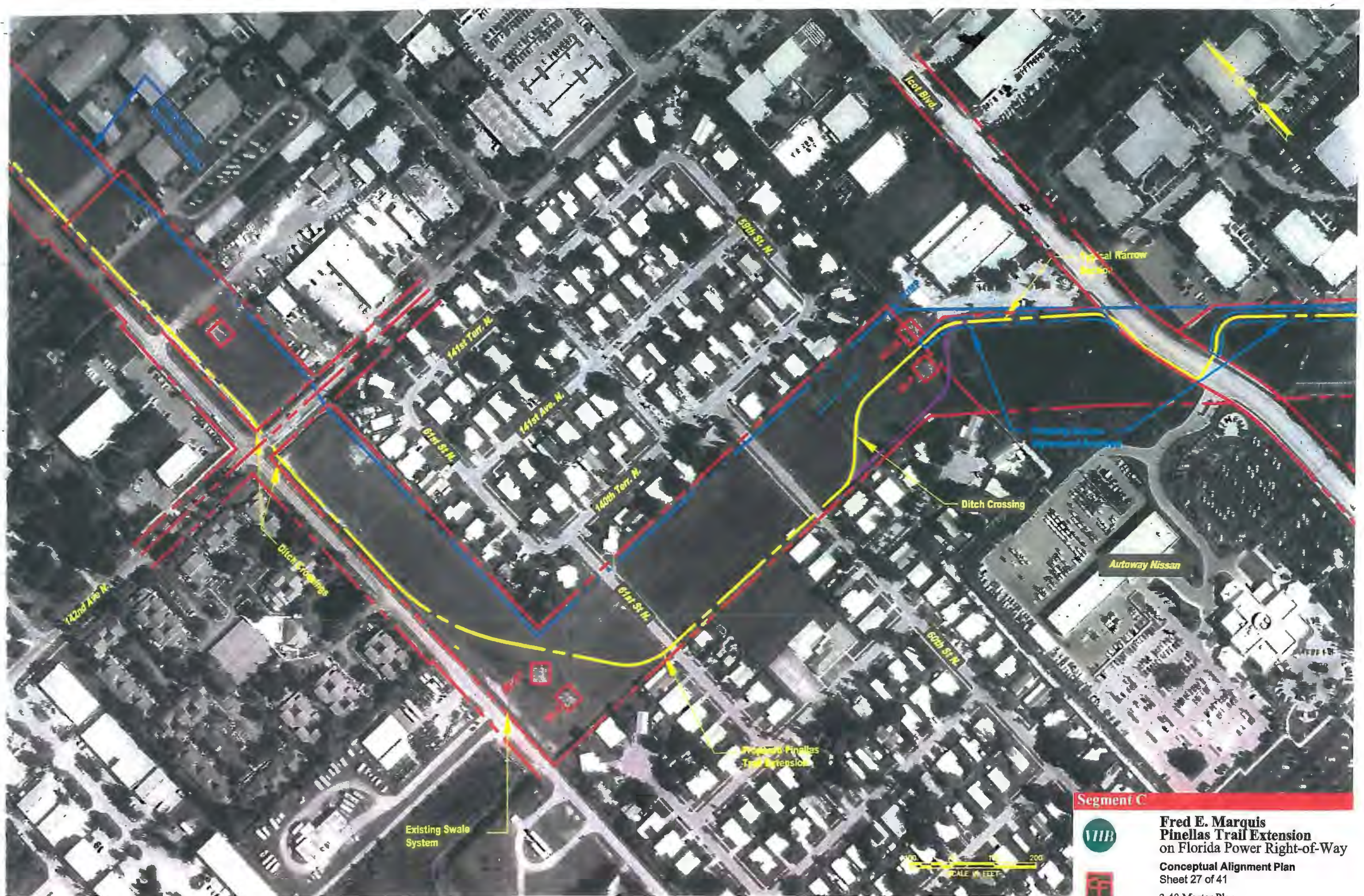


Segment C



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 26 of 41
3.39 Master Plan





Segment C



**Fred E. Marquis
Pinellas Trail Extension**
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 27 of 41
3.40 Master Plan



Segment C



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 28A of 41
3.41 Master Plan



Segment D – Preferred Alignment - From SR 688/Ulmerton Road to I-275

Segment D will begin at the trail crossing with State Route 688/Ulmerton Road and will extend south and east to I-275. This segment of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-



Way is approximately 4.47 miles long. For approximately the first half of this length, the trail runs through commercial and business areas. The trail proceeds adjacent to the Pinellas County Solid Waste Facility before passing through a mix of residential, business and recreational areas.

Approximately 1,000 feet of the trail segment diverges from the Florida Power corridor, running in line with 110th Avenue and then



34th Street North before rejoining the corridor. The following is a description of the Segment D alignment as it passes from north to south. Following this description are Conceptual Plans (Sheets 28 to 36 of 41) that graphically depict the trail alignment.

Beginning at the west end of the segment, the trail crosses SR 688/Ulmerton Road and the Cross Bayou Canal, Blueway/Canoe Trail (Sheet 28 of 41). FDOT currently has a project in design, which will widen Ulmerton Road from four to six lanes. As a part of this project, FDOT is investigating the possibility of replacing the Cross Bayou Canal box culvert with a bridge in order to provide enough clearance for the canoe trail. FDOT is proposing to design the bridge to accommodate not only a canoe trail, but the 15-foot wide Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way as well. The preferred alignment would pass under Ulmerton Road using

this bridge. It would then cross the Cross Bayou Canal via a separate concrete "boardwalk" structure.

Once across the Canal, the trail will cross the Rubin Center entrance driveway at-grade and then turn south to meander along the Florida Power easement. An easement or right-of-way acquisition will be required between Ulmerton Road and the Ulmerton Road Substation. In an effort to minimize the width of land required, the recommended trail width was reduced. In addition, there are several areas along this portion of the corridor that will be horizontally constrained and the full 15-foot width will not be achieved. The trail will extend between towers NC-23 and UL-2 and parallel to the Center's parking. As the trail approaches the Ulmerton Road Substation, special attention will need to be given to the at-grade crossing of the Rubin Center's access roadway near Tower NC-22.

Once the trail enters the Ulmerton Road Substation property (Sheet 29 of 41), the alignment will turn towards the south, crossing the hot oil pipeline twice, and then extending east, parallel to and 15 feet south of the hot oil pipeline. The typical section should be returned to the desired 15-foot width through this section.



Beyond the substation and training area, the alignment turns south and passes east of tower NC-20. As the trail approaches 126th Avenue, the alignment transitions across the right-of-way to cross 126th Avenue at-grade and pass to the west of tower HD-63. The trail will then turn east to cross a driveway at-grade (Sheet 30 of 41) and continue in a southeasterly direction, parallel to and 15 feet off the hot oil pipeline.

There is an existing utilization agreement between tower NC-18 and 49th Street North/Disston

Boulevard. In order to eliminate impacts to this utilization area, the trail alignment shifts approximately 50 feet west at NC-18.



When the trail reaches 49th Street North/Disston Boulevard, the typical section narrows as the trail turns south parallel the roadway, crossing 118th



Avenue and then 49th Street North at-grade at the existing traffic signal. There is an existing pedestrian signal and striping features at this

crossing to accommodate the trail. A marginal wetland area was observed adjacent to and immediately northwest of this intersection. This area may ultimately be deemed non-jurisdictional, however, its presence will be considered in the final design

The trail will continue along 118th Avenue and re-enter the Florida Power right-of-way, turning south just west of the retention pond. The preferred alignment consists of a 100-foot radius, requiring an easement as the trail turns south from 188th Avenue to rejoin the Florida Power right-of-way. In order to eliminate the need for an easement, the radius would have to be reduced to less than 50-feet. It is possible that detailed topographic and right-of-way survey will show that a greater radius may be used. For the purposes of this Master Plan, the minimum 100-foot radius and an easement will be called for. The alignment continues south, crossing St. Mary Boulevard, at-grade, then turning east just north of 110th Avenue, leaving Florida Power right-of-way (Sheet 31 of 41). To minimize right-of-way impacts, a narrow typical section is recommended in the section parallel to 110th Avenue. There may be some Florida Power distribution relocation impacts along this portion of the trail.

The trail alignment will cross 43rd Street North, at-grade, and continue east in the 34th Street dedicated right-of-way located along the south property line of the Pinellas County Solid Waste Facility. Access is currently provided along this dedicated right-of-way to a privately owned parcel just east of the intersection of 43rd Street North and 110th Avenue. A narrow typical section bordered by hand rail or fence will be required for this section in order for this access to continue. Parallel to and south of the 34th Street dedicated right-of-way is a haul road being used by the County to access the borrow pit/pond construction area located approximately 1500-feet east of 43rd Street North. This construction is part of the Brian Dairy Road project (Sheet 32 of 41). It does not appear that this haul road will have any impact to the trail alignment. The trail will continue with the narrower cross-section as it extends east and then turns south (Sheet 33 of 41).

The typical section may resume to the preferred 15-foot width as the trail approaches the area bordered by the Mainlands and MCI Drive (Sheet 34 of 41). Through this section, the trail is bordered by a concrete block wall and a drainage swale. As the trail approaches the Florida Power corridor, it must cross the drainage ditch, this will require an open bottom box culvert (Sheet 35 of 41). At this point the trail turns east and extends down the Florida Power right-of-way, maintaining a 15-foot offset from the hot oil pipeline. Approximately 700 feet east of tower NC-6, the trail will veer out of the right-of-way. Miele Lake and a small pond south of it appear to be hydraulically connected. According to the drainage basin study that has been reviewed, this connection is not supposed to exist. This area falls within the Roosevelt Creek Basin. Miele Lake (lake No. 99-02-4) to the north is within sub-basin 23-02-50, and the smaller pond to the south (lake No. 99-02-5) is within sub-basin 23-02-55. While the area is relatively flat, the drainage study indicates the basin divide runs E-W right across the land between these ponds. Plans are to build up the trail for this stretch to alleviate

the situation. Installation of a series of 18" pipes is estimated at this time since the existing conditions must be verified to determine a) if the hydraulic connection is necessary and b) how much flow is "supposed" to be occurring. Planning level drainage analysis indicates building up this "causeway" between the ponds would not have a negative impact on the natural and beneficial flow patterns of this area, but would be in concert with them. Further study will be required during design to determine the appropriate solution. The trail should be reduced to a narrow typical section in this area. Once the trail has cleared this area, it will resume its preferred dimensions and 15-foot offset south of the hot oil pipeline and continue east.

Just east of tower NC-5, the alignment shifts towards the center of the right-of-way. The trail will cross a drainage ditch, which will require a large pipe or small box culvert, and then 28th Street North at-grade. A mid-block pedestrian signal is recommended for this crossing. The trail continues east for approximately another 2,000 feet and Segment D is proposed to end just west of I-275.

Segment D – Alternate Routes

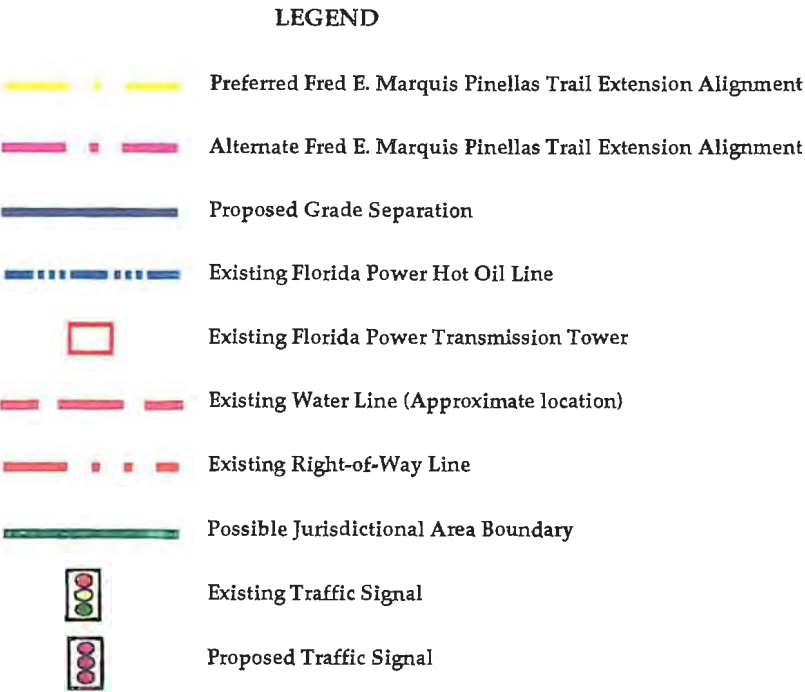
Alternative routes are proposed at three locations along Segment D. These are at the Ulmerton Road crossing and at the 126th Avenue crossing.

The preferred alignment at Ulmerton Road is proposed to cross under the roadway by way of the proposed FDOT bridge over the Cross Bayou Canal (Sheet 28 of 41). If for some reason the FDOT should not continue with this design, then two alternate routes have been provided. The first alternate would cross Ulmerton at-grade at the signalized intersection with 58th Street North. The trail would then run parallel to Ulmerton Road, separated by a barrier wall, until it ties back into the preferred alignment at the Cross Bayou Canal bridge crossing.

This alternate would require approximately 550 feet of swale to be piped and covered. The second alternate route would cross Ulmerton Road via a pedestrian overpass. This structure would cross Ulmerton Road and the Cross Bayou Canal. There are constructability issues with this alternative due to the location of the overhead powerlines. Once back at-grade, this alternative would cross the Rubin Center Driveway at-grade and continue along the preferred alignment.

Due to the location of numerous utility poles just north of tower HD-63 (Sheet 29 of 41), an alternate at-grade crossing has been proposed. This alignment would cross 126th Avenue and pass between towers HD-63 and NC-19. The alignment would then cross the driveway at-grade and return to the preferred alignment (Sheet 30 of 41).

The conceptual alignment for Segment D is provided on the following plan sheets (Sheets 28 to 36):





Segment D



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way

Conceptual Alignment Plan
Sheet 28 of 41

3.44 Master Plan



100 0 100 200
SCALE IN FEET



Segment D



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 29 of 41
3.45 Master Plan



FP Hot Oil
15.0' R/W Line (Proposed)

Proposed Access Point Required

Proposed Pinellas
Trail Extension

Segment D



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 30 of 41
3.46 Master Plan

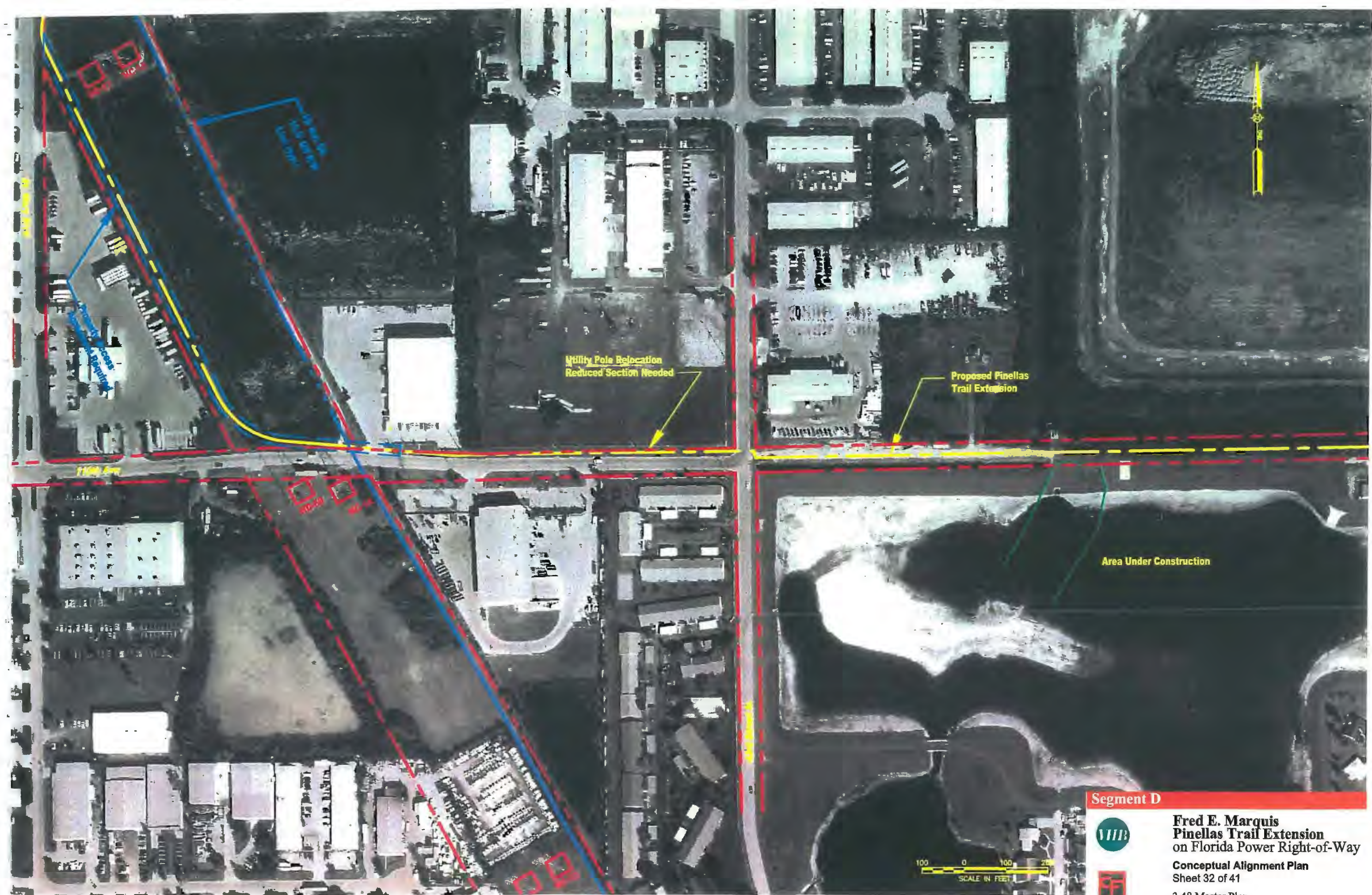




Segment D



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 31 of 41
3.47 Master Plan



Segment D



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 32 of 41
3.48 Master Plan



Segment D



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 33 of 41
3.49 Master Plan



Segment D



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 34 of 41
3.50 Master Plan



Segment D



**Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way**
Conceptual Alignment Plan
Sheet 35 of 41
3.51 Master Plan



Segment D



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 36 of 41
3.52 Master Plan

Segment E – Preferred Alignment - From I-275 to San Martin Boulevard

Segment E will begin at the trail crossing with Interstate 275 and will extend east to the project end at San Martin Boulevard near the Weedon Island Preserve. This segment of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way is approximately 2.86 miles long and runs through a business and industrial area for much of its length, with residential areas near the end of the segment. The segment will provide an important connection between the Pinellas Trail network and the proposed Friendship Trail. The following is a description of the Segment E alignment as it passes from north to south. Following this description are Conceptual Plans (Sheets 37 to 41) that graphically depict the trail alignment.



Beginning at the west end of the segment (Sheet 37 of 41), the crossing of I-275 will be accomplished with an overpass. From the overpass, the trail will continue through the middle of the Florida Power corridor as a typical section with 15-foot wide paved trail. Approximately 800 feet east of the I-275 right-of-way, the trail shifts to the north side of the Florida Power corridor to avoid a jurisdictional area. While maintaining a 15-foot offset from the hot oil pipeline, the alignment turns north just past the jurisdictional area. At the point where the corridor and trail turn north, the overhead electrical lines transition to buried electrical lines. Concurrent with this change, the width of the power corridor right-of-way shrinks from approximately 225 feet to 100 feet. The reduced corridor width continues to the end of the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way. The trail alignment proceeds north for approximately 900 feet before turning east. As the alignment continues to maintain the 15-foot offset

from the hot oil pipeline, it will cross 18th Street North and 16th Street North at-grade (Sheet 38 of 41).

An area east of 16th Street North and confined to the limits of the transmission line corridor supports a continuation of upland and wetland vegetation with only marginal hydric soil characteristics. The area is expected to support a dominance of wetland indicator species only during periods of extended rainfall. This situation appears to have resulted from the elevation of the building area for all of the construction between 16th Street North and Gandy Boulevard. The corridor is now approximately one-foot below the surrounding land which captures and retains water for the majority of its length. Further analysis of the jurisdictional status of this area is needed. Placement of fill is believed to be appropriate for this area. The area occurs over well-drained surface soils and is believed to exhibit saturated soils of short duration as evidenced by the existing vegetation. It is anticipated that the Trail's drainage design will consider this artificially created "low" area and alleviate any potential need for bridging.

As the trail extends east, the preferred alignment will cross the U.S. 92/Gandy Blvd. and 9th Street North/Dr. M.L. King Street intersection (Sheet 39 of 41). This will be accomplished via several at-grade crossings. As the trail approaches from the west, the trail will cross the frontage road with a mid-block crossing, followed by a ditch crossing which will require a pipe or small culvert. The trail will then cross the U.S. 92/Gandy Blvd. westbound lanes. A pedestrian signal and striping features, as well as warning signs for vehicles, will need to be installed at this existing traffic signal. The trail will then extend south for approximately 400 feet before crossing the U.S. 92/Gandy Blvd. eastbound lanes.



Once within the gore area of the intersection, the trail will turn east and cross 9th Street North/Dr. M.L. King Street. Again, a pedestrian signal and striping features, as well as warning signs for vehicles, will need to be installed at this existing traffic signal. FDOT has jurisdiction of US 92/Gandy Boulevard and Pinellas County has jurisdiction of 9th Street North/Dr. M.L. King Street. Coordination with each jurisdiction will occur during the preliminary engineering phases as detailed survey is obtained.



After paralleling U.S. 92/Gandy Blvd. for approximately 500 feet, the trail alignment will turn east back onto Florida Power right-of-way. Maintaining a 15-foot offset from the hot oil pipeline, the trail continues east and crosses a tidal creek with mangroves. A low level concrete boardwalk will be required for this crossing. Continuing east, the trail will cross 5th Street North at-grade (Sheet 40 of 41).



As the alignment approaches 4th Street North, the trail turns south to parallel 4th Street North for approximately 600 feet before crossing Executive Center Drive and 4th Street North at-grade. The trail will then turn north to re-enter Florida Power right-of-way.



Once back within the Florida Power right-of-way, the trail turns east and crosses 2nd Street North at-grade. Maintaining a 15-foot offset from the hot oil pipeline, the trail continues east, crossing a drainage canal (Sheet 41 of 41). A low level concrete boardwalk will be required for this crossing.

After the canal crossing, the trail continues along the Florida Power right-of-way to terminate at San Martin Boulevard. It is at this point which the Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way is proposed to tie into the Future Friendship Trail and future St. Petersburg First Street bike path. Directly from the trail terminus at San Martin Boulevard is a canoe trail launch site. A mulched area currently serves as parking for the trailhead. No other amenities are located here.



This would be a strategic location for a Fred E. Marquis Pinellas Trail - Extension on Florida Power Right-of-Way trailhead. If this site is utilized as a trailhead, special attention must be given to the roadway crossing of San Martin Boulevard. Sight distance along the roadway is limited by the horizontal geometry and roadside vegetation.

Segment E – Alternate Routes

Due to the geometric design and traffic volumes of the U.S. 92/Gandy Blvd. and 9th Street North/Dr.M.L. King Street intersection and 4th Street North, alternate routes requiring overpasses have been considered. Although the preferred at-grade alignments are suitable for existing conditions, future conditions may warrant overpasses to be constructed.



Two alternates have been shown for the U.S. 92/Gandy Blvd. and 9th Street North/Dr. M.L. King Street intersection. The first alternate would cross the frontage road and U.S. 92/Gandy Blvd. with one overpass and the intersection of eastbound U.S. 92/Gandy Blvd. with 9th Street North/Dr. M.L. King Street. The second alternate route

would continue straight across the entire intersection (and parallel to Florida Power's hot oil pipeline) with one continuous structure.

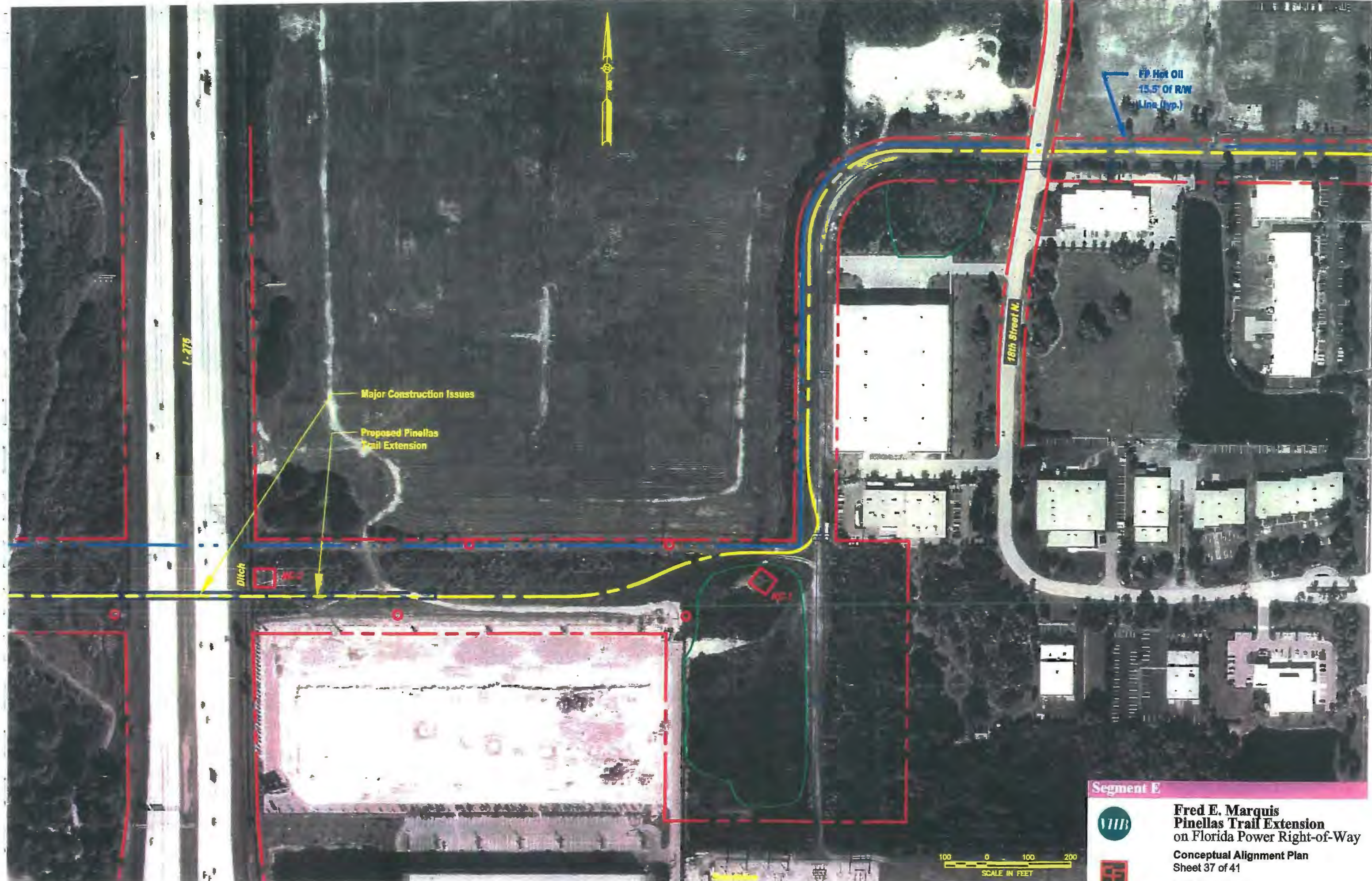


The overpass alternate for the 4th Street North crossing would continue straight across the roadway and parallel to the hot oil pipeline.

The conceptual design for Segment E is provided on the following plan sheets (Sheets 37 to 41):

LEGEND

	Preferred Fred E. Marquis Pinellas Trail Extension Alignment
	Alternate Fred E. Marquis Pinellas Trail Extension Alignment
	Proposed Grade Separation
	Existing Florida Power Hot Oil Line
	Existing Florida Power Transmission Tower
	Existing Water Line (Approximate location)
	Existing Right-of-Way Line
	Possible Jurisdictional Area Boundary
	Existing Traffic Signal
	Proposed Traffic Signal



Segment E

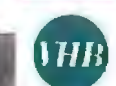


Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 37 of 41
3.55 Master Plan

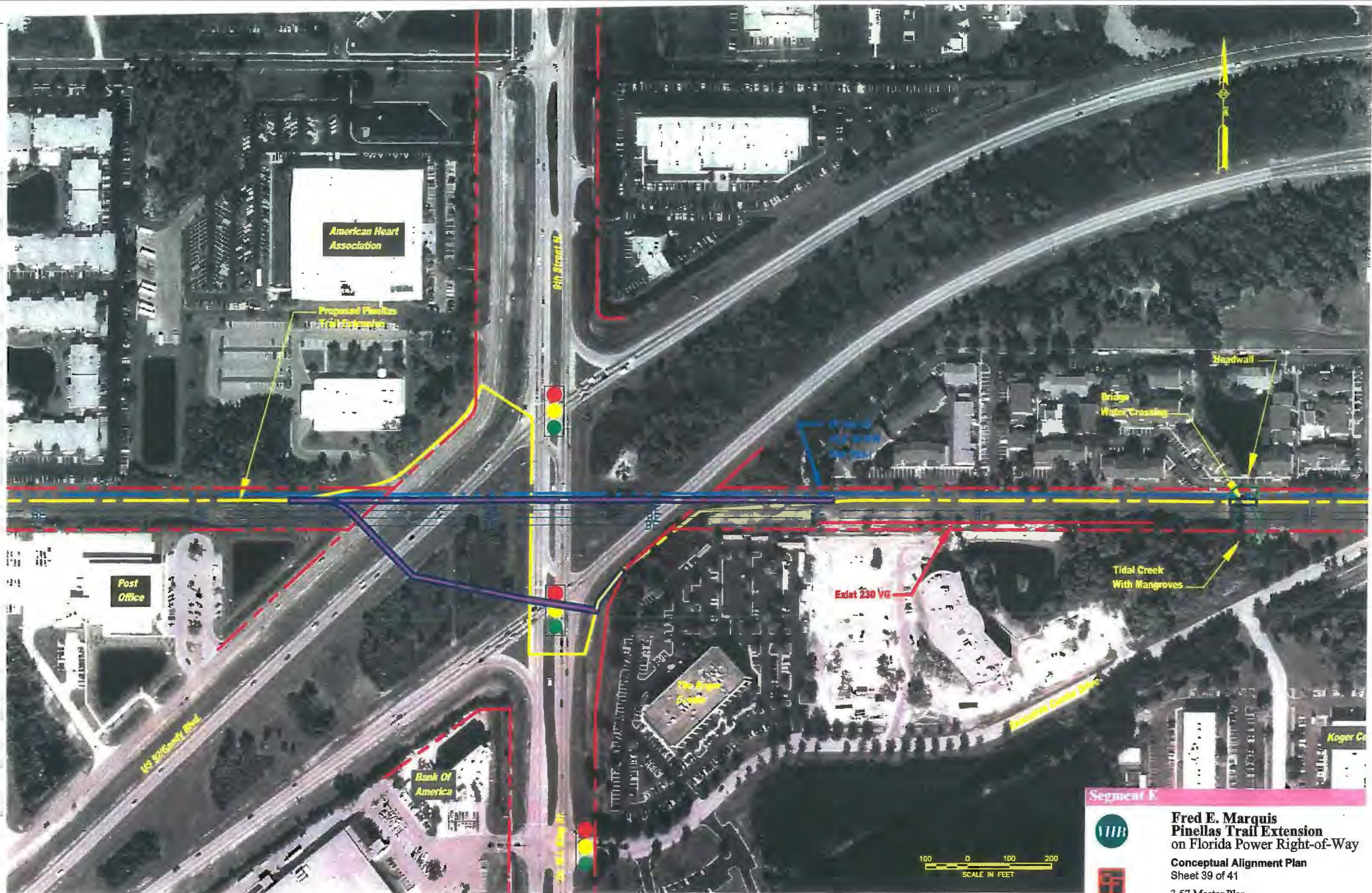




Segment E



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 38 of 41
3.56 Master Plan



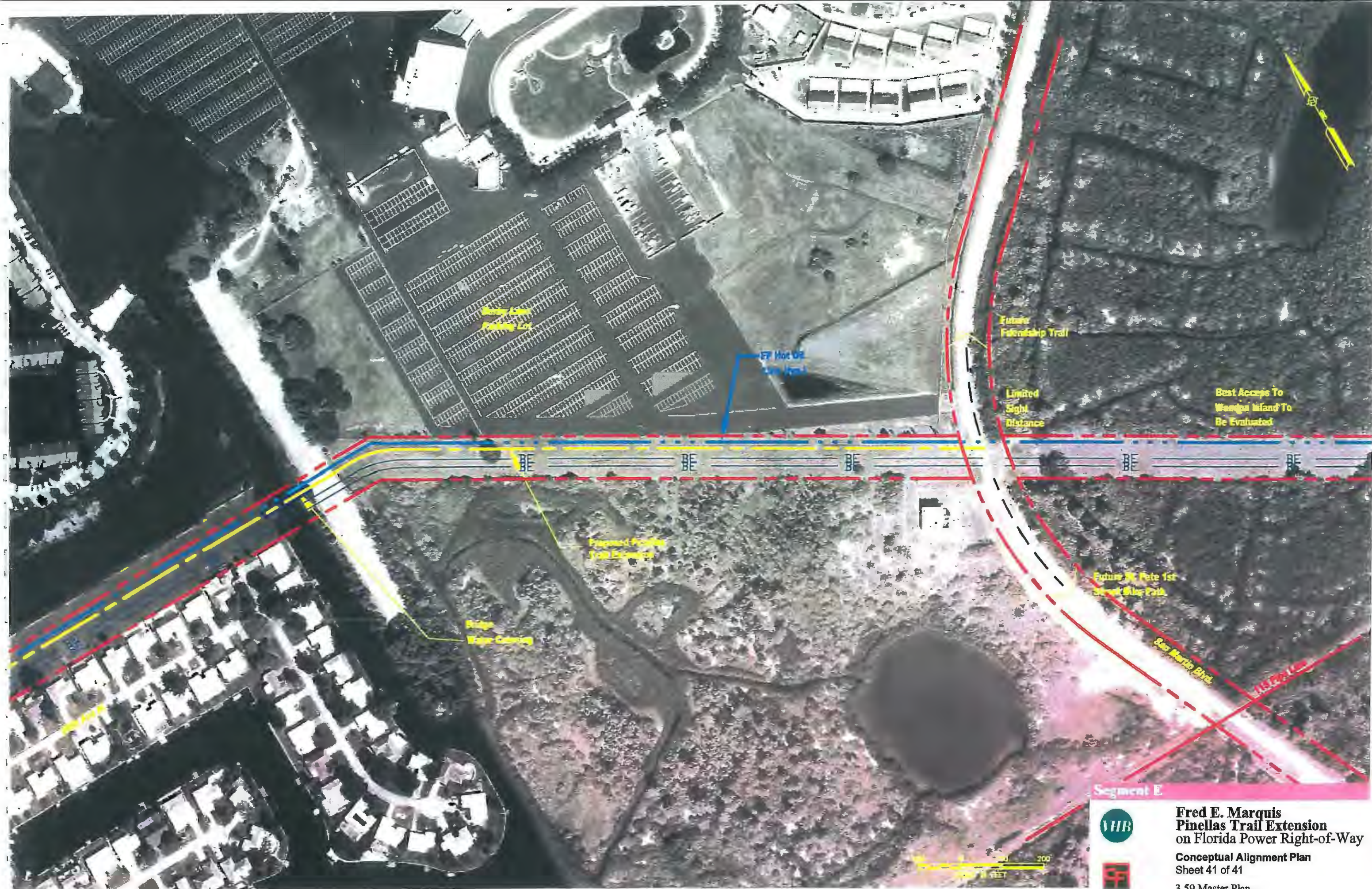
Segment E



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 39 of 41
3.57 Master Plan



Fred E. Marquis
Pinellas Trail Extension
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 40 of 41
3.58 Master Plan



Segment E



**Fred E. Marquis
Pinellas Trail Extension**
on Florida Power Right-of-Way
Conceptual Alignment Plan
Sheet 41 of 41
3.59 Master Plan

UE TO RECENT INTERSECTION IMPROVEMENTS AT N. OLD COACHMAN & NE COACHMAN RD., DRAINAGE MODIFICATIONS ALONG NE COACHMAN RD., A NEW ALIGNMENT (APPROXIMATELY FROM STA+11.38 TO STATION 453+50) AND DESIGN WILL NEED TO BE INCORPORATED INTO PLANS. SURVEY INFORMATION IS OBTAINED.

SAWCUT, REMOVE, AND REPLACE EXIST. SW

BEGIN CONST.
STA. 443+11.36

SAWCUT EXIST. SW

CURVE DATA CL12-1
PI STA. = 443+30.44
DELTA = 42° 17' 03" (LT)
D = 381' 58' 19"
T = 5.80
L = 11.07
R = 15.00
PC STA. = 443+24.64
PT STA. = 443+35.71

CURVE DATA CL12-2
PI STA. = 443+77.73
DELTA = 56° 46' 47" (RT)
D = 818' 30' 40"
T = 3.78
L = 6.94
R = 7.00
PC STA. = 443+96.16
PT STA. = 444+03.95

CURVE DATA CL12-6
PI STA. = 447+13.20
DELTA = 2° 44' 46" (LT)
D = 2' 01' 41"
T = 67.71
L = 135.40
R = 2,825.00
PC STA. = 446+45.49
PRC STA. = 447+80.89

CURVE DATA CL15-1
PI STA. = 447+86.95
DELTA = 13° 49' 18" (RT)
D = 114' 35' 30"
T = 6.06
L = 12.06
R = 50.00
PRC STA. = 447+80.89
PRC STA. = 447+92.95

CURVE DATA CL15-2
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DELTA = 14° 19' 12" (LT)
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T = 6.28
L = 12.50
R = 50.00
PRC STA. = 447+92.95
PCC STA. = 448+05.45

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PI STA. = 449+50.25
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PRC STA. = 451+05.75

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T = 222.79
L = 444.66
R = 2,825.00
PRC STA. = 451+05.75
PCC STA. = 455+50.41

LEGEND

EXIST. S/W TO BE REMOVED

At all public road crossings that do not have a separate point of access other than the Trail, the Trail will need to be constructed to FDOT weight standards to accommodate Duke Energy's 80,000 pound trucks

REVISIONS	SURVEY BOOK No.: SURV. BOOK	PINELLAS COUNTY, FLORIDA	PROGRESS ENERGY TRAIL	DATE: 05/09
	SURVEY DIVISION BY DATE	Public Works	PLAN	P.I.D. NO.: 920499
	DESIGNED CWC/SJA 5/06	BRENT D. McMURTRIE, PE	STA. 443+11-452+20	SURVEY FILE NO.:
	CHECKED CWC/SJA 5/06	ENGINEER		SHEET 10 OF 39
	DRAWN CWC/SJA 5/06	FLORIDA PROFESSIONAL ENGINEER NO. 66604		DEPT. DRAWING
	CHECKED SAL 5/06	DATE		

AYRES ASSOCIATES
8875 Hidden River Parkway
Suite 200
Tampa, Florida 33637
(813) 918-8688
Certification of Authorization No. 4356

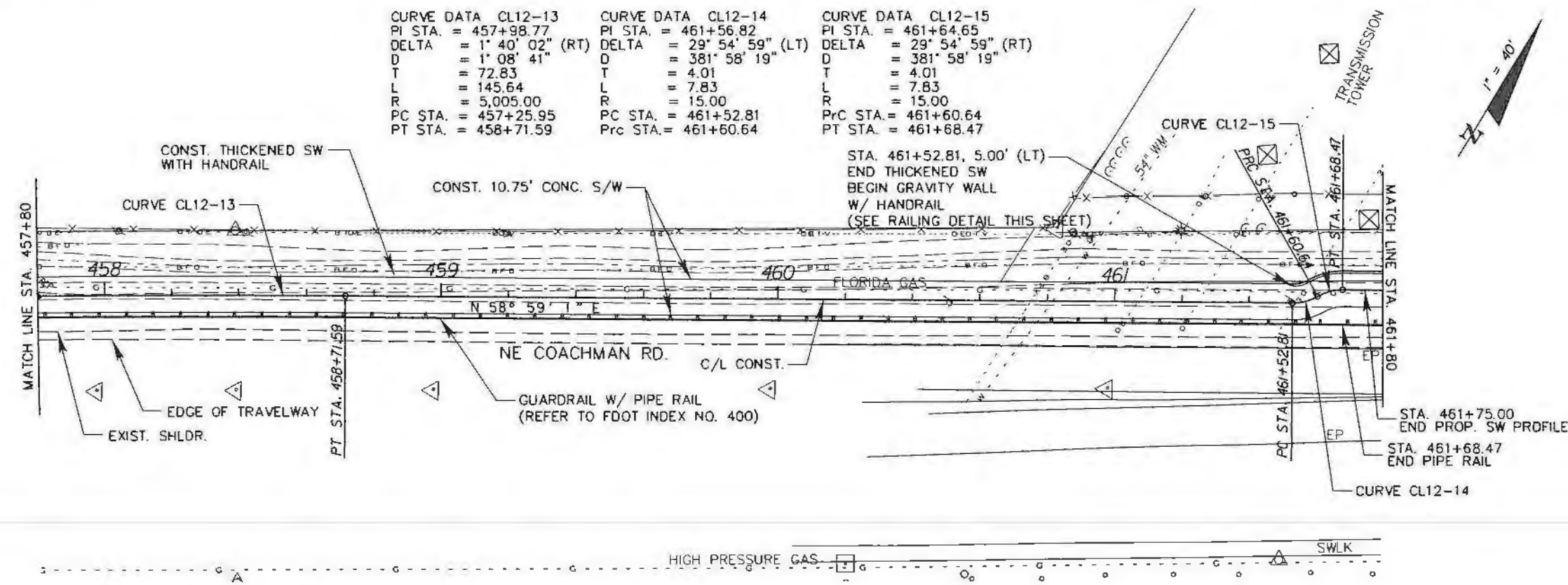
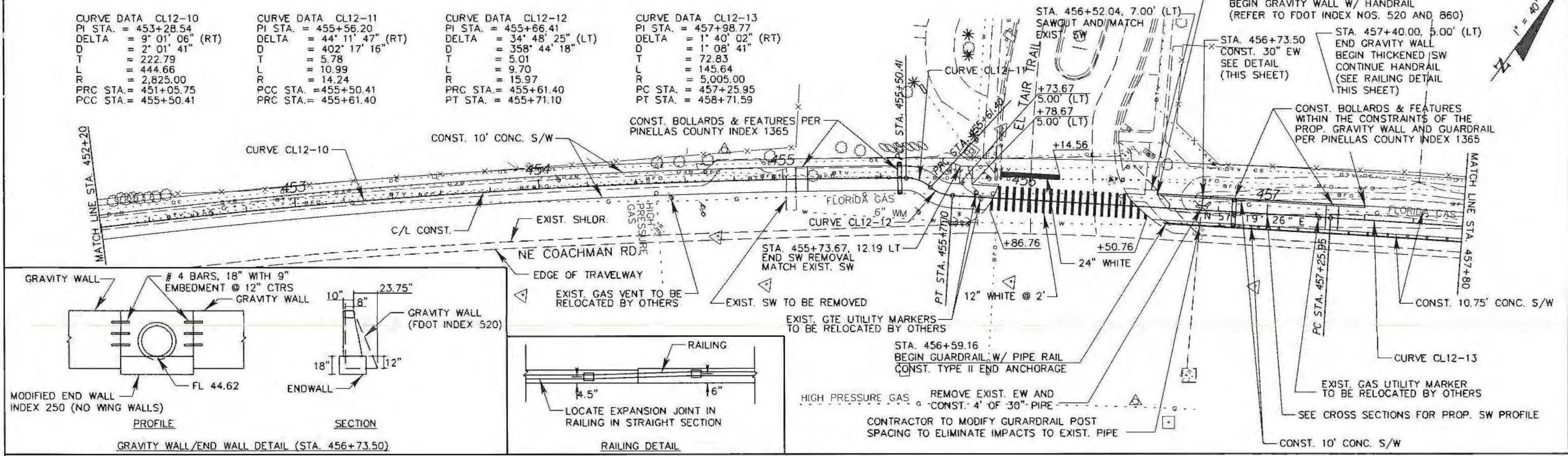
Progress Energy **Pinellas County**

CURVE DATA CL12-10
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 L = 444.66
 R = 2,825.00
 PRC STA. = 451+05.75
 PCC STA. = 455+50.41

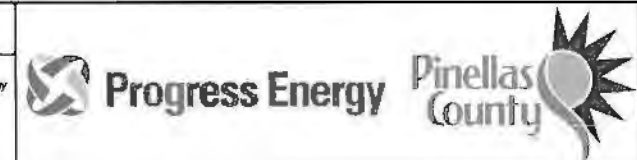
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 PCC STA. = 455+61.40

CURVE DATA CL12-12
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 DELTA = 34° 48' 25" (LT)
 D = 358° 44' 18"
 T = 5.01
 L = 9.70
 R = 15.97
 PRC STA. = 455+61.40
 PT STA. = 455+71.10

CURVE DATA CL12-13
 PI STA. = 457+98.77
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 D = 1° 08' 41"
 T = 72.83
 L = 145.64
 R = 5,005.00
 PC STA. = 457+25.95
 PT STA. = 458+71.59



REVISIONS	BY	DATE	SURVEY BOOK No.	SURV. BOOK	PINELLAS COUNTY, FLORIDA Public Works	DATE	PROGRESS ENERGY TRAIL PLAN STA. 452+20-461+80	DATE: 05/09
			SURVEY DIVISION	BY	BRENT D. McMURTRIE, PE			P.I.D. NO.: 920499
			DESIGNED	CWC/SJH	FLORIDA PROFESSIONAL ENGINEER NO. 66604			SURVEY FILE NO.:
			DRAWN	CWC/SJH				SHEET 11 OF 39
			CHECKED	SAL				DEPT: DRAWING



XREFS: XREFS 5/27/2009 2:24:45 PM K:\66012500\66012500\PLAN\002.dgn

CONST. MES (1:4) (12"x18") (CD)
FDOT INDEX NO. 272
FL 47.7

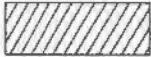

STA. 462+03.44, 5.00' (LT)
END GRAVITY WALL
END HANDRAIL

STA. 461+98.00
END 4" CONC. SW
BEGIN 6" CONC. SW

STA. 461+96.76
END GUARDRAIL
CONST. PARALLEL
END ANCHORAGE

STA. 462+11.00, 12.50' (RT)
END 6" CONC. SW
SAWCUT AND MATCH EXIST. SW

N 58° 59' 1" W
N 28° 37' 45" W

 = 6" CONCRETE SIDEWALK
 = 4" CONCRETE SIDEWALK

EDGE OF TRAVELWAY
EXIST. SHLDR.

MATCH EXIST. GROUND
REGRADE DITCH (TYP.)

BEGIN ASPHALT TRAIL
MATCH 6" CONC.

CONST. 22"
OF 12"x18" PIPE

TRAIL
ENDS

S-100
18"x24"
700-40-1
STA. 462+30

END BORDER CURB
+79.00
8.50' (LT)

CONST. BOLLARDS & FEATURES PER
PINELLAS COUNTY INDEX 1365

CONST. BORDER CURB
PT STA. 462+61.83

END BORDER CURB
+37.00
8.50' (RT)

TRANSMISSION TOWER

CONST. BOLLARDS & FEATURES PER
PINELLAS COUNTY INDEX 1365

EXIST. BOLLARD AND CABLE FENCE TO BE
REMOVED IN AREA OF TRAIL CONSTRUCTION

MATCH EXIST. GROUND

CONST. MES (1:4) (12"x18") (CD)
FDOT INDEX NO. 272
FL 47.80

STA. 462+16 LT AND RT
BEGIN BORDER CURB

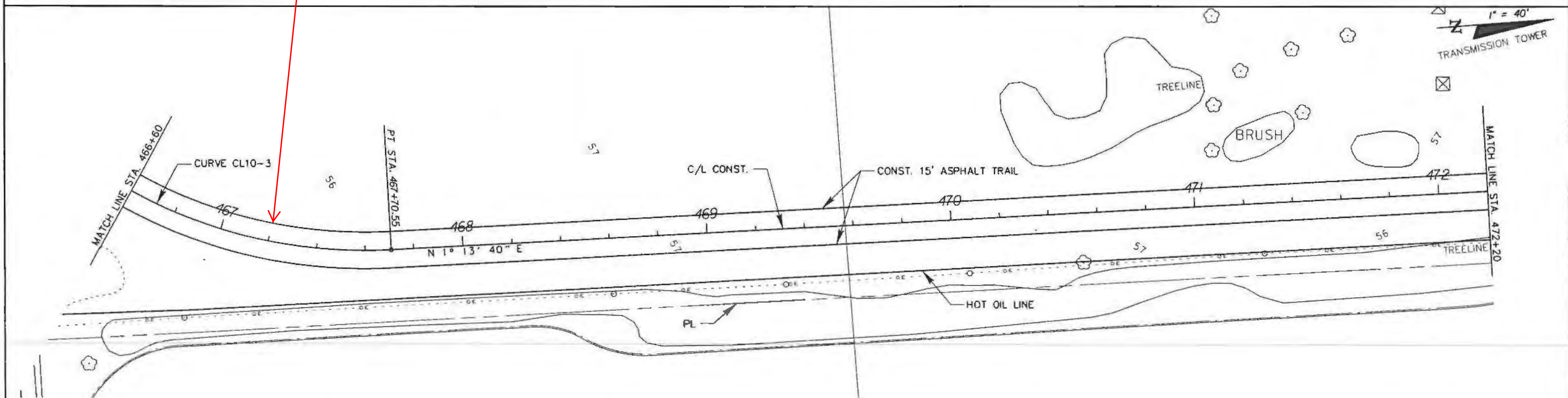
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PC STA. = 462+11.86
PT STA. = 462+61.83



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PRC STA. = 466+23.89

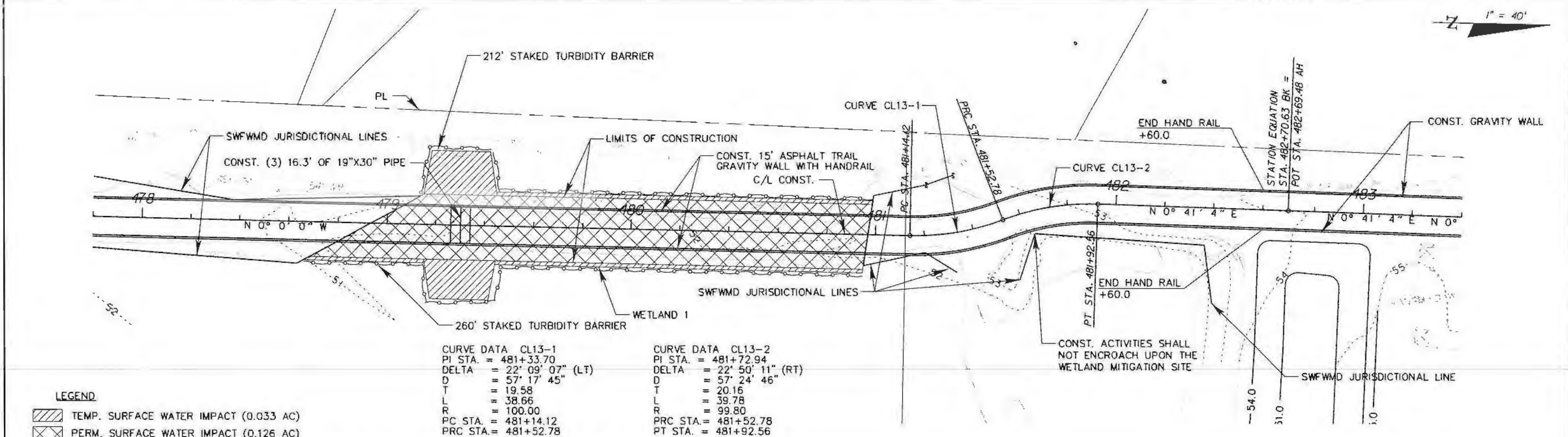
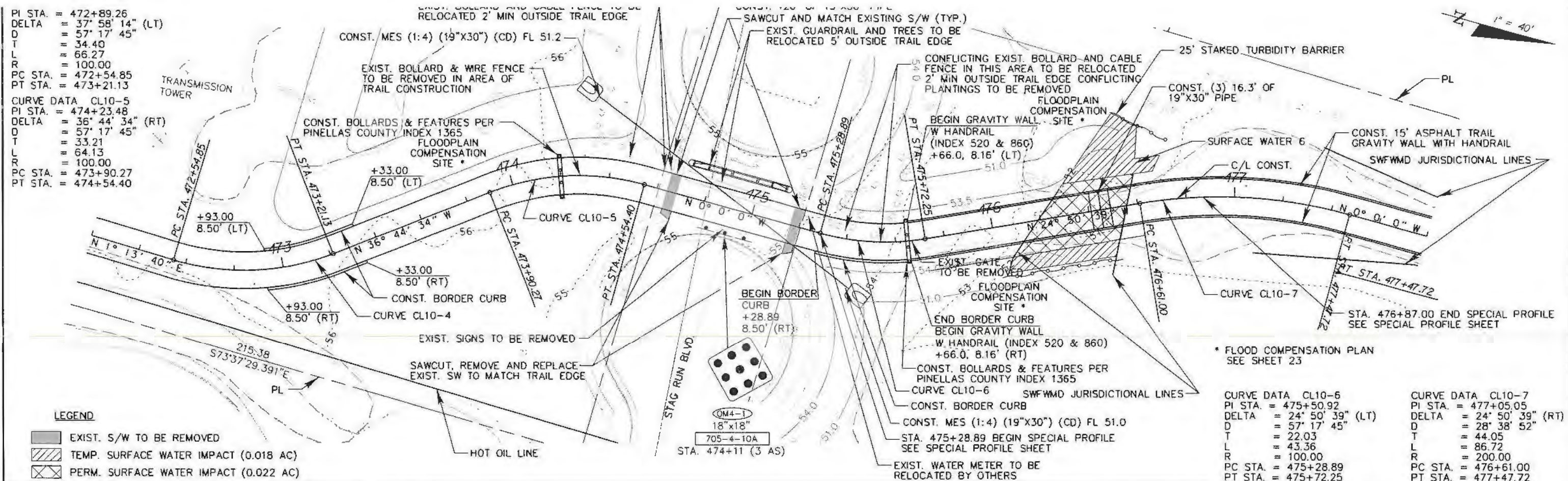
CURVE DATA CL10-3
PI STA. = 467+00.69
DELTA = 42° 00' 45" (LT)
D = 28' 38' 53"
T = 76.80
L = 146.65
R = 200.00
PC STA. = 466+23.89
PT STA. = 467+70.55

1" = 40'

At all public road crossings that do not
access other than the Trail, the Trail
to FDOT weight standards to accommodate
80,000 pound trucks



REVISIONS		BY	DATE	SURVEY BOOK No. SURV. BOOK	
				SURVEY DIVISION	BY DATE
				DESIGNED	CWC/SJH 5/06
				DRAWN	CWC/SJH 5/06
				CHECKED	SAL 5/06
				PINELLAS COUNTY, FLORIDA Public Works	
				BRENT D. McMURTRIE, PE ENGINEER FLORIDA PROFESSIONAL ENGINEER NO. 66604	
				AYRES ASSOCIATES 8875 Hidden River Parkway Suite 200 Tampa, Florida 33637 (813) 978-8688 Certification of Authorization No. 4356	
				 	
				PROGRESS ENERGY TRAIL PLAN STA. 461+80-472+20	
				DATE: 05/09 P.I.D. NO.: 920499 SURVEY FILE NO.: SHEET 12 OF 39 DEPT: DRAWING:	

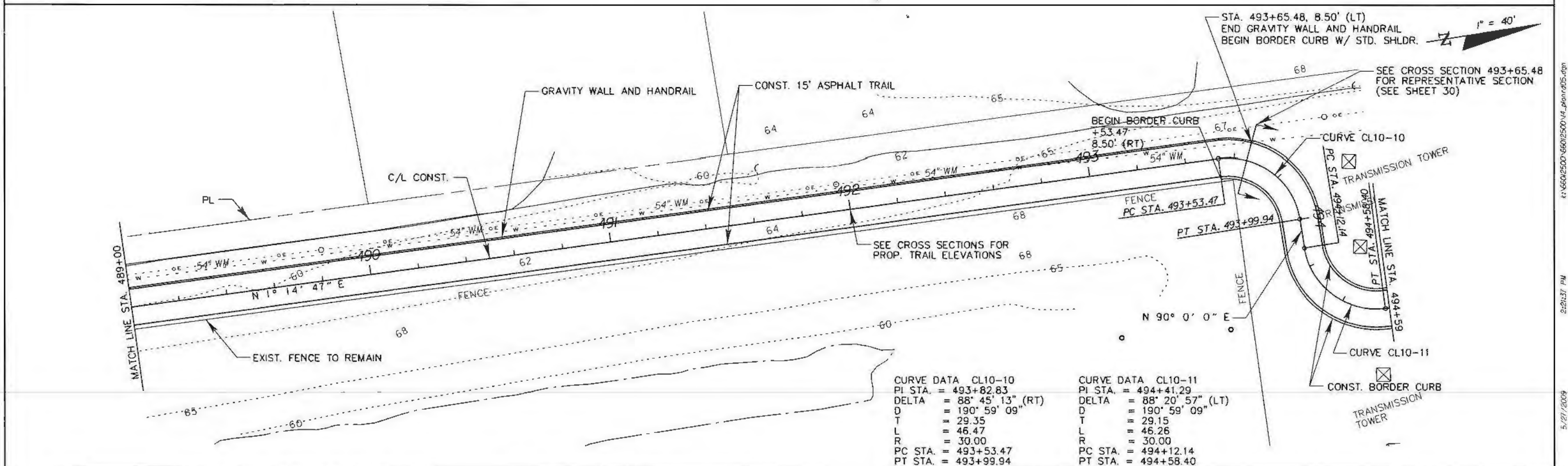
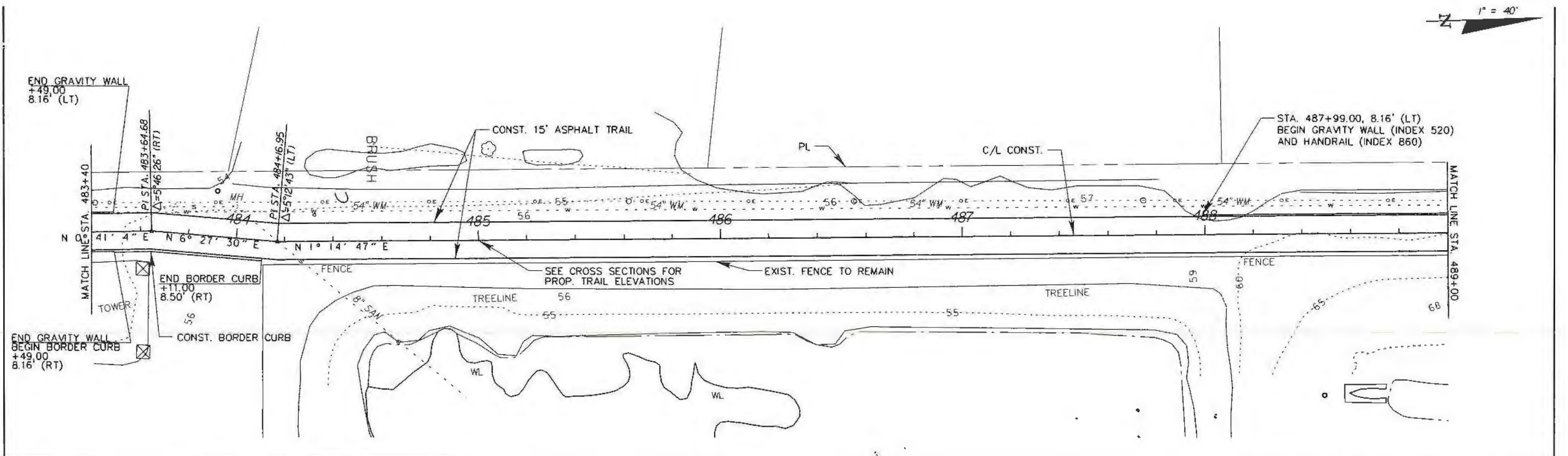


REVISIONS	BY	DATE	SURVEY BOOK No.	SURV. BOOK
			SURVEY DIVISION	BY DATE
			SURVEYED	
			TECHNICIAN	
			CHECKED	
			DESIGN DIVISION	
			DESIGNED	CWC/SJA 5/06
			DRAWN	CWC/SJA 5/06
			CHECKED	SAL 5/06

PINELLAS COUNTY, FLORIDA Public Works	
BRENT D. McMURTRIE, PE ENGINEER FLORIDA PROFESSIONAL ENGINEER NO. 66604	AYRES ASSOCIATES 8875 Hidden River Parkway Suite 200 Tampa, Florida 33637 813.976-8688 Certification of Authorization No. 4356
DATE	



PROGRESS ENERGY TRAIL PLAN STA. 472+20-483+40		DATE 05/09
		P.I.D. NO.: 920499
		SURVEY FILE NO.:
		SHEET 13 OF 39
		DEPT. DRAWING



REVISIONS		BY	DATE	SURVEY BOOK No. SURV. BOOK	
				SURVEY DIVISION	BY DATE
				TECHNICIAN	
				CHECKED	
				DESIGN DIVISION	
				DESIGNED	CWC/SJH 5/06
				DRAWN	CWC/SJH 5/06
				CHECKED	SAL 5/06

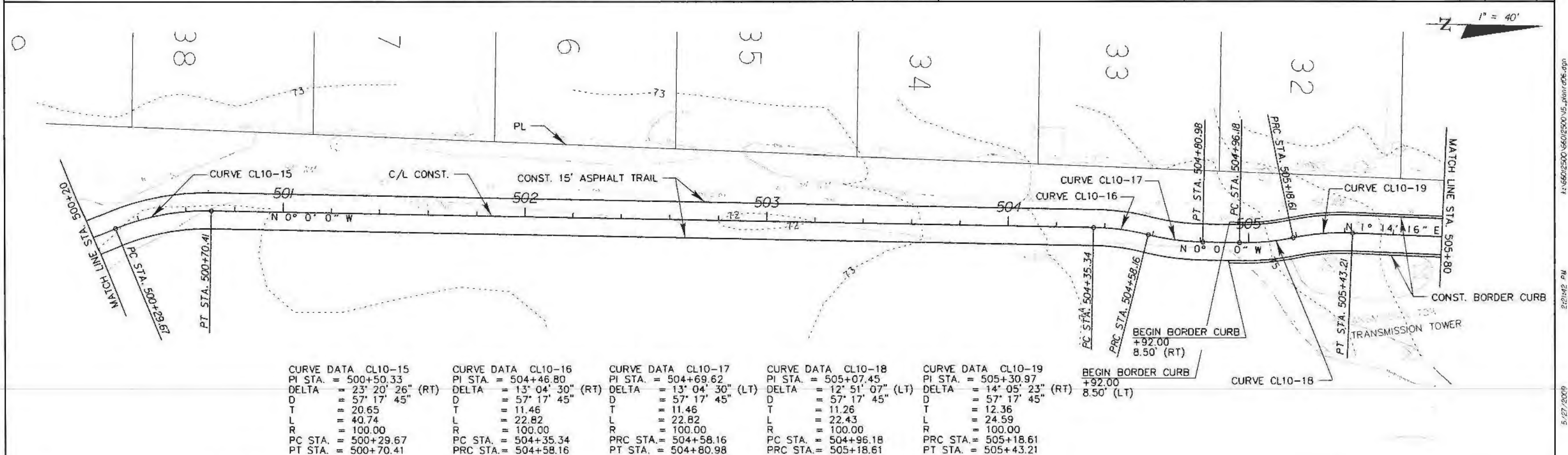
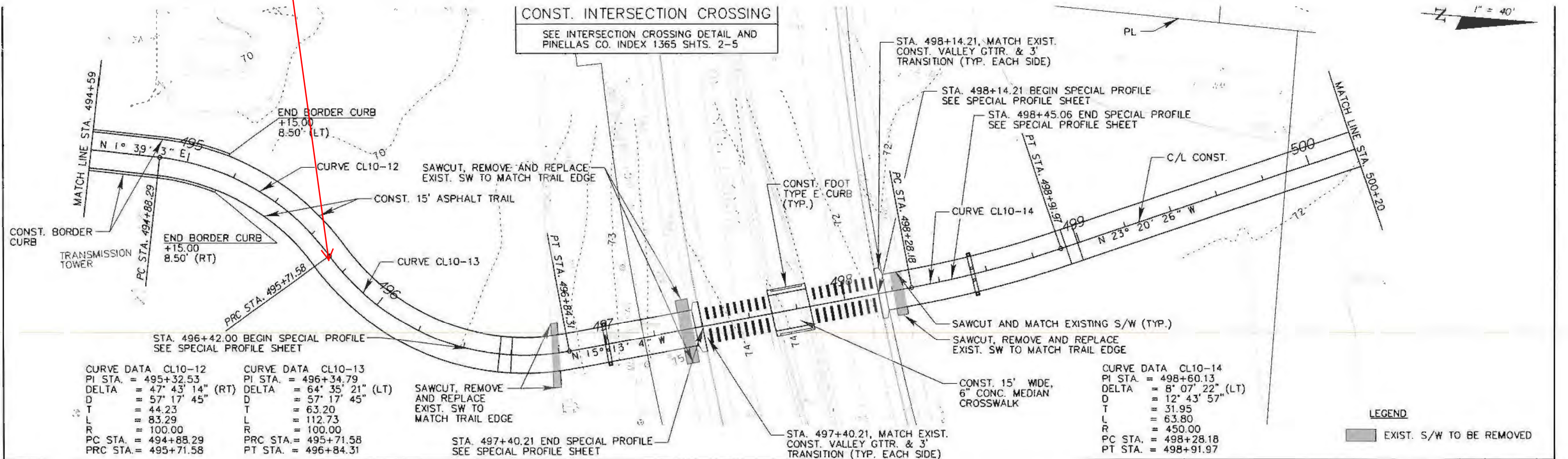
PINELLAS COUNTY, FLORIDA		Public Works	
BRENT D. McMURTRIE, PE		ENGINEER	
FLORIDA PROFESSIONAL ENGINEER NO. 66604		DATE	

AYRES ASSOCIATES	6875 Hidden River Parkway Suite 200 Tampa, Florida 33637 (813) 978-8686
Certification of Authorization No. 4356	

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PROGRESS ENERGY TRAIL	
PLAN	
STA. 483+40-494+59	

DATE: 05/09	
P.I.D. NO.: 920499	
SURVEY FILE NO.:	
SHEET 14 OF 39	DEPT. DRAWING:



REVISIONS	BY	DATE	SURVEY BOOK No.	SURV. BOOK
			SURVEY DIVISION	BY DATE
			TECHNICIAN	
			CHECKED	
			DESIGN DIVISION	
			DESIGNED	CWC/SJH 5/06
			DRAWN	CWC/SJH 5/06
			CHECKED	SAL 5/06

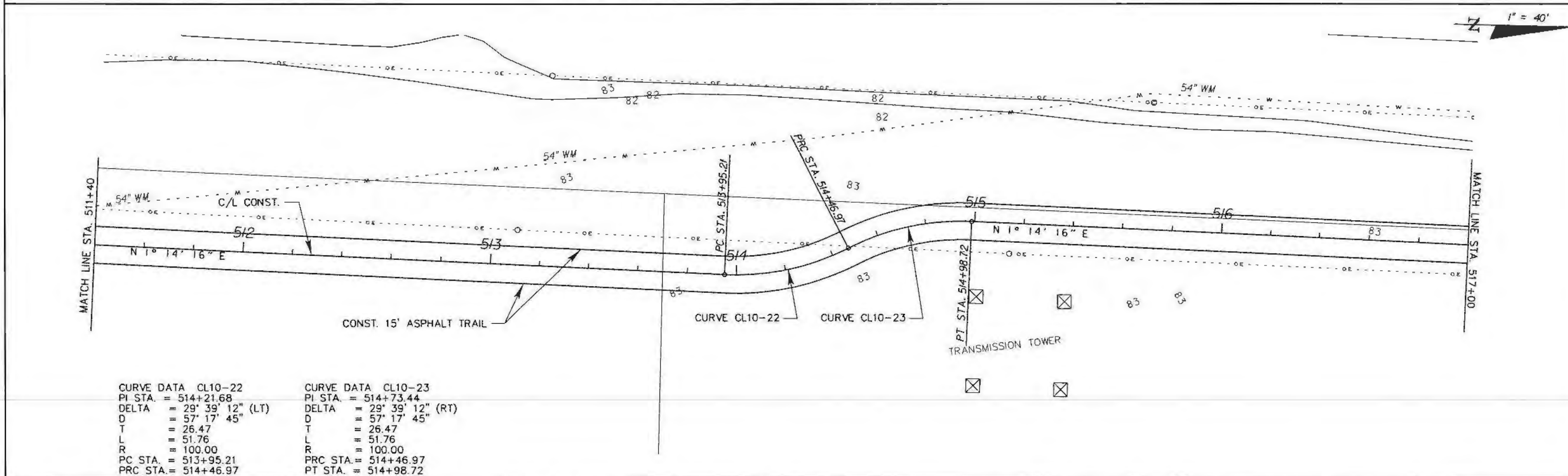
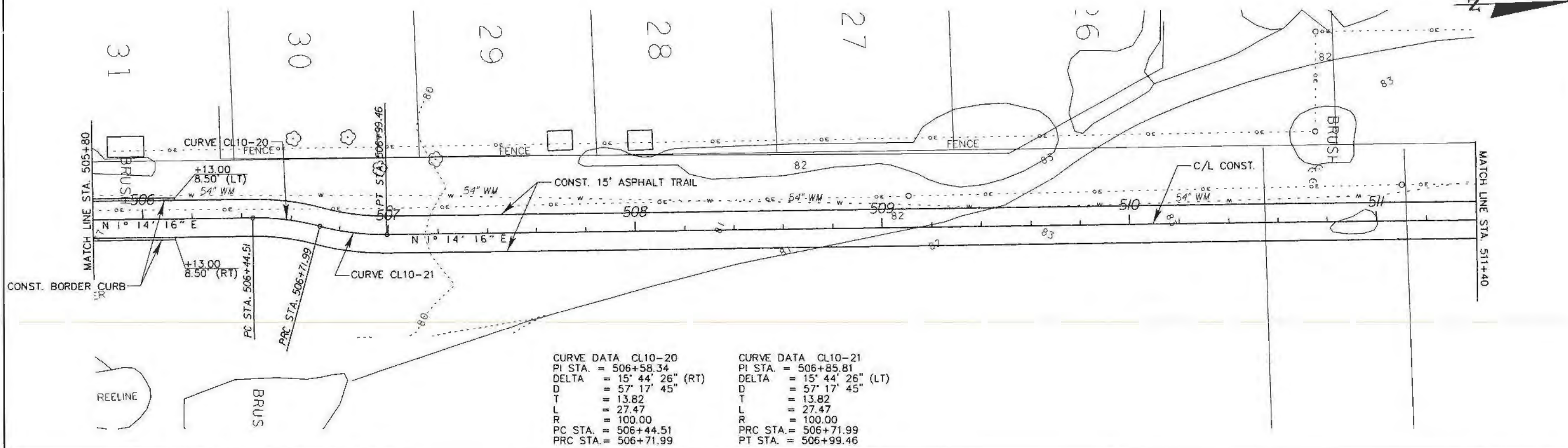
PINELLAS COUNTY, FLORIDA	
Public Works	
BRENT D. McMURTRIE, PE	ENGINEER
FLORIDA PROFESSIONAL ENGINEER NO. 66604	
DATE	

AYRES	8875 Hidden River Parkway Suite 200 Tampa, Florida 33637 (813) 978-9688
Certification of Authorization No. 4356	

Progress Energy	Pinellas County
------------------------	------------------------

PROGRESS ENERGY TRAIL	
PLAN	
STA. 494+59-505+80	

DATE: 05/09
P.I.D. NO.: 920499
SURVEY FILE NO.:
SHEET 15 OF 39
DEPT: DRAWING:

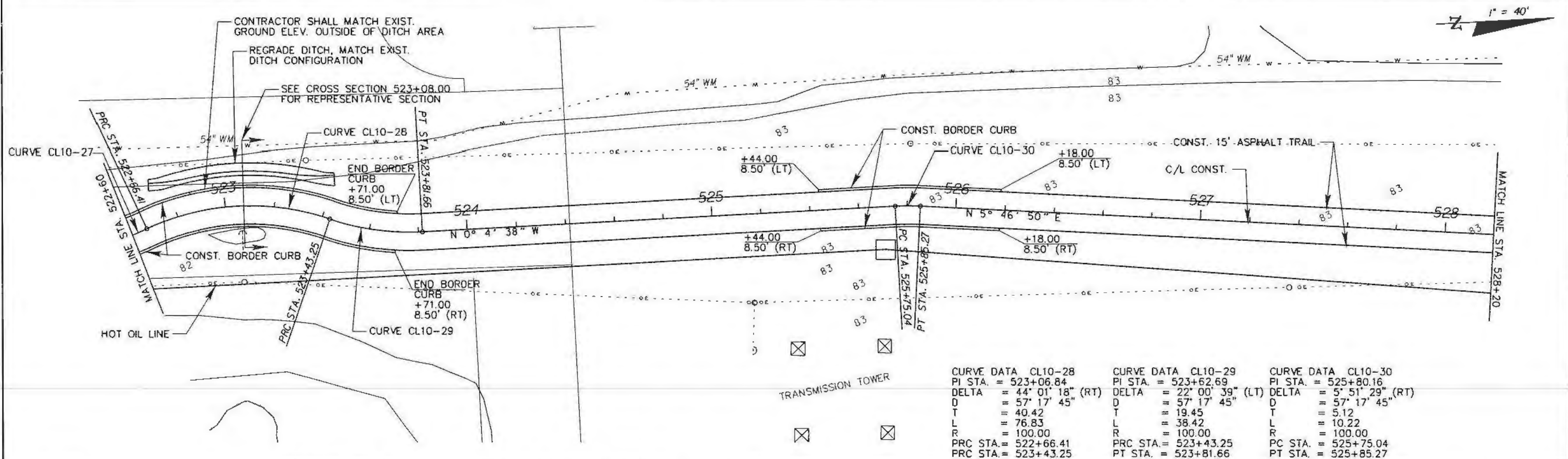
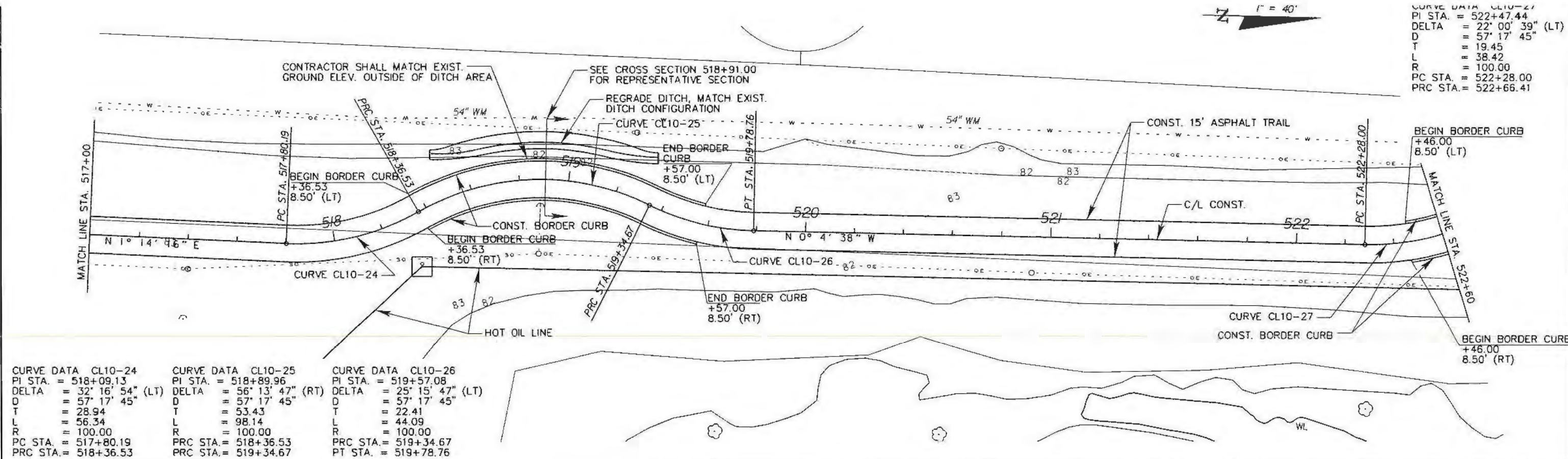


REVISIONS			BY	DATE	SURVEY BOOK No.	SURV. BOOK
					SURVEY DIVISION	BY DATE
					SURVEYED	
					TECHNICIAN	
					CHECKED	
					DESIGN DIVISION	
					DESIGNED	CWC/SJH 5/08
					DRAWN	CWC/SJH 5/08
					CHECKED	SAL 5/08

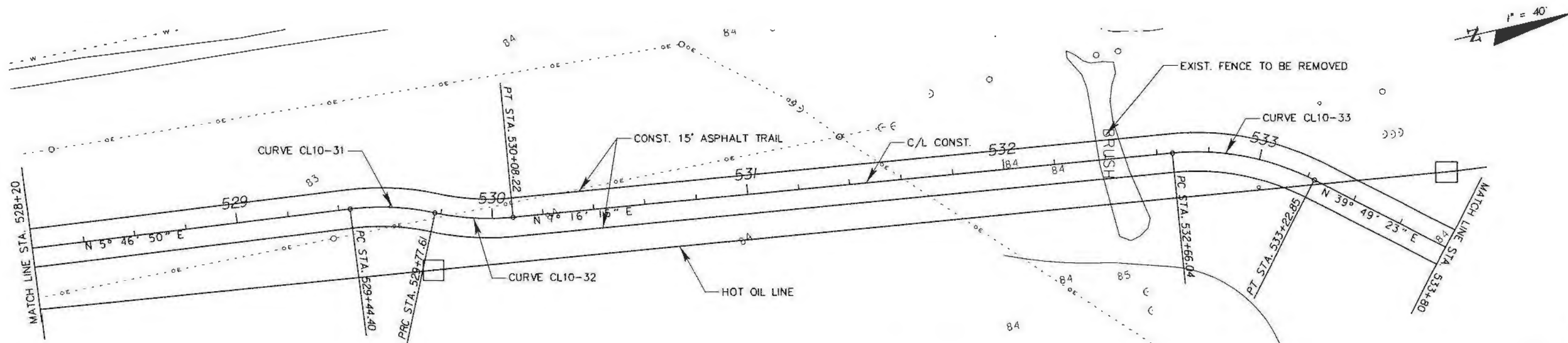
PINELLAS COUNTY, FLORIDA Public Works	
BRENT D. McMURTRIE, PE ENGINEER	AYRES ASSOCIATES 8875 Hidden River Parkway Suite 200 Tampa, Florida 33637 (813) 978-8688 Certification of Authorization No. 4356
FLORIDA PROFESSIONAL ENGINEER NO. 66604	DATE

PROGRESS ENERGY TRAIL PLAN STA. 505+80-517+00	

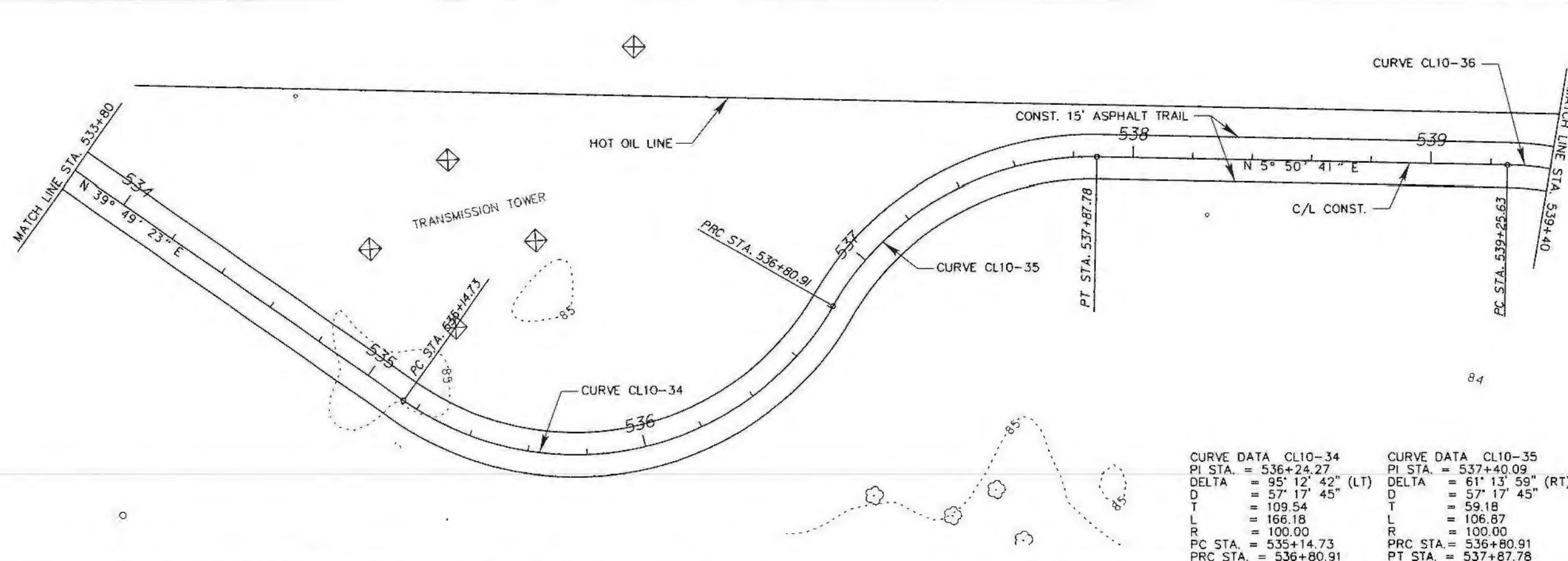
DATE: 05/09
P.I.D. NO.: 920499
SURVEY FILE NO.:
SHEET 16 OF 39
DEPT: DRAWING:





REVISIONS		BY	DATE	SURVEY BOOK No.: SURV. BOOK	PINELLAS COUNTY, FLORIDA		Progress Energy		DATE: 05/09	
				SURVEY DIVISION	Public Works		Pinellas County		P.I.D. No.: 920499	
				SURVEYED	BRENT D. McMURTRIE, PE				SURVEY FILE No.:	
				TECHNICIAN	ENGINEER				SHEET 17 OF 39	
				CHECKED	FLORIDA PROFESSIONAL ENGINEER NO. 66604				DEPT: DRAWING	
				DESIGN DIVISION	AYRES ASSOCIATES					
				DESIGNED	9875 Hidden River Parkway					
				DRAWN	Suite 200					
				CHECKED	Tampa, Florida 33637					
					(813) 978-8688					
					Certification of Authorization No. 4356					
					DATE					

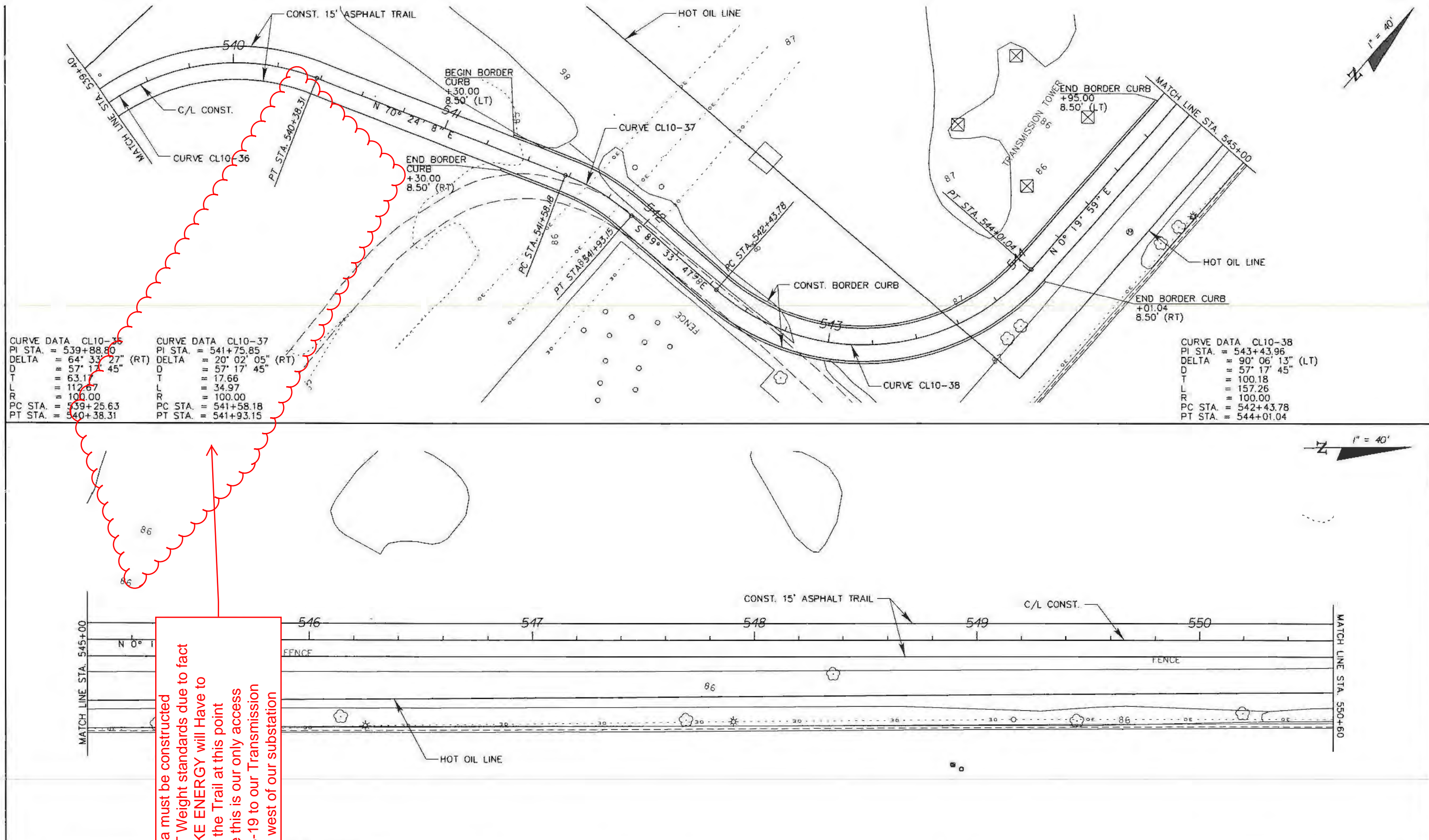


CURVE DATA CL10-31			CURVE DATA CL10-32			CURVE DATA CL10-33		
PI STA.	=	529+61.16	PI STA.	=	529+93.03	PI STA.	=	532+95.23
DELTA	=	19° 01' 40" (RT)	DELTA	=	17° 32' 16" (LT)	DELTA	=	32° 33' 08" (RT)
D	=	57' 17' 45"	D	=	57' 17' 45"	D	=	57' 17' 45"
T	=	16.76	T	=	15.43	T	=	29.20
L	=	33.21	L	=	30.61	L	=	56.81
R	=	100.00	R	=	100.00	R	=	100.00
PC STA.	=	529+44.40	PRC STA.	=	529+77.61	PC STA.	=	532+66.04
PRC STA.	=	529+77.61	PT STA.	=	530+08.22	PT STA.	=	533+22.85





CURVE DATA CL10-34			CURVE DATA CL10-35			CURVE DATA CL10-36		
PI STA.	=	536+24.27	PI STA.	=	537+40.09	PI STA.	=	539+88.80
DELTA	=	95° 12' 42" (LT)	DELTA	=	61° 13' 59" (RT)	DELTA	=	64° 33' 27" (RT)
D	=	57' 17' 45"	D	=	57' 17' 45"	D	=	57' 17' 45"
T	=	109.54	T	=	59.18	T	=	63.17
L	=	166.18	L	=	106.87	L	=	112.67
R	=	100.00	R	=	100.00	R	=	100.00
PC STA.	=	535+14.73	PRC STA.	=	536+80.91	PC STA.	=	539+25.63
PRC STA.	=	536+80.91	PT STA.	=	537+87.78	PT STA.	=	540+38.31

REVISIONS	BY	DATE	SURVEY BOOK No. SURV. BOOK		PINELLAS COUNTY, FLORIDA Public Works		 Progress Energy 	PROGRESS ENERGY TRAIL PLAN STA. 528+20-539+40		DATE: 05/09	
			SURVEY DIVISION	BY DATE						P.I.D. NO.: 920499	
			SURVEYED		BRENT D. McMURTRIE, PE ENGINEER FLORIDA PROFESSIONAL ENGINEER NO. 66604					SURVEY FILE NO.:	
			TECHNICIAN								
			CHECKED		AYRES ASSOCIATES 8875 Hidden River Parkway Suite 200 Tampa, Florida 33637 (813) 976-6688 Certification of Authorization No. 4356					SHEET 18 OF 39 DEPT. DRAWING:	
			DESIGN DIVISION								
		DESIGNED	CWC/SJH 5/06								
		DRAWN	CWC/SJH 5/06								
		CHECKED	SAL 5/06	DATE							



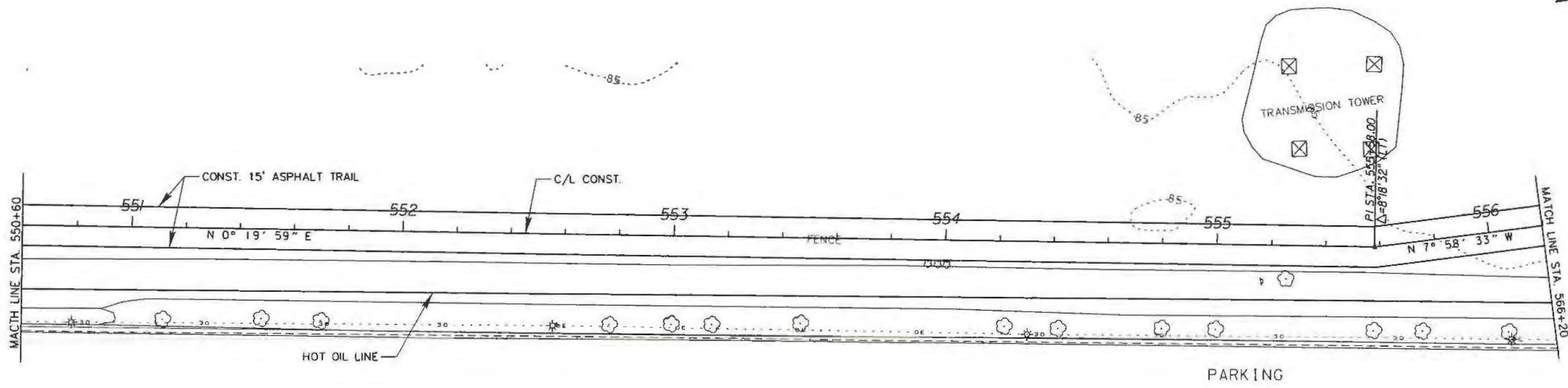
This area must be constructed to FDOT Weight standards due to fact that DUKE ENERGY will have to drive on the Trail at this point because this is our only access from US-19 to our Transmission Corridor west of our substation

REVISIONS 	BOOK No.: SURV. BOOK	PINELLAS COUNTY, FLORIDA		PROGRESS ENERGY TRAIL PLAN STA. 539+40-550+60	DATE: 05/09
	DIVISION BY DATE	Public Works			P.I.D. NO.: 920499
	DESIGNED CWC/SJA 5/06	BRENT D. McMURTRIE, PE			SURVEY FILE NO.:
	DRAWN CWC/SJA 5/06	ENGINEER			SHEET 19 OF 39
CHECKED SAL 5/06	FLORIDA PROFESSIONAL ENGINEER NO. 66604	AYRES ASSOCIATES 8875 Hidden River Parkway Suite 200 Tampa, Florida 33637 (813) 978-8608 Certification of Authorization No. 4356		DEPT. DRAWING:	

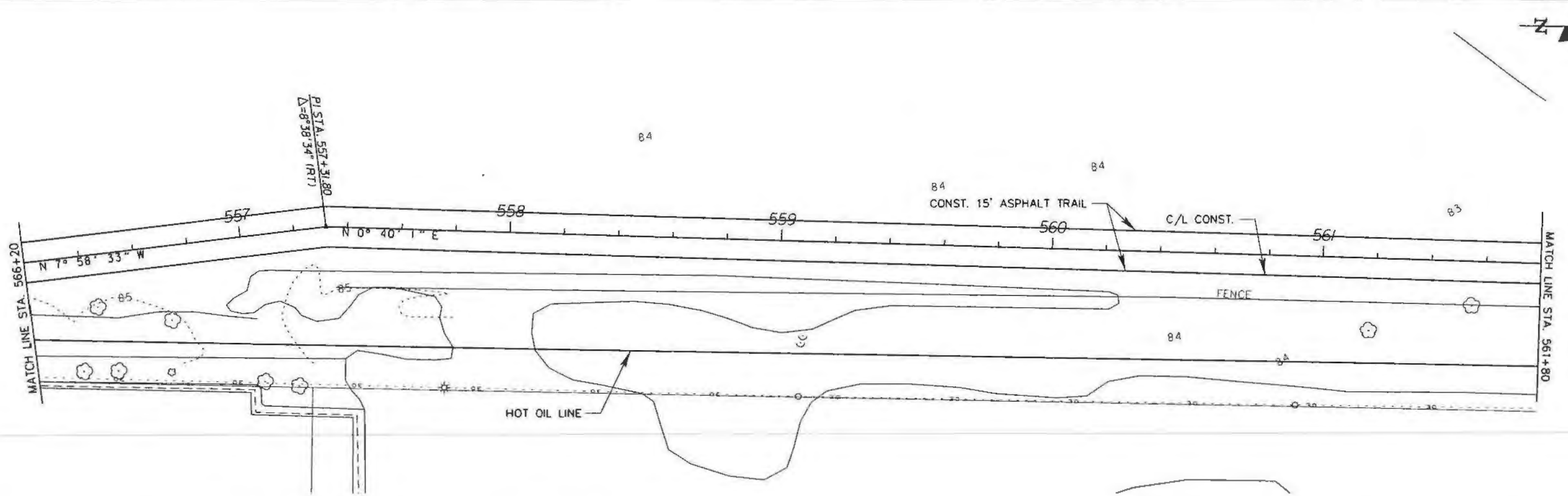


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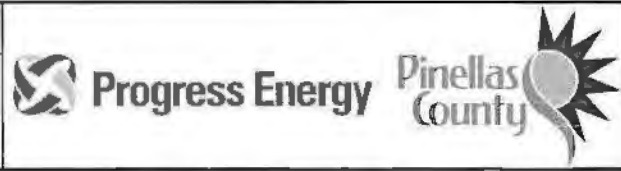
1" = 40'



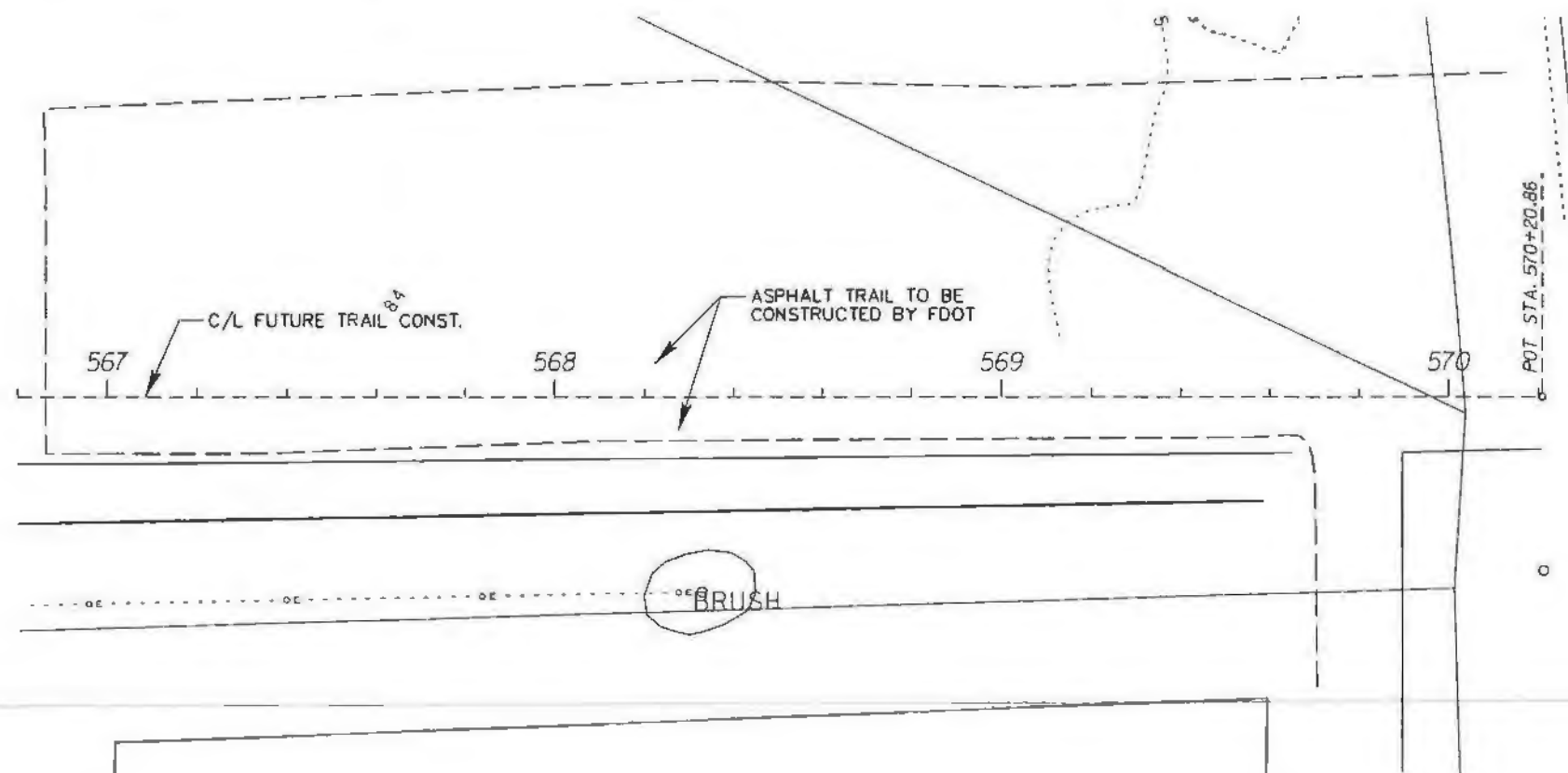
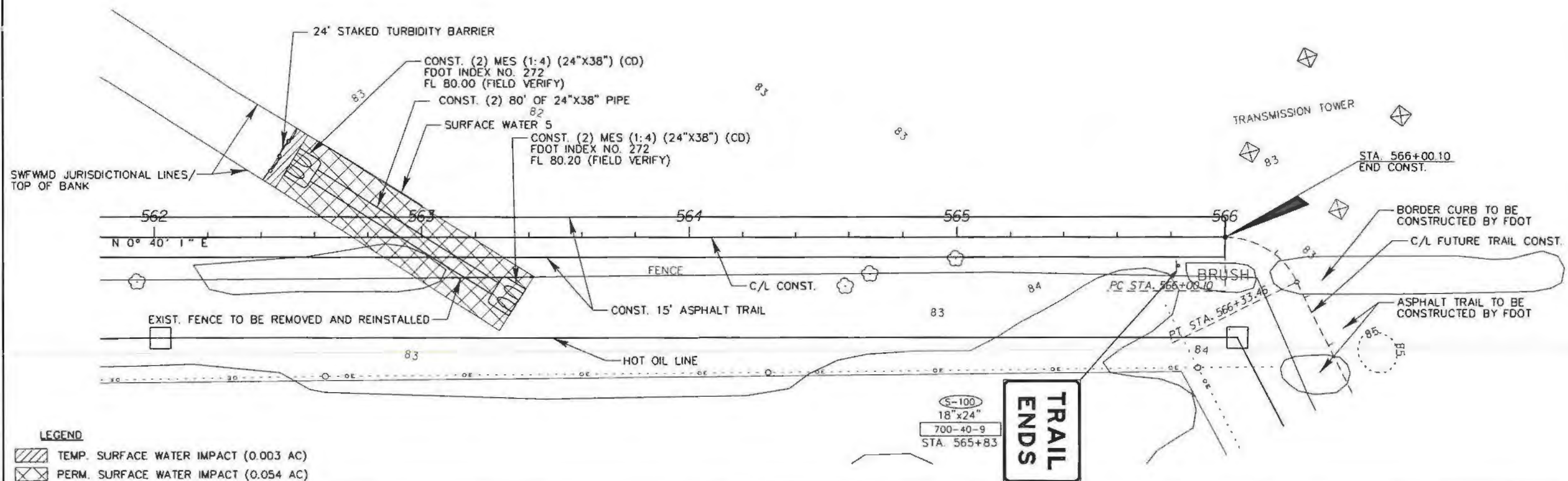
1" = 40'



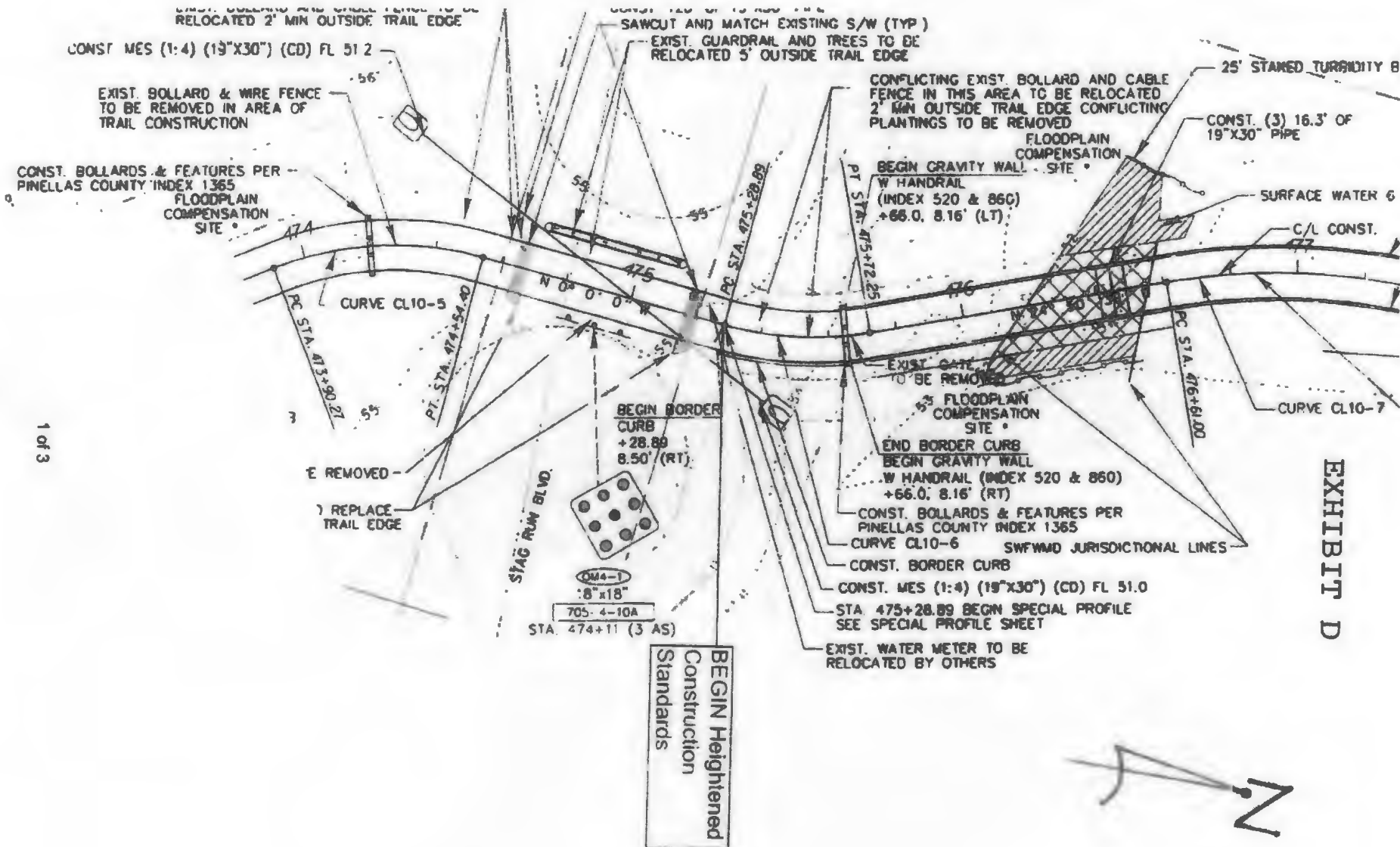
REVISIONS		BY	DATE	SURVEY BOOK No. SURV BOOK		PINELLAS COUNTY, FLORIDA		DATE: 05/09	
				SURVEY DIVISION		Public Works		P.I.D. NO.: 920499	
				SURVEYED		BRENT D. McMURTRIE, PE		SURVEY FILE NO.:	
				TECHNICIAN		ENGINEER		SHEET 20 OF 39	
				CHECKED		FLORIDA PROFESSIONAL ENGINEER NO. 66604		DEPT. DRAWING:	
				DESIGN DIVISION		AYRES ASSOCIATES			
				DESIGNED		8875 Hidden River Parkway			
				DRAWN		Suite 200			
				CHECKED		Tampa, Florida 33637			
						(813) 978-4688			
						Certification of Authorization No. 4356			
						DATE			



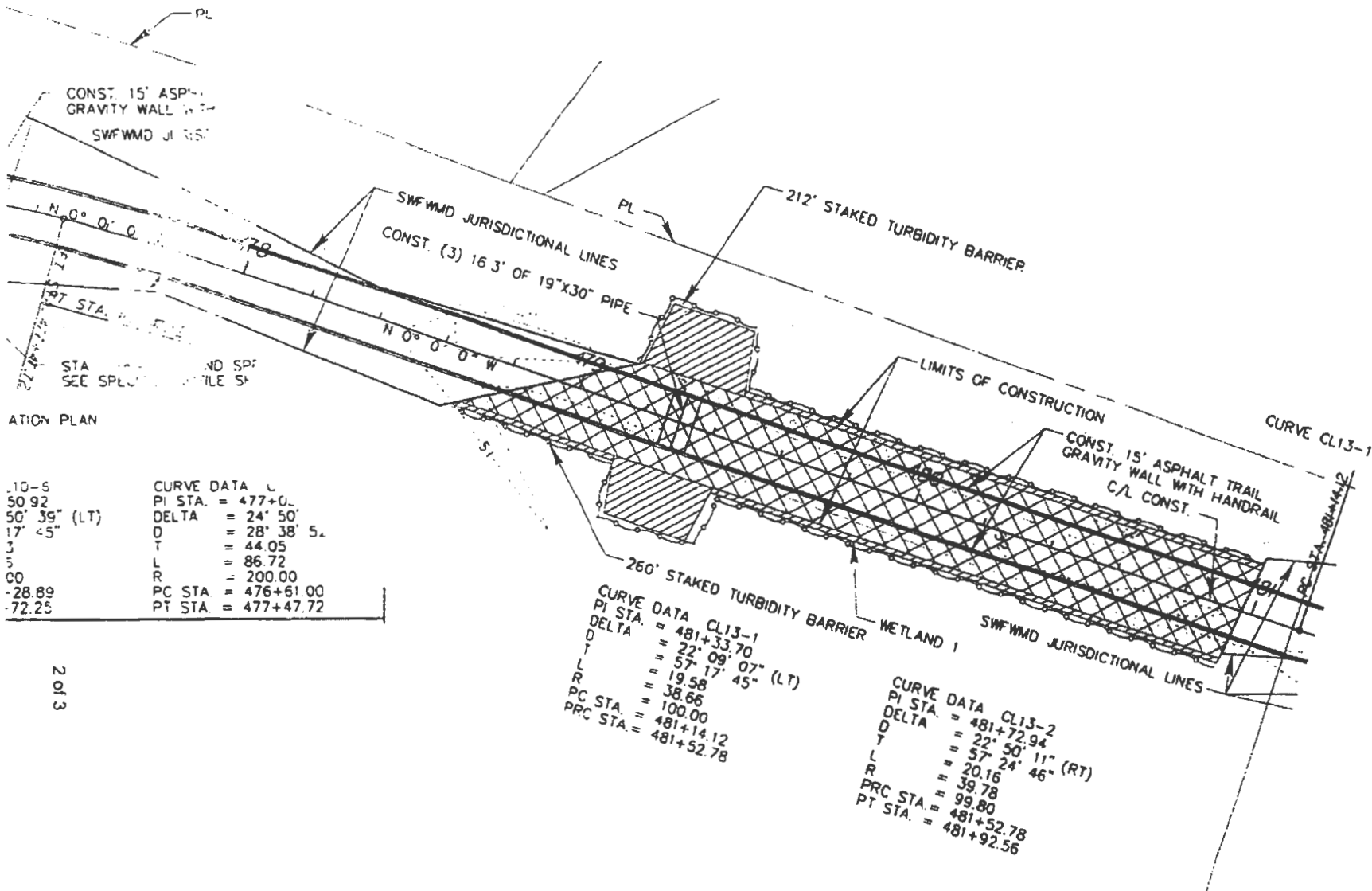
PROGRESS ENERGY TRAIL
PLAN
STA. 550+60-561+80



REVISIONS	BY	DATE	SURVEY BOOK No. SURV. BOOK	PINELLAS COUNTY, FLORIDA Public Works	Progress Energy	PINELLAS COUNTY	PROGRESS ENERGY TRAIL PLAN STA. 561+80-566+00.10	DATE: 05/09
			SURVEY DIVISION BY DATE	BRENT D. McMURTRIE, PE				P.I.D. NO.: 920499
			SURVEYED	ENGINEER				SURVEY FILE NO.:
			TECHNICIAN	FLORIDA PROFESSIONAL ENGINEER NO. 66604				SHEET 21 OF 39
			CHECKED					DEPT. DRAWING
			DESIGN DIVISION					
			DESIGNED CWC/SJH 5/06					
			DRAWN CWC/SJH 5/06					
			CHECKED SAL 5/06					



PREP



PINELLAS TRAIL LOOP

PROJECT SEGMENTS

 EXISTING
 PLANNED

* SEGMENT B2 ONLY SEGMENT SHOWN IN CIP

